

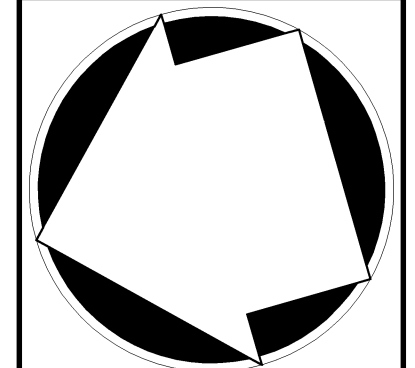
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**NOT FOR CONSTRUCTION**

NO.	REVISION	DATE	BY

Previous Editions Obsolete



**CONCEPTUAL HIGHWAY IMPROVEMENT PLAN**  
**VOLKSWAGEN OF NEWBURGH**  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK

**JMJC**  
 JOHN MEYER CONSULTING, PC

120 Bedford Road  
 Armonk, NY 10504  
 voice 914.273.5225 • fax 914.273.2102  
 www.johnmeyerconsulting.com

ROUTE 17 CARS, LLC  
 1143 DUTCHESS TURNPIKE  
 Poughkeepsie, New York 12603

**CHP-1**

# SITE PLAN APPROVAL DRAWINGS

## VOLKSWAGEN OF NEWBURGH

### SECTION 95, BLOCK 1, LOT 53

### ORANGE COUNTY

### ROUTE 17K VW DEALERSHIP

### TOWN OF NEWBURGH, NEW YORK

**APPLICANT:**

ROUTE 17 CARS, LLC  
 1143 DUTCHESS TURNPIKE  
 POUGHKEEPSIE, NEW YORK 12603

**OWNER :**

NEWBURGH COMMONS LLC  
 51 NORTH BROADWAY  
 NYACK, NEW YORK 10960

**ATTORNEY:**

DRAKE, LOEB, HELLER, KENNEDY, GOERTY, GABA, ROD, LLC.  
 DOMINIC CORDISCO, ESQ.  
 555 HUDSON VALLEY AVENUE, SUITE 100  
 NEW WINDSOR, NEW YORK 12553  
 (845) 561-1235

**SITE PLANNER, SURVEYOR, CIVIL & TRAFFIC  
 ENGINEER & LANDSCAPE ARCHITECTS:**

**JMC** JOHN MEYER CONSULTING  
 JOHN MEYER CONSULTING  
 120 BEDFORD ROAD  
 ARMONK, NEW YORK 10504  
 (914) 273-5225

**ARCHITECT:**

CLARIS CONSTRUCTION INC.  
 153 SOUTH MAIN STREET  
 NEWTOWN, CONNECTICUT 06470  
 (203) 364-9460

**JOHN MEYER CONSULTING DRAWINGS:**

- SP-1 COVER SHEET
- SP-2 EXISTING CONDITIONS/DEMOLITION PLAN
- SP-3 LAYOUT PLAN
- SP-4 GRADING PLAN
- SP-5 UTILITIES PLAN
- SP-6 SEDIMENT & EROSION CONTROL PLAN
- SP-7 LANDSCAPING PLAN
- SP-8 LIGHTING PLAN
- SP-9 CONSTRUCTION DETAILS
- SP-10 CONSTRUCTION DETAILS
- SP-11 CONSTRUCTION DETAILS
- SP-12 CONSTRUCTION DETAILS
- SP-13 CONSTRUCTION DETAILS
- SP-14 CONSTRUCTION DETAILS
- SP-15 CONSTRUCTION DETAILS
- SP-16 CONSTRUCTION DETAILS
- SP-17 TRUCK TURNING ANALYSIS PLAN
- CHP-1 CONCEPTUAL HIGHWAY IMPROVEMENT PLAN

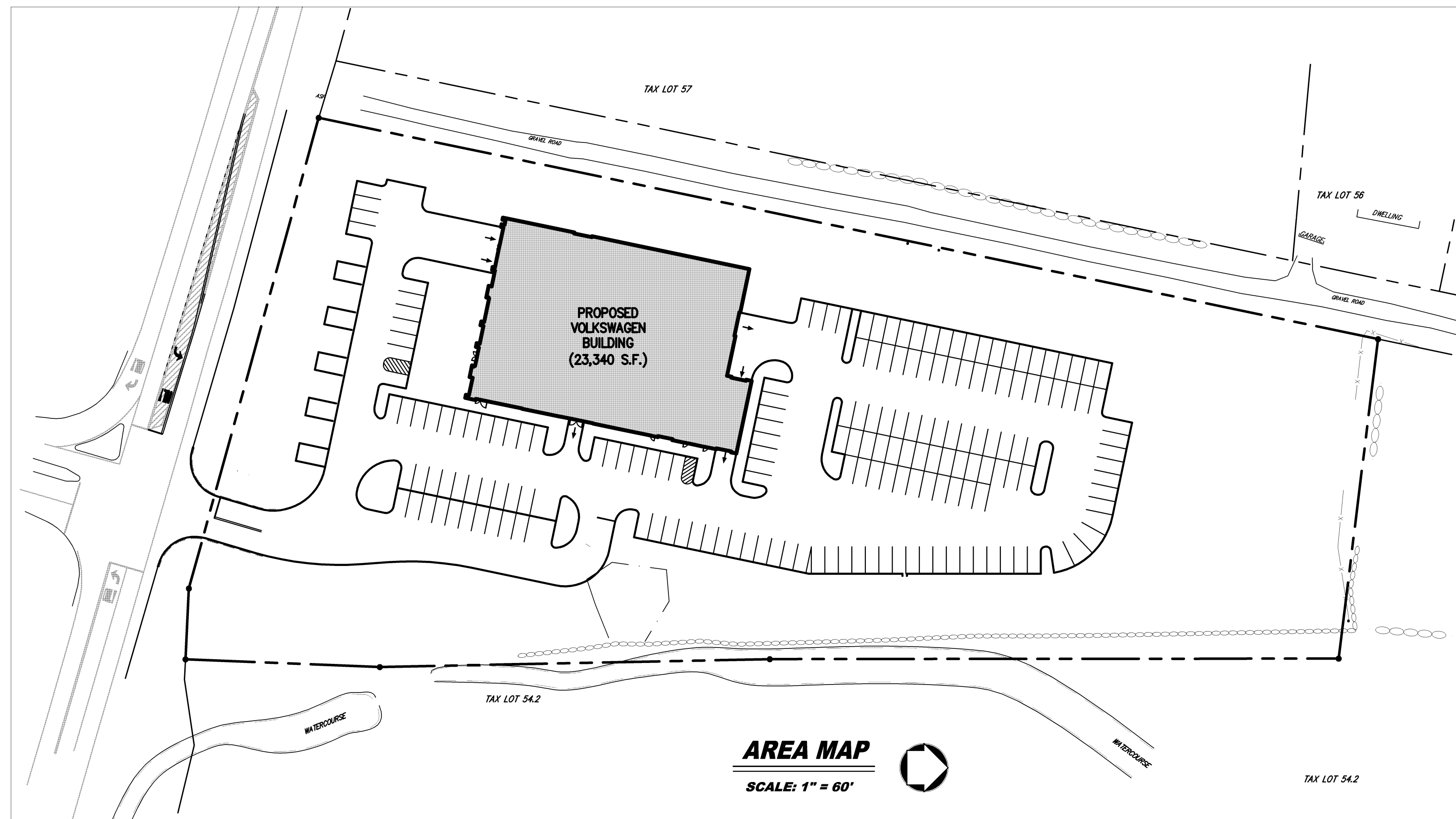
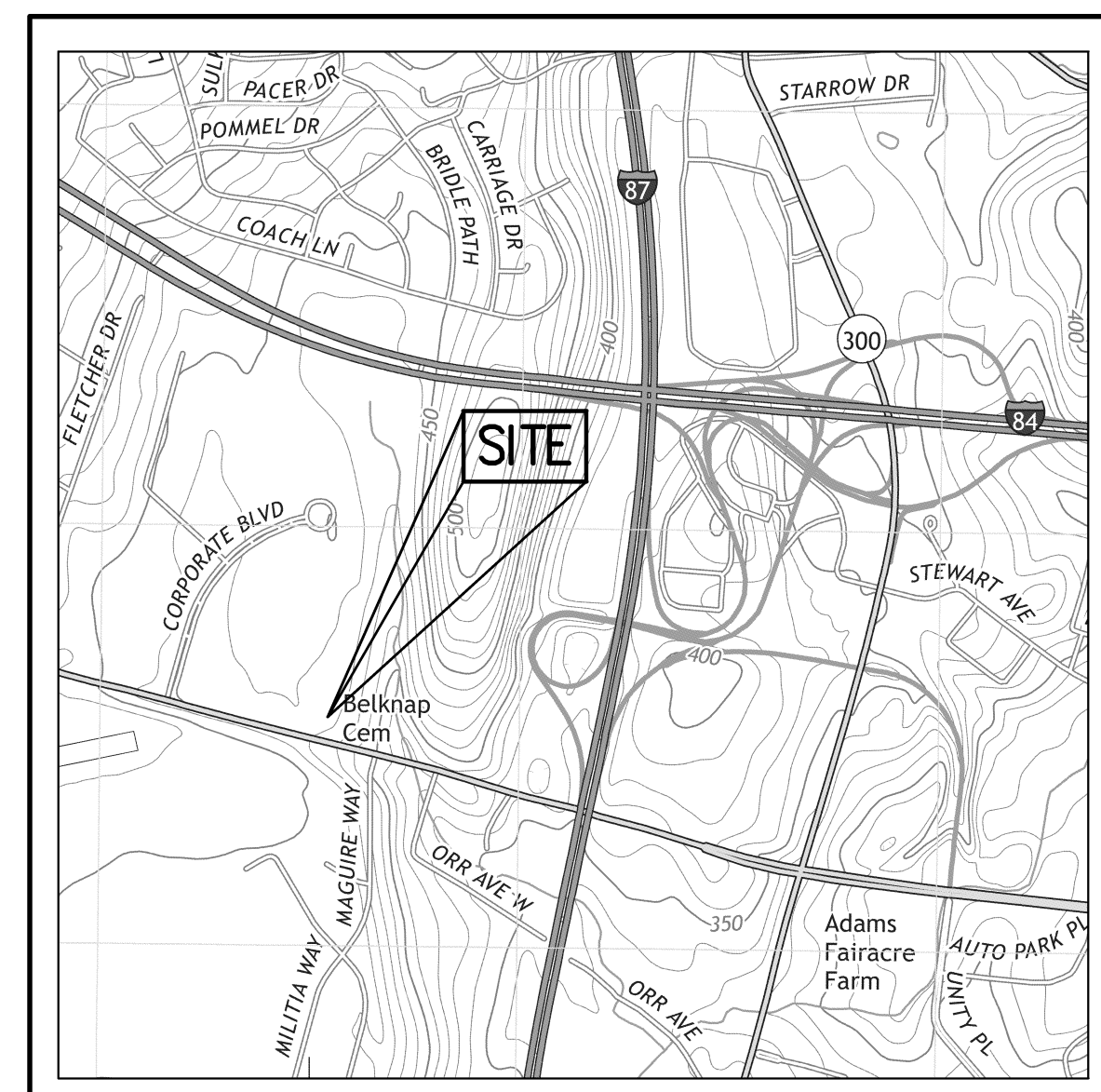
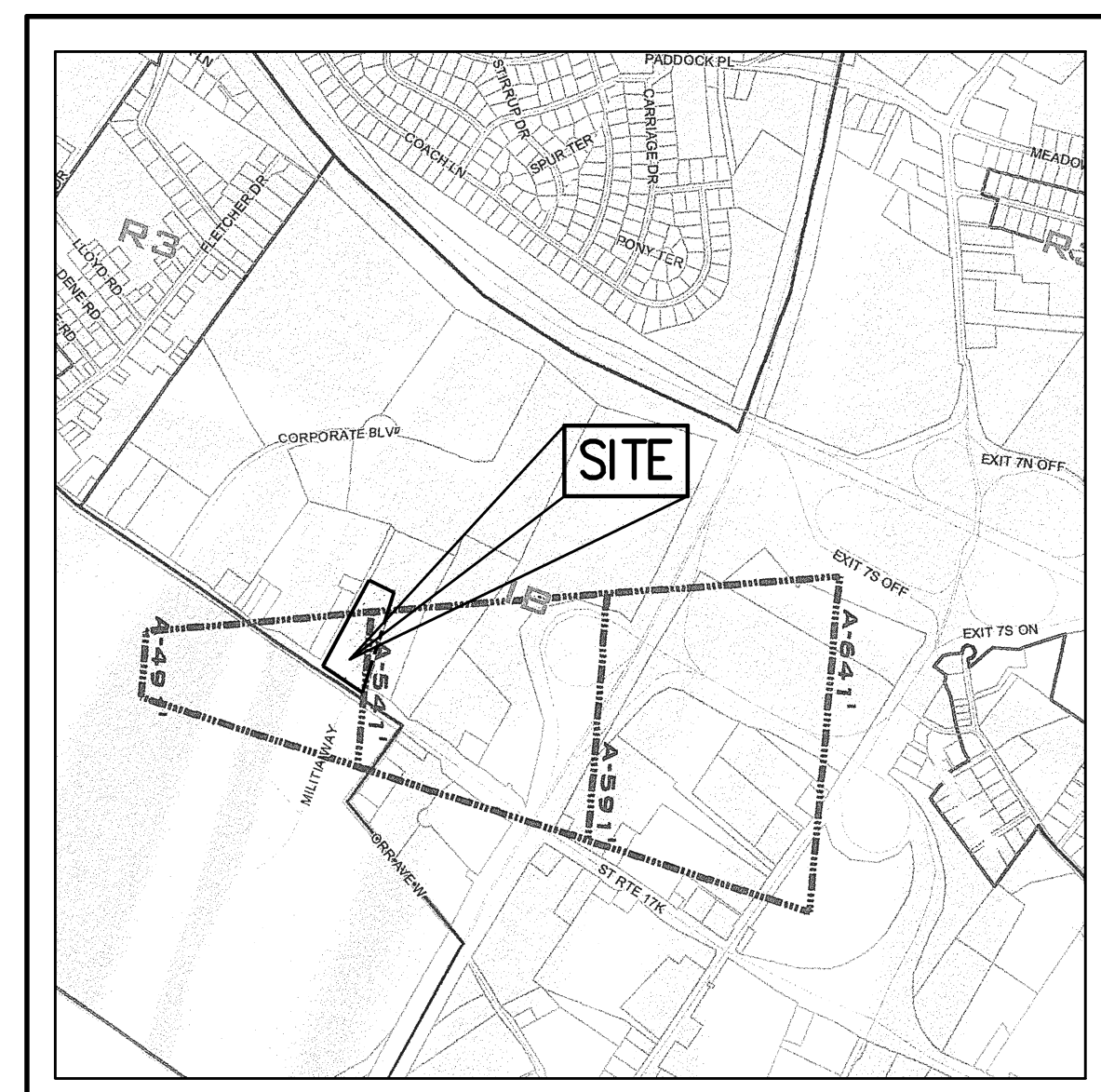


TABLE OF LAND USE			
INTERCHANGE BUSINESS DISTRICT (IB)	PERMITTED/REQUIRED	EXISTING	PROPOSED
LOT AREA (AC.)(S.F.)	0.92 AC./40,000 S.F.	5.02 AC./218,658 S.F.	5.02 AC./218,658 S.F.
LOT WIDTH (FT.)	150	372	372
LOT DEPTH (FT.)	150	733	733
SETBACK TO PARKING FRONT YARD (FT.)	35 <sup>(1)</sup>	N/A	35
<b>BUILDING SETBACKS</b>			
FRONT YARD (ROUTE 17K) (FT.)	60 <sup>(2)</sup>	124.4	136
FRONT YARD (MULBURY LANE) (FT.)	50	21.1	40 <sup>(5)</sup>
REAR YARD (FT.)	60	417.5	411.2
SIDE YARD (FT.)	30/80	20.0/252.8	40.0/137.6
LOT SURFACE COVERAGE (%)	80	8.9	57.7 <sup>(3)</sup>
LOT BUILDING COVERAGE (%)	40	2.4	9.7
PARKING LOT LANDSCAPE (%)	5	N/A	5.0
BUILDING HEIGHT (FT.)	40	N/A	26.5
<b>PARKING SUMMARY</b>			
STANDARD PARKING	-	N/A	185
HANDICAP PARKING	-	N/A	3 <sup>(6)</sup>
FUTURE PARKING	-	N/A	51
TOTAL PARKING	-	N/A	246 <sup>(4)(7)</sup>
LOADING	1	N/A	1



**VICINITY MAP**

SCALE: 1" = 1400'



**ZONING DISTRICT MAP**

N.T.S.

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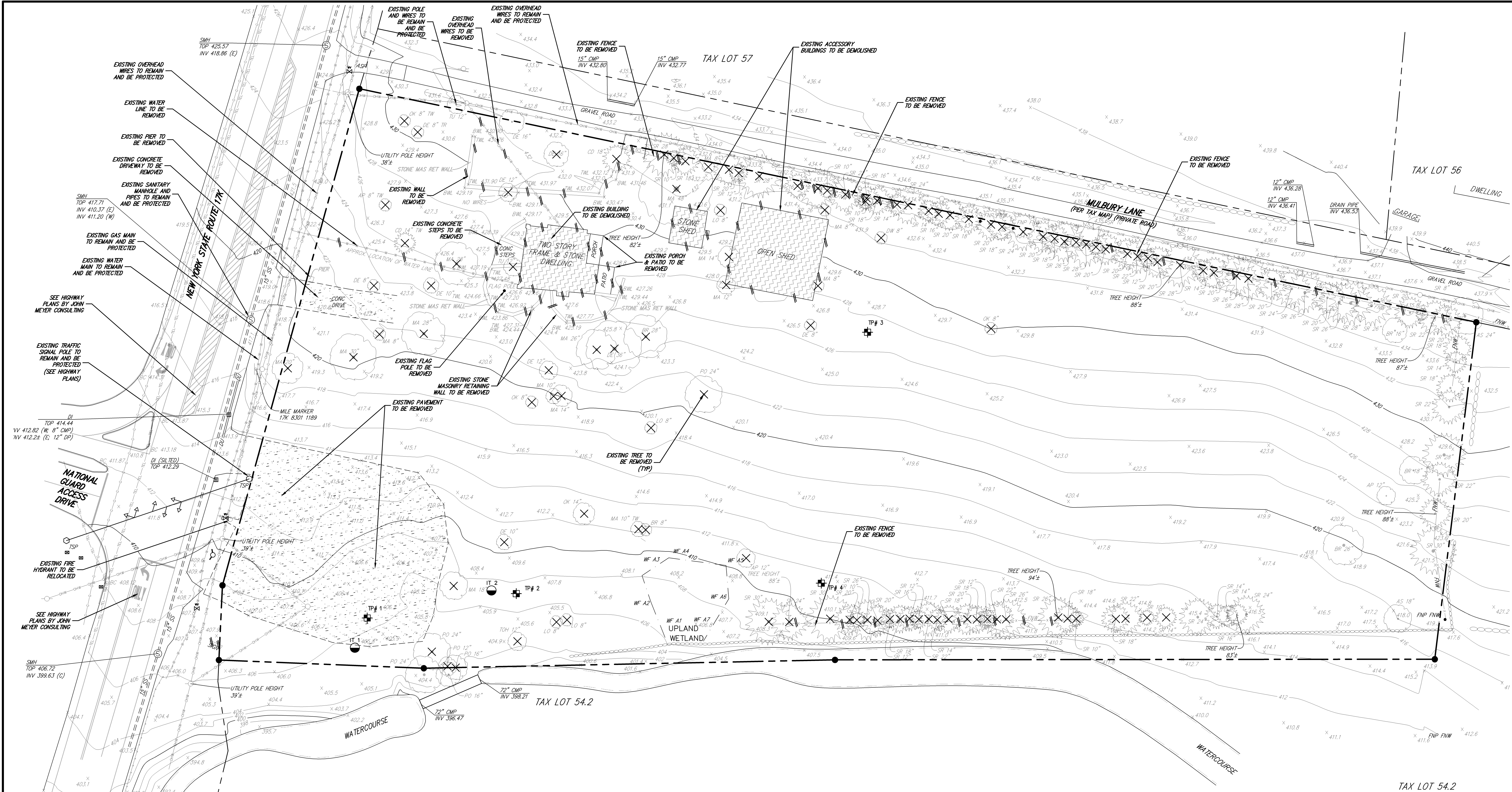
- (1) THE FIRST 35 FEET OF THE FRONT YARD SHALL BE LANDSCAPED PER SECTION 185-18 (C4-C).
- (2) A FRONT YARD ABUTTING ALL COUNTY AND STATE HIGHWAYS SHALL BE AT LEAST 60 FEET IN DEPTH PER SECTION 185-18 (C4-B).
- (3) LOT COVERAGE CALCULATION IS BASED ON PROPOSED AND FUTURE PARKING.
- (4) PHASE I PARKING IS PROPOSED AT 195 PARKING SPACES. IF PROPOSED "FUTURE PARKING INVENTORY" IS CONSTRUCTED THE TOTAL NUMBER OF PARKING SPACES IS 246 PARKING SPACES (CONSIDERS 11 PARKING SPACES WOULD BE LOST FROM PHASE I).
- (5) ZONING VARIANCE OBTAINED.
- (6) NUMBER OF HANDICAPPED PARKING SPACES IS BASED ON THE TOTAL NUMBER OF CUSTOMER EMPLOYEE AND SERVICE PARKING SPACES (70).
- (7) PARKING SUMMARY:
 

CUSTOMER PARKING SPACES	= 19
SERVICE PARKING SPACES	= 23
EMPLOYEE PARKING SPACES	= 28
NEW CAR INVENTORY PARKING SPACES	= 176
	246

NO.	REVISION	DATE	BY
1.	REVISED	03/21/2014	JE

DRAWN	JE	APPROVED	RR
SCALE: AS SHOWN			
DATE: 11/07/2013			
PROJECT NO: 13021			
DWC:	TAR:	SOP:	COVER:
1302-SITE	COVER	COVER	COVER
DRAWING NO: <b>SP-1</b>			

Previous Editions Obsolete



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JOHN MEYER CONSULTING, PC

EXISTING CONDITIONS/DEMOLITION PLAN

VOLKSWAGEN OF NEWBURGH  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

**DEMOLITION NOTES**

- EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN FROM SURVEY ENTITLED, "TOPOGRAPHIC SURVEY," PREPARED BY JOHN MEYER CONSULTING, DATED 3/15/2013.
- TEST PITS DEPICTED ON THIS PLAN WERE OBSERVED BY JOHN MEYER CONSULTING ON 10/16/2013. INFILTRATION TESTS DEPICTED ON THIS PLAN WERE PERFORMED BY JOHN MEYER CONSULTING ON 10/17/2013.
- PRIOR TO THE START OF ANY DEMOLITION THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND/OR APPROVALS FROM THE TOWN OF NEWBURGH AND ALL OTHER AUTHORITIES HAVING JURISDICTION.
- THE CONTRACTOR SHALL COORDINATE THE DISCONNECTION OF GAS AND ELECTRIC UTILITIES WITH CENTRAL HUDSON GAS & ELECTRIC CORP. UTILITY COMPANY, WATER AND SEWER CONNECTIONS WITH TOWN OF NEWBURGH UTILITIES SHALL BE PROVIDED TO THE TOWN OF NEWBURGH BUILDING DEPARTMENT, IN ACCORDANCE WITH THEIR REQUIREMENTS.
- ALL EXISTING UTILITY SERVICES TO REMAIN SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
- ALL CONSTRUCTION/DEMOLITION DEBRIS, NOT PROPOSED TO BE RECYCLED, SHALL BE REMOVED AND DISPOSED OF OFF-SITE IN ACCORDANCE WITH THE REGULATIONS OF ALL LOCAL, STATE AND FEDERAL AGENCIES HAVING JURISDICTION.
- THE CONTRACTOR SHALL COORDINATE THE TERMINATION POINTS FOR ALL EXISTING UTILITIES WITH DRAWING SP-5 "UTILITIES PLAN".
- ANY UNSUITABLE MATERIAL FOUND ON-SITE DURING CONSTRUCTION SHALL BE DISPOSED OF OFF-SITE IN A MANNER APPROVED BY ALL AUTHORITIES HAVING JURISDICTION AND REPLACED WITH SUITABLE MATERIAL AS REQUIRED. ALL REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL SHALL BE COMPLETED UNDER THE DIRECT SUPERVISION OF A GEOTECHNICAL ENGINEER.
- CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING UTILITIES TO BE DEMOLISHED AND PROTECTED. IF ANY DISCREPANCIES ARE FOUND, THE CONTRACTOR SHALL NOTIFY THE OWNER'S FIELD REPRESENTATIVE, GENERAL CONTRACTOR, AND JOHN MEYER CONSULTING PRIOR TO THE START OF CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE THE REMOVAL OF THE EXISTING SANITARY SEWER AND WATER MAIN SYSTEMS AND THE INSTALLATION OF NEW SANITARY AND WATER SYSTEMS WITH THE TOWN ENGINEER AND WATER DEPARTMENT.

**SEQUENCE OF DEMOLITION**

- INSTALLATION OF TEMPORARY BARRIERS, SEDIMENT AND EROSION CONTROL, SIGNAGE AND MAINTENANCE AND PROTECTION OF IN ACCORDANCE WITH THE GENERAL CONTRACTORS MOBILIZATION PLAN, AS APPROVED BY THE TOWN OF NEWBURGH, NY.
- DISCONNECTION OF ALL UTILITY SERVICES (GAS, ELECTRIC, WATER, SANITARY SEWER, TELEPHONE, ETC.) AS SHOWN ON THE PLAN.
- REMOVE EXISTING OIL TANK IN EXISTING HOUSE
- REMOVE AND DISPOSE OF OR STOCKPILE EXISTING BUILDING SLABS/FOOTINGS AS DIRECTED BY THE OWNER'S REPRESENTATION AND/OR GEOTECHNICAL ENGINEER.
- CLEAR AND GRUB SITE. REMOVE AND STOCKPILE EXISTING TOPSOIL.
- REMOVE AND DISPOSE OF ALL EXISTING UTILITIES, PAVEMENT, CONCRETE SLABS AND RETAINING WALLS. PREPARE THE AREA FOR FUTURE CONSTRUCTION.
- CONTINUE REMOVAL OF EXISTING PAVEMENT, UTILITIES, CONCRETE CURBS AND SIDEWALKS AS REQUIRED, WITHIN LIMITS OF SITE DEMOLITION AREA.
- ALL DEBRIS MUST BE REMOVED AND DISPOSED OFF SITE WITHIN ALL CONSTRUCTION AREAS.

**DEEP HOLE TEST LOG**

DEEP HOLE	EXISTING GRADE ELEVATION	TEST PIT DEPTHS (BOTTOM ELEV.)	DEPTH OF GROUND WATER FROM EXISTING GRADE	DEPTH OF BEDROCK FROM EXISTING GRADE
TP-1	406.00	9' (397.00)	7' (399.00)	9' (397.00)
TP-2	407.00	11' (396.00)	NOT ENCOUNTERED	NOT ENCOUNTERED
TP-3	427.00	14' (413.00)	14' (413.00)	NOT ENCOUNTERED
TP-4	411.00	10' (401.00)	NOT ENCOUNTERED	10' (401.00)

**SOIL INFILTRATION TEST DATA**

TEST HOLE	APPROXIMATE GROUND SURFACE ELEVATION	DEPTH OF INFILTRATION TEST (ELEVATION)	SOIL INFILTRATION RATE (INCHES/HOUR)
IT-1	406.00	2'0" (404.00)	7.0
IT-2	407.00	2'0" (405.00)	13.0

NOTE:  
1. DEEP HOLE SOIL TESTING AND INFILTRATION TEST HOLE PRE-SOAKING WAS PERFORMED BY JOHN MEYER CONSULTING, PC AND OBSERVED ON OCTOBER 16, 2013.  
2. INFILTRATION TESTING WAS PERFORMED BY JOHN MEYER CONSULTING, PC ON OCTOBER 17, 2013.

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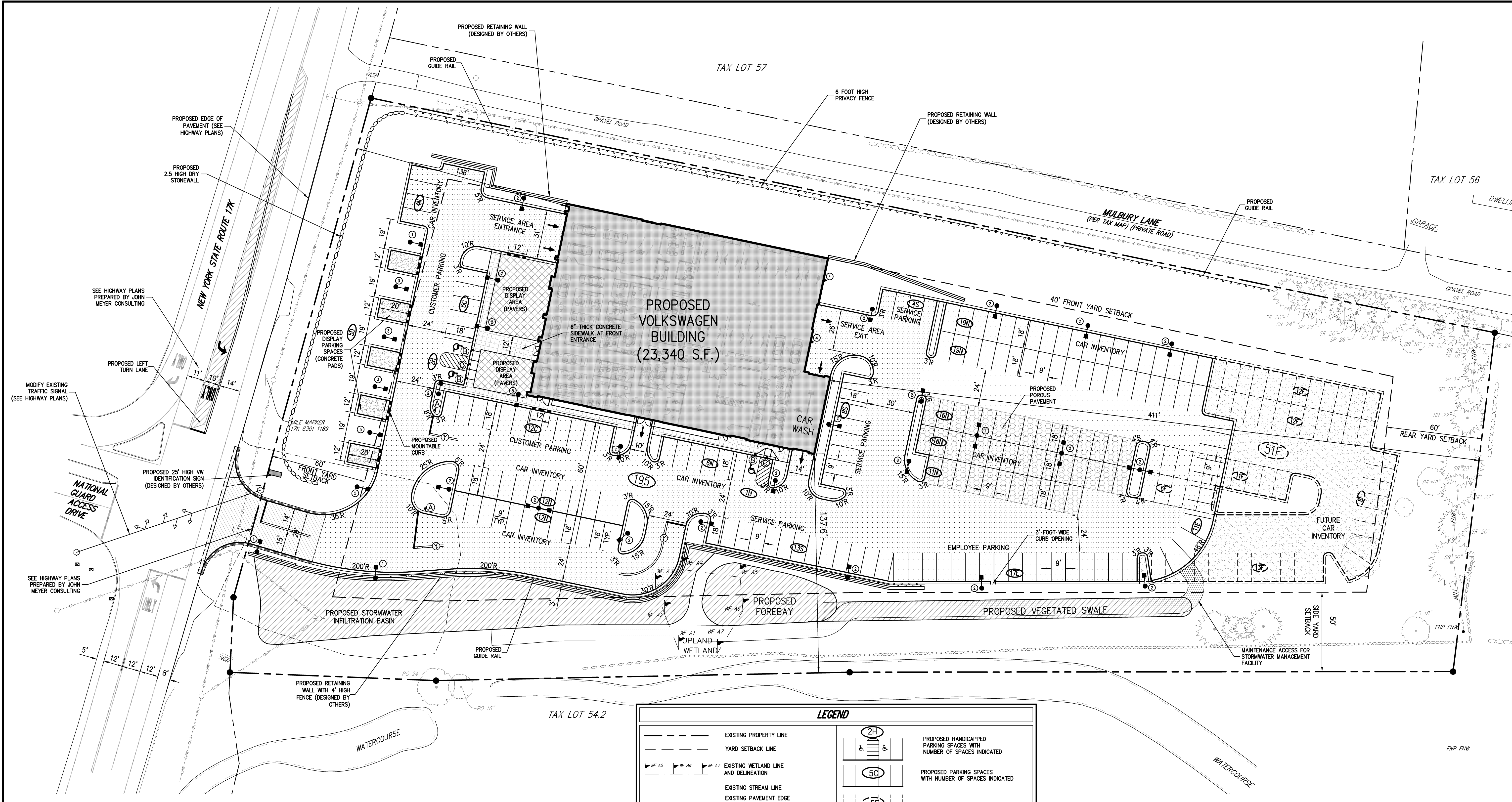
**LEGEND**

<p>--- EXISTING PROPERTY LINE</p> <p>--- ADJACENT PROPERTY LINE</p> <p>--- EXISTING WETLAND LINE AND DELINEATION</p> <p>--- EXISTING BUILDING LINE</p> <p>--- EXISTING PAVEMENT EDGE</p> <p>--- EXISTING CONTOUR</p> <p>--- EXISTING INDEX CONTOUR</p> <p>--- EXISTING STONE WALL</p> <p>--- EXISTING RETAINING WALL</p> <p>--- EXISTING FENCE</p> <p>--- EXISTING TREE AND DESIGNATION</p> <p>--- EXISTING PAINT</p> <p>--- EXISTING STORM DRAIN LINE AND SIZE</p> <p>--- EXISTING SANITARY LINE AND SIZE</p>	<p>--- EXISTING WATER LINE</p> <p>--- EXISTING GAS LINE</p> <p>--- EXISTING OVERHEAD WIRES</p> <p>--- EXISTING TELEPHONE WIRE</p> <p>--- EXISTING FIRE HYDRANT</p> <p>--- EXISTING GAS VALVE</p> <p>--- EXISTING WATER VALVE</p> <p>--- EXISTING DRAIN INLET</p> <p>--- EXISTING MANHOLE</p> <p>--- EXISTING UTILITY POLE</p> <p>--- EXISTING FEATURE TO BE REMOVED</p> <p>--- EXISTING TREE TO BE REMOVED</p> <p>--- EXISTING BUILDING TO BE REMOVED</p> <p>--- EXISTING PAVEMENT TO BE REMOVED</p> <p>--- INFILTRATION TEST LOCATION AND DESIGNATION</p> <p>--- TEST PIT LOCATION AND DESIGNATION</p>
--	---

NO.	REVISION	DATE	BY	JE	RR
1.	REVISED	03/21/2014	JE		

SCALE: 1" = 30'  
DATE: 11/07/2014  
PROJECT NO: 13021  
DWC: TAE DEMO SOP: EX. CON.  
DRAWING NO: SP-2

Previous Editions Obsolete



**SIGN TABLE**

DESCRIPTION	SYMBOL	SIZE	DESCRIPTION	MOVING TYPE	MARKING HEIGHT	REGULATORY	RECOMMENDED
A		30"x30"	WHITE ON RED	STEEL CHANNEL	7'-0"	R1-1	X
B		12"x18"	GREEN & BLUE ON WHITE	STEEL CHANNEL	7'-0"	R7-8	X
C		12"x18"	RED ON WHITE	STEEL CHANNEL	7'-0"	NYPI-2	X

**LEGEND**

	EXISTING PROPERTY LINE		PROPOSED HANDICAPPED PARKING SPACES WITH NUMBER OF SPACES INDICATED
	YARD SETBACK LINE		PROPOSED PARKING SPACES WITH NUMBER OF SPACES INDICATED
	EXISTING WETLAND LINE AND DELINEATION		PROPOSED FUTURE PARKING SPACES
	EXISTING STREAM LINE		PROPOSED CUSTOMER PARKING SPACES
	EXISTING PAVEMENT EDGE		PROPOSED SERVICE PARKING SPACES
	EXISTING CURB LINE		PROPOSED INVENTORY PARKING SPACES
	EXISTING STONE WALL		PROPOSED EMPLOYEE PARKING SPACES
	EXISTING RETAINING WALL		PROPOSED DISPLAY PARKING SPACES
	EXISTING FENCE		PROPOSED FUTURE PARKING SPACES
	EXISTING TREE AND DESIGNATION		PROPOSED CONCRETE SIDEWALK
	EXISTING PAINT		PROPOSED DROP CURB AND RAMP
	EXISTING UTILITY POLE		PROPOSED HEAVY DUTY PAVEMENT
	PROPOSED BUILDING LINE		PROPOSED LIGHT DUTY PAVEMENT
	PROPOSED CONCRETE CURB		PROPOSED NYS DOT FULL DEPTH PAVEMENT
	PROPOSED FUTURE CONCRETE CURB		PROPOSED POROUS PAVEMENT
	PROPOSED NYS DOT TYPE V150 CONCRETE CURB (TYPICAL)		PROPOSED FUTURE POROUS PAVEMENT
	PROPOSED CONCRETE CURB		PROPOSED DISPLAY AREA
	6 FOOT HIGH PRIVACY FENCE		PROPOSED CONCRETE PAD
	PROPOSED RETAINING WALL WITH 4' HIGH FENCE (DESIGN BY OTHERS)		PROPOSED FUTURE PAVEMENT
	PROPOSED DRY STONEWALL (DESIGN BY OTHERS)		
	PROPOSED GUIDE RAIL		
	PROPOSED SIGN AND DESIGNATION		

**NOTE:**  
 1. EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM SURVEY TITLED, "TOPOGRAPHIC SURVEY," PREPARED BY JOHN MEYER CONSULTING, DATED 03/05/2013.

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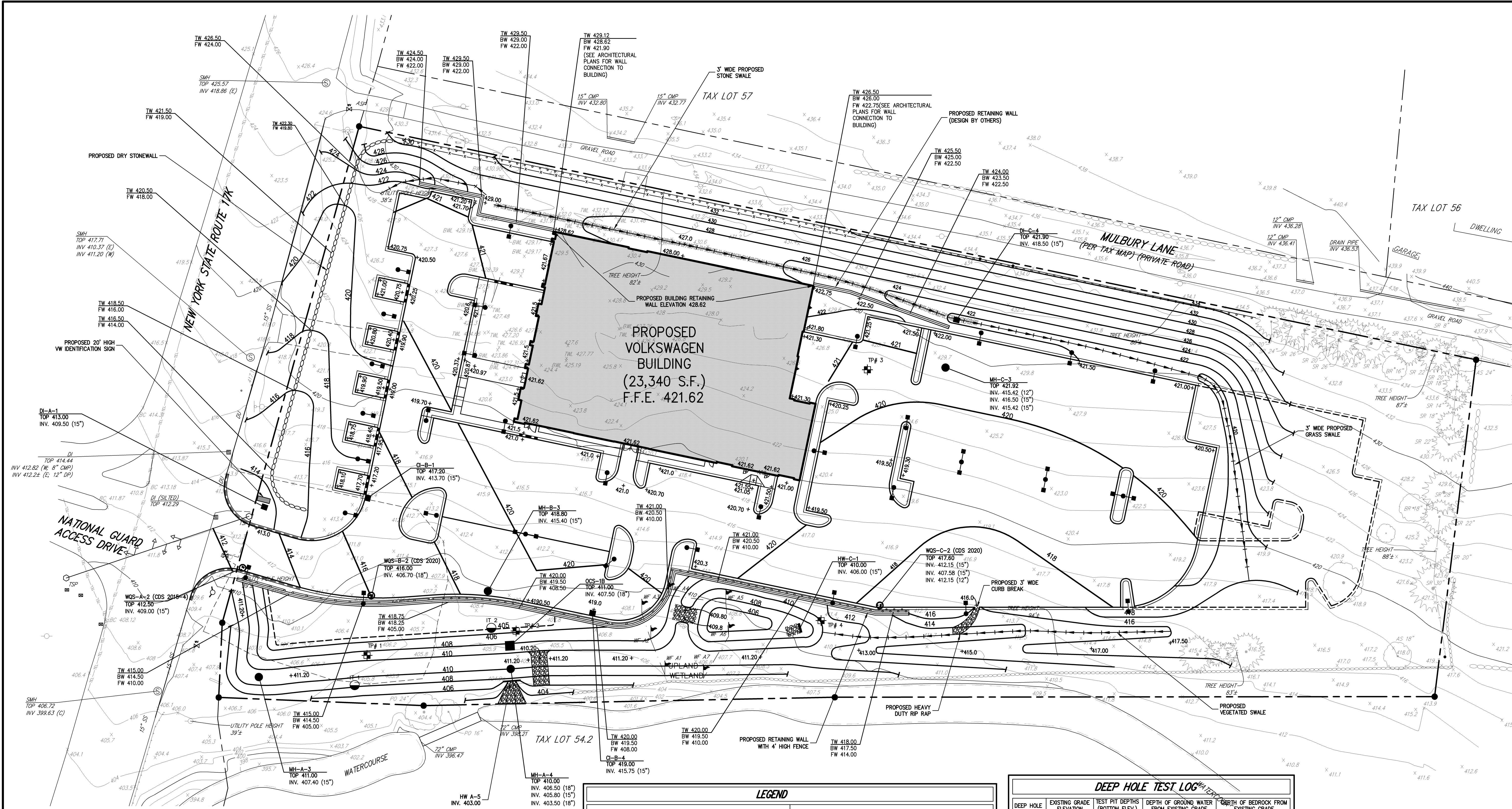
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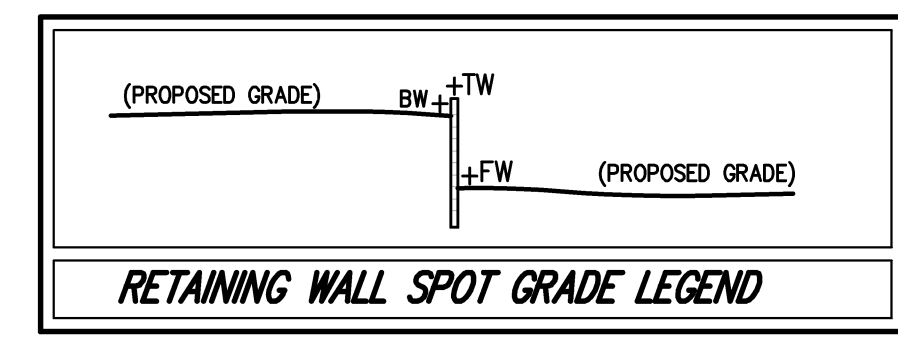
**LAYOUT PLAN**  
**VOLKSWAGEN OF NEWBURGH**  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK

**SP-3**



- NOTES:**
- EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM SURVEY ENTITLED, "TOPOGRAPHIC SURVEY," PREPARED BY JOHN MEYER CONSULTING, DATED 03/15/2013.
  - DESIGN FOR THE RETAINING WALL SHOWN HEREON SHALL BE PREPARED BY A NYS LICENSED PROFESSIONAL ENGINEER AND SUBMITTED TO THE TOWN BUILDING INSPECTOR FOR RECORD PRIOR TO CONSTRUCTION. SUCH DESIGN DRAWINGS (OR SHOP DRAWINGS) SHALL BEAR THE STAMP AND SIGNATURE OF SUCH ENGINEER, AND SHALL BE SPECIFIC FOR THE SITE AND SPECIFIC TO THE RETAINING WALL SYSTEM TO BE UTILIZED AND SHALL CONSIDER ALL APPROPRIATE AND NECESSARY POSSIBLE LOADINGS AND CONDITIONS RELATED TO THIS PROJECT.
  - THE AFOREMENTIONED DESIGN AND DETAILS SHALL CONSIDER/IDENTIFY/INCLUDE, BUT SHALL NOT BE LIMITED TO: SIGNED AND SEALED DESIGN CALCULATIONS; COMPLETE AND SPECIFIC CONSTRUCTION PLANS AND DETAILS FOR EACH WALL; APPROPRIATE SIZING FOR DRAINAGE SYSTEM TO HANDLE INTENSE STORM CONDITIONS; MAINTENANCE ABILITY TO CLEAN STORMWATER PIPING SYSTEMS; APPROPRIATE BACKFILL MATERIAL SUFFICIENT POROSITY TO ALLOW FREE DRAINAGE OF WATER; EVALUATE POSSIBLE FAILURE BY INTERNAL/EXTERNAL FAILURE MECHANISMS, GLOBAL FAILURE OR OTHER POTENTIAL FAILURES; AND SEISMIC DESIGN CONSIDERATIONS.
  - IF THE WALL OR WALLS ARE TIERED WALLS, THE DESIGN SHALL INCLUDE AN ANALYSIS OF THE MINIMUM SPACING OF WALLS TO ALLOW THE INDIVIDUAL WALLS TO ACT AS INDIVIDUAL WALLS BASED ON THE SPECIFIC SITE AND CONSTRUCTION CONDITIONS. IF THE WALLS ARE TO BE PLACED CLOSER THAN THE SAME, THE SPECIFIC DESIGN SHALL CONSIDER THE LOADS SUPERIMPOSED BY ONE WALL TO THE OTHER.

- DURING CONSTRUCTION, THE WORK MUST BE INSPECTED BY A NYS LICENSED PROFESSIONAL ENGINEER WHO SHALL PROVIDE WRITTEN VERIFICATION TO THE TOWN BUILDING INSPECTOR, PRIOR TO THE REQUEST FOR A CERTIFICATE OF OCCUPANCY, THAT HE/SHE HAS PERSONALLY INSPECTED THE WORK, AND THE INSTALLATION IS IN COMPLIANCE WITH THE DESIGN DRAWINGS AND MANUFACTURER'S INSTALLATION RECOMMENDATIONS.
- IF DEEMED NECESSARY BY THE DESIGN ENGINEER AND/OR THE TOWN, THIRD PARTY TESTING WILL BE PERFORMED REGARDING MATERIAL COMPACTION, FILL QUALITY, ETC. A COPY OF ALL SUCH TESTING RECORDS SHALL BE PROVIDED TO THE TOWN BUILDING DEPARTMENT FOR RECORD.



**LEGEND**

	EXISTING PROPERTY LINE		PROPOSED DRY STONEWALL (DESIGN BY OTHERS)
	ADJACENT PROPERTY LINE		PROPOSED GUIDEWALL
	EXISTING WETLAND LINE AND DELINEATION		PROPOSED FINISHED GRADE
	EXISTING PAVEMENT EDGE		PROPOSED SPOT GRADE
	EXISTING CURB LINE		PROPOSED DITCH OR SWALE
	EXISTING CONTOUR		PROPOSED STORM DRAIN MANHOLE
	EXISTING INDEX CONTOUR		PROPOSED TYPE CI DRAIN INLET
	EXISTING STONE WALL		PROPOSED TYPE DI DRAIN INLET
	EXISTING RETAINING WALL		PROPOSED WATER QUALITY STRUCTURE
	EXISTING RETAINING WALL		PROPOSED SUBSURFACE DRAINAGE OUTLET CONTROL STRUCTURE
	EXISTING FENCE		PROPOSED CLEANOUT
	EXISTING DRAIN INLET		PROPOSED HYDRANT
	EXISTING MANHOLE		PROPOSED DOUBLE ARM LIGHTING STANDARD
	EXISTING UTILITY POLE		PROPOSED SINGLE ARM LIGHTING STANDARD
	PROPOSED BUILDING LINE		PROPOSED WALL PACK LIGHTING STANDARD
	PROPOSED CONCRETE SIDEWALK		PROPOSED LIMIT OF DISTURBANCE
	PROPOSED DROP CURB AND RAMP		PROPOSED HEAVY DUTY RIP RAP
	PROPOSED RETAINING WALL WITH 6' HIGH PRIVACY FENCE (DESIGN BY OTHERS)		INFILTRATION TEST LOCATION AND DESIGNATION
	PROPOSED RETAINING WALL WITH 4' HIGH FENCE (DESIGN BY OTHERS)		

**DEEP HOLE TEST LOG**

DEEP HOLE	EXISTING GRADE ELEVATION	TEST PIT DEPTHS (BOTTOM ELEV.)	DEPTH OF GROUND WATER FROM EXISTING GRADE	DEPTH OF BEDROCK FROM EXISTING GRADE
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- NOTE:**
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  - INFILTRATION TESTING WAS PERFORMED BY JOHN MEYER CONSULTING, PC ON OCTOBER 17, 2013.

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1.	REVISED	03/21/2014	JE		

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**SITE GRADING PLAN**

**VOLKSWAGEN OF NEWBURGH**  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK

SCALE: 1" = 30'

DATE: 11/07/2013

PROJECT NO: 13021

DWG: 13021-SITE T&E GRADING

SOC: GRADING

DRAWING NO: **SP-4**

Previous Editions Obsolete

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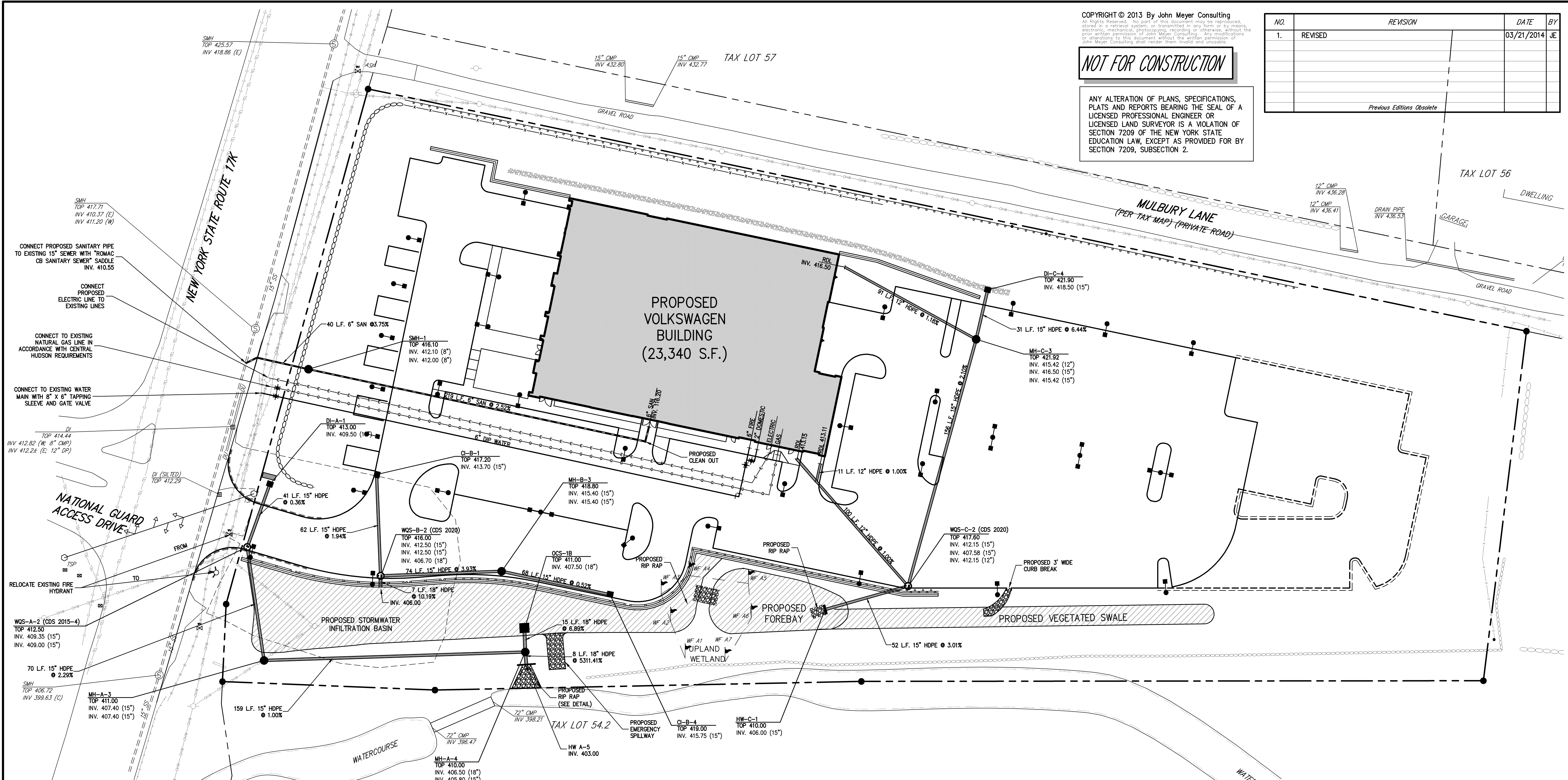
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**JOHN MEYER CONSULTING, P.C.**

**SITE UTILITIES PLAN**

**VOLKSWAGEN OF NEWBURGH**  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

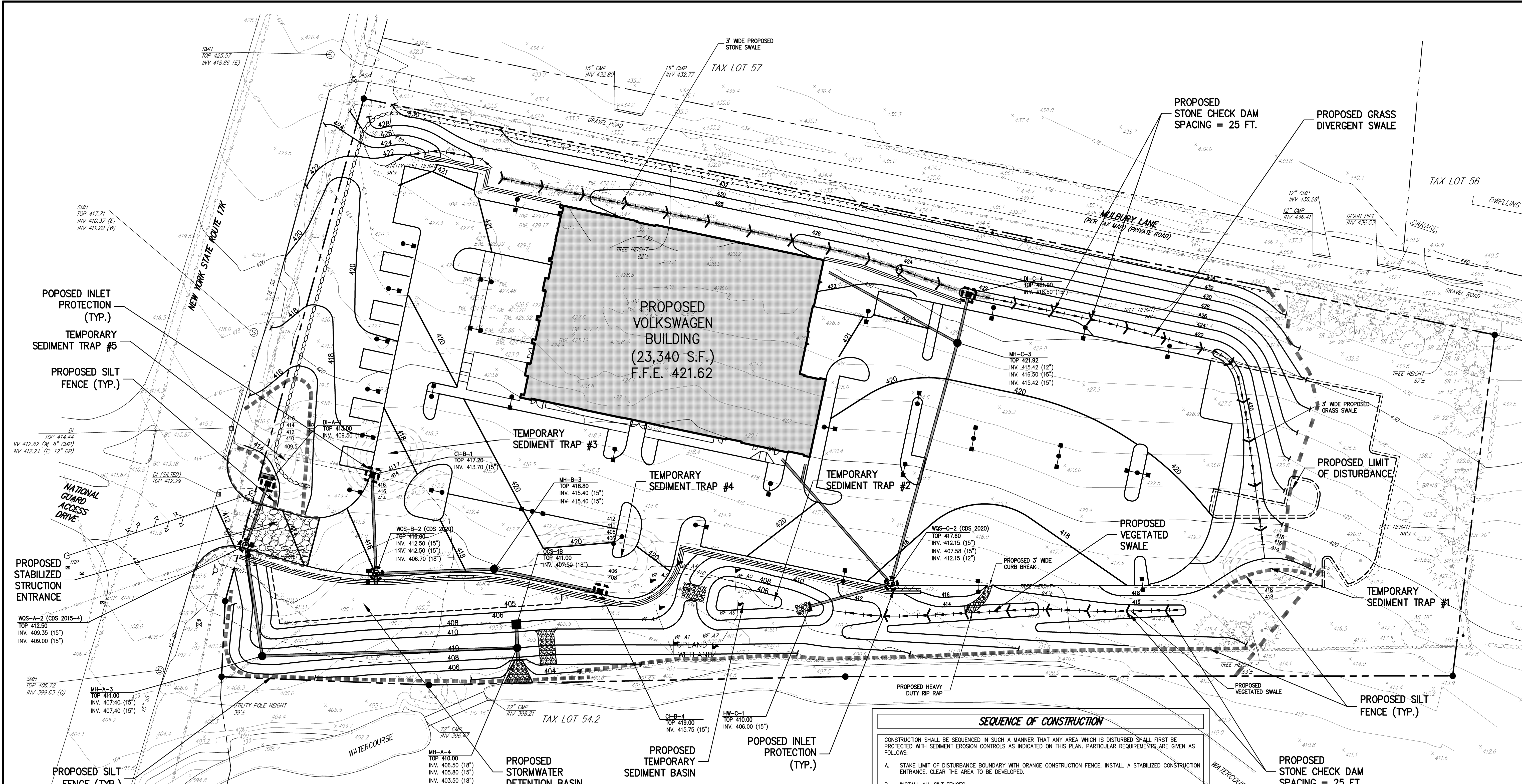
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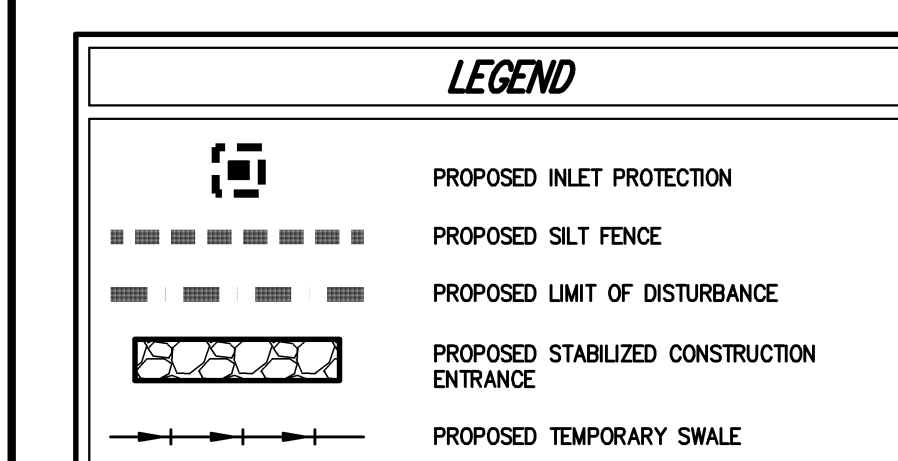
- NOTES:**
- EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM SURVEY ENTITLED, "TOPOGRAPHIC SURVEY," PREPARED BY JOHN MEYER CONSULTING, DATED 03/15/2013.
  - UNLESS OTHERWISE SPECIFIED, PIPE FOR STORM DRAINS SHALL BE HIGH DENSITY POLYETHYLENE PIPE (HDPE) WITH A SMOOTH INTERIOR AND ANNULAR EXTERIOR CORRUGATIONS IN ACCORDANCE WITH ASTM D-3212.
  - ELECTRIC, TELEPHONE, FIRE ALARM AND CABLE TELEVISION LINES SHALL BE INSTALLED UNDERGROUND IN CONDUIT IN ACCORDANCE WITH THE REQUIREMENTS OF THE UTILITY COMPANY HAVING JURISDICTION.
  - CONSTRUCTION OF SANITARY SEWER FACILITIES AND CONNECTION TO THE TOWN OF NEWBURGH SANITARY SEWER SYSTEM REQUIRES A PERMIT FROM THE TOWN OF NEWBURGH SEWER DEPARTMENT. ALL CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF NYSDEC AND THE TOWN OF NEWBURGH.
  - ALL SEWER PIPE INSTALLATION SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF NEWBURGH SEWER DEPARTMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL INSPECTIONS AS REQUIRED WITH THE TOWN OF NEWBURGH SEWER DEPARTMENT.
  - ALL GRAVITY SANITARY SEWER SERVICE LINES SHALL BE 4 INCHES IN DIAMETER OR LARGER AND SHALL BE SDR-35 PVC PIPE CONFORMING TO ASTM D-3034-89. JOINTS SHALL BE PUSH-ON WITH ELASTOMERIC RING GASKET CONFORMING TO ASTM D-3212. FITTINGS SHALL BE AS MANUFACTURED BY THE PIPE SUPPLIER OR EQUAL AND SHALL HAVE A BELL AND SPIGOT CONFIGURATION COMPATIBLE WITH THE PIPE.
  - THE SEWER MAIN SHALL BE TESTED IN ACCORDANCE WITH THE TOWN OF NEWBURGH REQUIREMENTS. ALL TESTING SHALL BE COORDINATED WITH THE TOWN OF NEWBURGH SEWER DEPARTMENT.
  - CONSTRUCTION OF POTABLE WATER UTILITIES AND CONNECTION TO THE TOWN OF NEWBURGH WATER SYSTEM REQUIRES A PERMIT FROM THE TOWN OF NEWBURGH WATER DEPARTMENT. ALL REQUIREMENTS SHALL CONFORM TO THE REQUIREMENTS OF THE NEW YORK STATE DEPARTMENT OF HEALTH AND THE TOWN OF NEWBURGH.
  - ALL WATER SERVICE LINES 4 INCHES AND LARGER IN DIAMETER SHALL BE CEMENT LINED, CLASS 52, DUCTILE IRON PIPE CONFORMING TO ANS/AWWA C151/A21.51 FOR DUCTILE IRON PIPE JOINTS SHALL BE EITHER PUSH-ON OR MECHANICAL JOINT AS REQUIRED.
  - THRUST RESTRAINT OF THE PIPE SHALL BE THROUGH THE USE OF JOINT RESTRAINT. THRUST BLOCKS ARE NOT ACCEPTABLE. JOINT RESTRAINT SHALL BE THROUGH THE USE OF MECHANICAL JOINT PIPE WITH RETAINER GLANDS. ALL FITTINGS AND VALVES SHALL ALSO BE INSTALLED WITH RETAINER GLANDS FOR JOINT RESTRAINT. RETAINER GLANDS SHALL BE EBBA IRON MEGALUG SERIES 1100 OR APPROVAL EQUAL. THE USE OF A MANUFACTURED RESTRAINED JOINT PIPE IS ACCEPTABLE WITH PRIOR APPROVAL OF THE TOWN OF NEWBURGH WATER DEPARTMENT.
  - ALL FITTINGS SHALL BE CAST IRON OR DUCTILE IRON, MECHANICAL JOINT, CLASS 250 AND CONFORM TO ANS/AWWA C110/A21.10 FOR DUCTILE AND GRAY IRON FITTINGS OR ANS/AWA C153/A21.53 FOR DUCTILE IRON COMPACT FITTINGS.
  - ALL VALVES SHALL BE RESILIENT WEDGE, MECHANICAL JOINT GATE VALVES CONFORMING TO ANS/AWWA C509 SUCH AS MUELLER A-2360-23 OR APPROVAL EQUAL. ALL GATE VALVES SHALL OPEN LEFT (COUNTER CLOCK WISE).
  - TAPPING SLEEVE SHALL BE MECHANICAL JOINT SUCH AS MUELLER H-615 OR EQUAL. TAPPING SLEEVE SHALL BE RESILIENT WEDGE GATE VALVES CONFORMING TO ANS/AWWA C509 SUCH AS MUELLER T-2360-19 OR APPROVAL EQUAL. ALL TAPPING SLEEVES AND VALVES SHALL BE TESTED TO 150 PSI MINIMUM. TESTING OF THE TAPPING SLEEVE AND VALVE MUST BE WITNESSED AND ACCEPTED BY THE TOWN OF NEWBURGH WATER DEPARTMENT PRIOR TO CUTTING INTO THE PIPE.
  - ALL SERVICE LINES 2 INCHES IN DIAMETER AND SMALLER SHALL BE TYPE K COPPER TUBING. CORPORATION STOPS SHALL BE MUELLER H-15000 FOR 3/4 AND 1 INCH, MUELLER H-15000 OR B-25000 FOR 1 1/2 OR 2 INCH SIZES. CURB VALVES SHALL BE MUELLER H-1501-2 FOR 3/4 AND 1 INCH AND MUELLER B-25024 FOR 1 1/2 AND 2 INCH SIZES. CURB BOXES SHALL BE MUELLER H-10312 FOR 3/4 AND 1 INCH AND MUELLER H-10310 FOR 1 1/2 AND 2 INCH SIZES.
  - ALL PIPE INSTALLATION SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF NEWBURGH WATER DEPARTMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL INSPECTIONS AS REQUIRED WITH THE TOWN OF NEWBURGH WATER DEPARTMENT.
  - THE WATER MAIN SHALL BE TESTED, DISINFECTED AND FLUSHED IN ACCORDANCE WITH THE TOWN OF NEWBURGH REQUIREMENTS. ALL TESTING, DISINFECTION AND FLUSHING SHALL BE COORDINATED WITH THE TOWN OF NEWBURGH WATER DEPARTMENT. PRIOR TO PUTTING THE WATER MAIN IN SERVICE, SATISFACTORY SANITARY RESULTS FROM A CERTIFIED LAB MUST BE SUBMITTED TO THE TOWN OF NEWBURGH WATER DEPARTMENT. THE TEST SAMPLES MUST BE COLLECTED BY A REPRESENTATIVE OF THE TESTING LABORATORY AND WITNESSED BY THE WATER DEPARTMENT.
  - THRUST RESTRAINT SHALL BE PROVIDED BY THE RODS AND RETAINER GLANDS. THE LENGTH OF RESTRAINED PIPE SHALL BE DETERMINED BASED UPON WORKING PRESSURES, SOIL CONDITIONS AND DEPTH OF BURY ACCORDING TO DIPPA STANDARDS.
  - PRESSURE AND LEAKAGE TESTS ARE REQUIRED AND SHALL BE DONE IN ACCORDANCE WITH AWWA C600 STANDARDS.
  - DISINFECTION OF ALL NEW WORK SHALL BE DONE IN ACCORDANCE WITH AWWA C651 STANDARDS.
  - ALL WATER MAINS SHALL BE 4", CLASS 52, DUCTILE IRON PIPE UNLESS OTHERWISE NOTED.
  - ALL SECTIONS OF WATER MAINS PIPES WITH LESS THAN 4.5' COVER SHALL BE PERMA-PIPE DUAL-GUARD PREINSULATED PIPE WITH OUTER CONCRETE ENCASEMENT. THE PVC INSULATING JACKET SIZE SHALL BE 14" WITH A 2.55" POLYURETHANE INSULATION THICKNESS. PIPE JOINTS SHALL BE FIELD INSULATED.
  - THIS PROJECT HAS INDICATED THE INTENT TO PROVIDE ADEQUATE FIRE FLOW BY THE PROPOSED INSTALLATION OF SPRINKLER SYSTEMS MEETING NFPA REQUIREMENTS, AND IS, THEREFORE, EXEMPT FROM THE NEEDED FIRE FLOW GUIDELINES OF THE INSURANCE SERVICES OFFICE (ISO). THE PROPOSED SPRINKLER SYSTEM DESIGN HAS NOT BEEN EVALUATED BY THE ORANGE COUNTY DEPARTMENT OF HEALTH FOR COMPLIANCE WITH NFPA REQUIREMENTS.
  - BACKFLOW PREVENTION DEVICES FOR BOTH THE 2" DOMESTICS AND 4" FIRE SERVICES TO BE APPROVED BY OGDON AND ARE DESIGNED BY OTHERS UNDER A SEPARATE APPLICATION.
  - THE CONTRACTOR SHALL PROVIDE ALL NECESSARY EQUIPMENT AND SHALL PERFORM ALL WORK REQUIRED IN CONNECTION WITH ALL THE TESTS AS SPECIFIED HEREIN. ALL PIPE SHALL BE TESTED BY HYDROSTATIC PRESSURE, FIFTY (50) PERCENT IN EXCESS OF THE NORMAL PSI WORKING PRESSURE BUT NOT LESS THAN 150 PSI OR MORE THAN THE DESIGN RATING OF THE PIPE APPURTENANCES, IN ACCORDANCE WITH AWWA SPECIFICATION C-600. THE TEST SHALL BE DETERMINED BY THE WATER AUTHORITY.
  - IF THE SECTION BEING TESTED SHALL FAIL TO PASS THE PRESSURE TEST OR THE LEAKAGE TEST, OR BOTH, THE CONTRACTOR SHALL DO EVERYTHING NECESSARY TO LOCATE, UNCOVER, AND REPAIR OR REPLACE THE DEFECTIVE PIPE, FITTINGS OR JOINTS, AND ALL SUCH WORK SHALL BE DONE AT HIS EXPENSE AND AT NO ADDITIONAL COST TO THE OWNER.
  - A LEAKAGE TEST SHALL BE CONDUCTED IN ACCORDANCE WITH AWWA SPECIFICATION C-600. THERE SHALL BE NO LEAKAGE DURING THE TEST.
  - IF THE SECTION BEING TESTED SHALL FAIL TO PASS THE PRESSURE TEST OR THE LEAKAGE TEST, OR BOTH, THE CONTRACTOR SHALL DO EVERYTHING NECESSARY TO LOCATE, UNCOVER, AND REPAIR OR REPLACE THE DEFECTIVE PIPE, FITTINGS OR JOINTS, AND ALL SUCH WORK SHALL BE DONE AT HIS EXPENSE AND AT NO ADDITIONAL COST TO THE OWNER.
  - IN THE EVENT OF CONFLICT BETWEEN THE TESTS SPECIFIED HEREIN AND THE TEST REQUIREMENTS OF THE TOWN OF NEWBURGH WATER DISTRICT, HEALTH DEPARTMENT OR ANY OTHER AUTHORITY HAVING JURISDICTION OVER ALL OR ANY PORTION OF THE WATER LINES INSTALLED UNDER THIS CONTRACT, THE MORE RESTRICTIVE REQUIREMENTS SHALL GOVERN.
  - AFTER THE WATER LINE HAS PASSED THE REQUIRED PRESSURE AND LEAKAGE TESTS AND BEFORE BEING PLACED INTO SERVICE, THE ENTIRE LINE SHALL BE DISINFECTED. ALL DISINFECTING METHODS AND MATERIALS SHALL BE IN ACCORDANCE WITH AWWA SPECIFICATION C-601. ALL DISINFECTION OPERATIONS AND PROCEDURES SHALL MEET WITH THE APPROVAL OF THE WATER AUTHORITY AND HEALTH DEPARTMENT.
  - IF THE INITIAL BACTERIOLOGICAL TESTS ARE NOT SATISFACTORY, THE CONTRACTOR SHALL DO EVERYTHING NECESSARY TO OBTAIN SATISFACTORY BACTERIOLOGICAL TESTS, INCLUDING MAKING PROVISIONS TO ISOLATE SHORTER SECTIONS OF LINE TO LOCATE THE SOURCE OF CONTAMINATION. ALL WORK NECESSARY AND REQUIRED TO OBTAIN SATISFACTORY BACTERIOLOGICAL TESTS SHALL BE AT THE CONTRACTORS EXPENSE AND AT NO ADDITIONAL COST TO THE OWNER.
  - THE DESIGN, CONSTRUCTION AND INSTALLATION OF THE WATER MAINS SHALL BE IN ACCORDANCE WITH THIS PLAN AND GENERALLY ACCEPTED STANDARDS IN EFFECT AT THE TIME OF CONSTRUCTION WHICH INCLUDE:
    - "RECOMMENDED STANDARDS FOR WATER WORKS (TEN STATES)"
    - "RURAL WATER SUPPLY, NEW YORK STATE DEPARTMENT OF HEALTH"
    - "NEW YORK STATE DEPARTMENT OF HEALTH AND ORANGE COUNTY DEPARTMENT OF HEALTH POLICES, PROCEDURES, AND STANDARDS"
  - ON COMPLETION OF THE FACILITIES THE FINISHED WORKS SHALL BE INSPECTED, TESTED, AND CERTIFIED COMPLETE BY THE PROFESSIONAL ENGINEER SUPERVISING CONSTRUCTION. NO PART OF THE FACILITIES SHALL BE PLACED INTO SERVICE UNTIL ACCEPTED BY THE PROFESSIONAL ENGINEER.
  - ALL WATER DISTRIBUTION SYSTEM PIPES AND APPURTENANCES SHALL CONFORM TO CURRENT TOWN OF NEWBURGH STANDARDS.
  - BACKFLOW PREVENTION DEVICE WILL BE LOCATED IN THE BUILDING. APPLICATION FOR APPROVAL SHALL BE SUBMITTED UNDER SEPARATE COVER BY THE MECHANICAL ENGINEER/ARCHITECT.
  - UNDER INDUSTRIAL CODE 753, THE CONTRACTOR SHALL BE REQUIRED TO NOTIFY ALL OPERATORS OF UTILITIES LISTED ON THE CURRENT MASTER LIST OF OPERATORS' ON FILE WITH THE CENTRAL REGISTRY AS WELL AS THE TOWN OF NEWBURGH AND NYSDOT PRIOR TO THE START OF THIS WORK SO THAT ALL THE VARIOUS UNDERGROUND UTILITY OPERATORS WILL BE ABLE TO LOCATE AND MARK THE LOCATIONS OF THEIR OWN UTILITIES. NO WORK SHALL COMMENCE UNTIL ALL THE OPERATORS HAVE NOTIFIED THE CONTRACTOR THAT THEIR UTILITIES HAVE BEEN LOCATED.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF ALL PUBLIC AND PRIVATE UNDERGROUND AND SURFACE UTILITIES AND STRUCTURES AT OR ADJACENT TO THE SITE OF CONSTRUCTION, INsofar AS THEY MAY BE ENDANGERED BY HIS OPERATIONS. THIS SHALL HOLD TRUE WHETHER OR NOT THEY ARE SHOWN ON THE CONTRACT DRAWINGS. IF THEY ARE SHOWN ON THE DRAWINGS, THEIR LOCATIONS ARE NOT GUARANTEED EVEN THOUGH THE INFORMATION WAS OBTAINED FROM THE BEST AVAILABLE SOURCES. THE CONTRACTOR SHALL, AT HIS OR HER OWN EXPENSE, REPAIR OR REPLACE ANY STRUCTURES OR UTILITIES THAT HE OR SHE DAMAGES, AND SHALL CONSTANTLY PROCEED WITH CAUTION TO PREVENT UNDESIRABLE INTERRUPTION TO UTILITY SERVICE.
  - ALL UTILITIES SHOWN HEREON TO BE CUT AND CAPPED SHALL BE DISCONNECTED IN ACCORDANCE WITH THE UTILITY COMPANY HAVING JURISDICTION.
  - ALL NEW UTILITY CONNECTIONS, DISCONNECTION AND RELOCATION OF EXISTING UTILITIES SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE UTILITY COMPANY HAVING JURISDICTION. ANY COORDINATION, PERMITS OR APPROVALS REQUIRED SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - IN ALL AREAS WHERE EARTH CUTS ARE PROPOSED OVER EXISTING UTILITIES TO REMAIN, THE CONTRACTOR SHALL HAND DIG TEST PITS TO LOCATE THE EXISTING UTILITIES AND CONTACT THE UTILITY COMPANY HAVING JURISDICTION FOR APPROVAL OF PROPOSED COVER.
  - IN ALL AREAS WHERE EARTH CUTS ARE PROPOSED OVER EXISTING UTILITIES TO REMAIN, THE CONTRACTOR SHALL HAND DIG TEST PITS TO LOCATE THE EXISTING UTILITIES AND CONTACT THE UTILITY COMPANY HAVING JURISDICTION FOR APPROVAL OF PROPOSED COVER.
  - CONTRACTOR SHALL REFER TO ARCHITECTURAL DRAWINGS FOR EXACT LOCATION OF UTILITY CONNECTION POINTS TO BUILDINGS AND COORDINATE WORK WITH BUILDING CONTRACTORS.
  - CONTRACTOR SHALL REFER TO DRAWING SP-2 "EXISTING CONDITIONS AND DEMOLITION PLAN" FOR ADDITIONAL INFORMATION REGARDING THE DISCONNECTION, REMOVAL AND/OR ABANDONMENT OF EXISTING UTILITIES.
  - ANY MODIFICATION TO TOWN OR NYSDOT OWNED AND MAINTAINED STRUCTURES SHALL BE INSTALLED WITH CORE DRILLING ONLY.
  - REFER TO DRAWING SP-2 "EXISTING CONDITIONS AND DEMOLITION PLAN" FOR ALL UTILITIES TO REMAIN AND TO BE REMOVED.
  - BACKFILL FOR PIPE OR CONDUIT SHALL BE PLACED EVENLY AND CAREFULLY AROUND AND OVER THE PIPE OR CONDUIT IN SIX (6) INCH MAXIMUM LAYERS. EACH LAYER SHALL BE THOROUGHLY AND CAREFULLY COMPACTED UNTIL TWELVE (12) INCHES OF COVER EXISTS OVER THE PIPE OR CONDUIT. THE REMAINDER OF THE BACKFILL AND BACKFILL OF EMPTY TRENCHES AFTER REMOVAL OF UTILITIES, FOOTINGS, ETC., MAY THEN BE PLACED AND COMPACTED IN A MAXIMUM OF TWELVE (12) INCH LAYERS. EACH LAYER SHALL BE COMPACTED BY APPROVED MECHANICAL TAMPING MACHINES, UNLESS OTHERWISE SPECIFIED BACKFILL SHALL BE COMPACTED TO NOT LESS THAN 95% MAXIMUM MOISTURE DENSITY IN PROPOSED BUILDING AND PAVED AREAS AND 92% MAXIMUM MOISTURE DENSITY IN NON-PAVED AREAS. IN ACCORDANCE WITH ASTM DESIGNATION D-1557 IN THE MANNER HEREIN DESCRIBED, BACKFILL SHALL PROCEED UP TO THE LINES AND GRADES AS SHOWN ON THE DRAWINGS. THE CONTRACTOR SHALL CERTIFY THAT ALL BACKFILL MEETS THE ABOVE REQUIREMENTS.

**LEGEND**

	EXISTING PROPERTY LINE		PROPOSED SANITARY SEWER MANHOLE
	ADJACENT PROPERTY LINE		PROPOSED STORM DRAIN MANHOLE
	EXISTING STREAM		PROPOSED TYPE CI DRAIN INLET
	EXISTING WETLAND LINE AND DELINEATION		PROPOSED TYPE DI DRAIN INLET
	EXISTING PAVEMENT EDGE		PROPOSED WATER QUALITY STRUCTURE
	EXISTING STONE WALL		PROPOSED CLEANOUT
	EXISTING RETAINING WALL		PROPOSED STORM DRAIN LINE & SIZE
	EXISTING FENCE		PROPOSED SANITARY SEWER LINE & SIZE
	EXISTING STORM DRAIN LINE AND SIZE		PROPOSED WATER LINE & SIZE
	EXISTING SANITARY LINE AND SIZE		PROPOSED GAS LINE
	EXISTING WATER LINE		PROPOSED ELECTRIC LINE
	EXISTING GAS LINE		EXISTING FIRE HYDRANT
	EXISTING OVERHEAD WIRES		PROPOSED GAS VALVE
	EXISTING TELEPHONE WIRE		PROPOSED RETAINING WALL (DESIGN BY OTHERS)
	EXISTING DRAIN INLET		EXISTING UTILITY POLE
	EXISTING MANHOLE		PROPOSED BUILDING LINE
	EXISTING FIRE HYDRANT		PROPOSED HEADWALL
	EXISTING GAS VALVE		
	PROPOSED RETAINING WALL (DESIGN BY OTHERS)		
	EXISTING UTILITY POLE		
	PROPOSED BUILDING LINE		
	PROPOSED HEADWALL		



- NOTES:**
- EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM SURVEY TITLED, "TOPOGRAPHIC SURVEY," PREPARED BY JOHN MEYER CONSULTING, DATED 03/15/2013.
  - ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH ALL THE PLANS, PRIOR TO BEGINNING ANY CLEARING, GRUBBING OR EXCAVATION.
  - EXPOSED SLOPES AND ALL GRADED AREAS SHALL BE SEEDDED WITH THE FOLLOWING GRASS MIX PER 1000 S.F. IN THE FOLLOWING PROPORTIONS:  
 CREEPING RED FESCUE 30 %  
 PERENNIAL RYE GRASS 70 %
  - GRASS SEED MIX FOR EROSION AND SEDIMENT CONTROL MAY BE APPLIED BY EITHER MECHANICAL OR HYDROSEEDING METHODS. HYDROSEEDING SHALL BE PERFORMED IN ACCORDANCE WITH THE AMERICAN ASSOCIATION OF NURSERYMEN, AMERICAN STANDARD FOR NURSERY STOCK, LATEST EDITION.
  - SEEDDED AREAS SHALL BE MULCHED WITH STRAW AT A RATE OF 2 TONS PER ACRE (90 LBS. PER 1,000 S.F.) SUCH THAT THE MULCH FORMS A CONTINUOUS BLANKET.
  - EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED AND MAINTAINED ON A DAILY BASIS BY THE CONTRACTOR. ALL COLLECTED SEDIMENT WITHIN SEDIMENT BARRIERS SHALL BE REMOVED PERIODICALLY TO MAINTAIN THE FUNCTION OF THE SEDIMENT BARRIER. ALL SEDIMENT COLLECTED SHALL BE RESPADE ON-SITE WITHIN STABILIZED AREAS AS DIRECTED BY THE OWNERS FIELD REPRESENTATIVE.
  - DUST SHALL BE CONTROLLED BY SPRINKLING OR OTHER APPROVED METHODS AS NECESSARY, OR AS DIRECTED BY THE TOWN ENGINEER.
  - CUT AND FILLS SHALL NOT ENDANGER ADJOINING PROPERTIES, NOR DIVERT WATER ONTO THE PROPERTY OF OTHERS.
  - ALL FILLS SHALL BE COMPACTED TO PROVIDE STABILITY OF MATERIAL AND TO PREVENT SETTLEMENT.
  - THE CONTRACTOR SHALL INSPECT DOWNSTREAM CONDITIONS FOR EVIDENCE OF SEDIMENTATION ON A WEEKLY BASIS AND AFTER RAINSTORMS.
  - AS WARRANTED BY FIELD CONDITIONS, SPECIAL ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED BY THE CONTRACTOR AS REQUIRED.



**Soil Restoration Requirements**

Type of Soil Disturbance	Soil Restoration Requirement	Comments/Examples
No soil disturbance	Restoration not permitted	Preservation of Natural Features
Minimal soil disturbance	Restoration not required	Clearing and grubbing
Areas where topsoil is stripped only - no change in grade	HSG A&B apply 6 inches of topsoil	Protect area from any ongoing construction activities
Areas of cut or fill	HSG A&B Apply full Soil Restoration**	Clearing and grubbing
Heavy traffic areas on site (especially) in a zone 5-25 feet around buildings but not within a 5 foot perimeter around foundation walls)	Apply full Soil Restoration (decompaction and compost enhancement)	
Areas where Runoff Reduction and/or Infiltration practices are applied	Restoration not required, but may be applied to enhance the reduction specified for appropriate practices.	Keep construction equipment from crossing these areas. To protect newly installed practice from any ongoing construction activities construct a single phase operation fence area.

\* Aeration includes the use of machines such as tractor-drawn implements with coulters making a narrow slit in the soil, a roller with many spikes making indentations in the soil, or prongs which function like a mini-subsoiler.  
 \*\* Per "Deep Ripping and De-compaction, DEC 2008."

- SEQUENCE OF CONSTRUCTION**
- CONSTRUCTION SHALL BE SEQUENCED IN SUCH A MANNER THAT ANY AREA WHICH IS DISTURBED SHALL FIRST BE PROTECTED WITH SEDIMENT EROSION CONTROLS AS INDICATED ON THIS PLAN. PARTICULAR REQUIREMENTS ARE GIVEN AS FOLLOWS:
- STAKE LIMIT OF DISTURBANCE BOUNDARY WITH ORANGE CONSTRUCTION FENCE. INSTALL A STABILIZED CONSTRUCTION ENTRANCE. CLEAR THE AREA TO BE DEVELOPED.
  - INSTALL ALL SILT FENCES.
  - GRUB THE AREA TO BE CONSTRUCTED.
  - INSTALL TEMPORARY SEDIMENT TRAPS #1 AND #2.
  - CONSTRUCT DIVERSION SWALES ON THE WESTERN SIDE OF THE PROPERTY TO DIVERT THE OFFSITE AREA DRAINING TOWARDS THE SITE TOWARDS THE SEDIMENT TRAP #1 AND EVENTUALLY INTO THE EXISTING WATERCOURSE LOCATED TO THE EAST OF THE PROPERTY WHILE MAINTAINING THE EXISTING DRAINAGE PATTERN.
  - PROVIDE STONE CHECK DAMS AT REGULAR INTERVALS IN THE DIVERSION SWALES.
  - INSTALL THE TEMPORARY SEDIMENT TRAPS #3 AND #4 AND CONSTRUCT DIVERSION SWALES TO DIVERT THE STORMWATER RUNOFF FROM THE WEST OF THE PROPERTY TOWARDS THE SEDIMENT TRAPS.
  - REMOVE THE TEMPORARY SEDIMENT TRAP #4 ONCE THE AREA DRAINING INTO THE TRAP IS STABILIZED.
  - INSTALL THE TEMPORARY SEDIMENT TRAPS #5 AND CONSTRUCT DIVERSION SWALE ALONG ROUTE 17K TO DIVERT THE STORMWATER RUNOFF FROM THE SOUTHWEST OF THE PROPERTY TOWARDS THE SEDIMENT TRAP.
  - REMOVE AND STOCKPILE TOPSOIL. INSTALL SILT FENCING AROUND THE TEMPORARY TOPSOIL STOCKPILE LOCATION FOR EROSION CONTROL PURPOSES.
  - PROCEED WITH ROUGH GRADING OF THE AREA UNDER ACTIVE CONSTRUCTION.
  - INITIAL STORMWATER INFILTRATION BASIN EXCAVATION SHOULD BE CARRIED TO WITHIN 2 FEET OF THE FINAL ELEVATION OF THE BASIN FLOOR. FINAL EXCAVATION TO THE FINISHED GRADE SHOULD BE DEFERRED UNTIL ALL DISTURBED AREAS HAVE BEEN STABILIZED.
  - INSTALL THE STORM DRAINAGE SYSTEM CONSISTING OF CATCH BASINS, MANHOLES AND UNDERGROUND STORM PIPES ALONG WITH THE EROSION AND SEDIMENT CONTROL DEVICES ASSOCIATED WITH THE STORM DRAINAGE SYSTEM (I.E. INLET PROTECTION, STONE CHECK DAMS, ETC., AS SHOWN ON THE PLANS).
  - INSTALL UTILITIES (SANITARY SEWER, WATER, GAS, ELECTRIC, TELEPHONE, ETC.), AS REQUIRED.
  - INSTALL GREEN INFRASTRUCTURE PRACTICE, VEGETATED SWALE.
  - BEGIN ROAD CONSTRUCTION INCLUDING SUBBASE AND BASE PAVEMENT SECTIONS FOR ASPHALT POROUS PAVEMENT AND THE ASPHALT PAVEMENT.
  - FINISH GRADING, REDISTRIBUTE TOPSOIL AND ESTABLISH VEGETATION AND/OR LANDSCAPING.
  - COMPLETE FINAL GRADING FOR THE STORMWATER INFILTRATION BASIN.
  - CLEAN PAVEMENTS AND STORM DRAIN SYSTEM OF ALL ACCUMULATED SEDIMENT IN CONJUNCTION WITH THE REMOVAL OF ALL TEMPORARY SEDIMENT AND EROSION CONTROL DEVICES.
  - COMPLETE BUILDING CONSTRUCTION.

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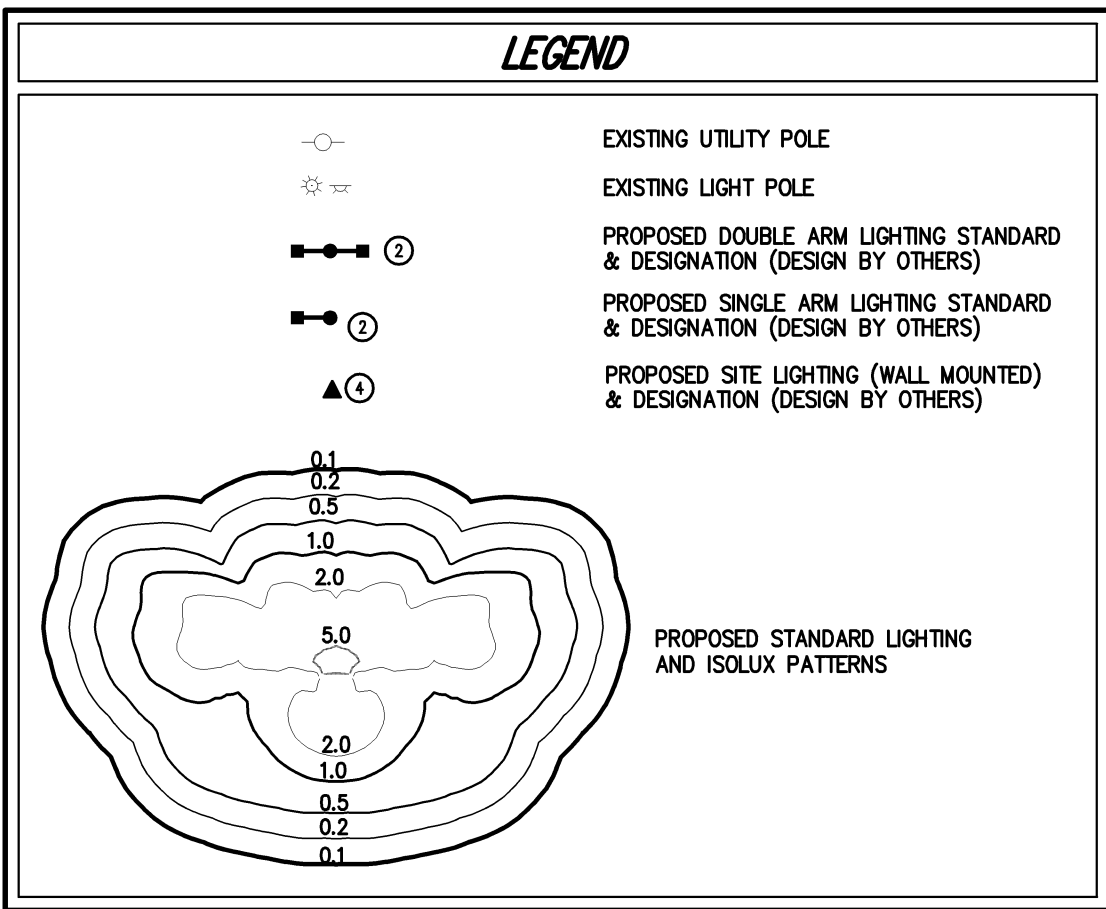
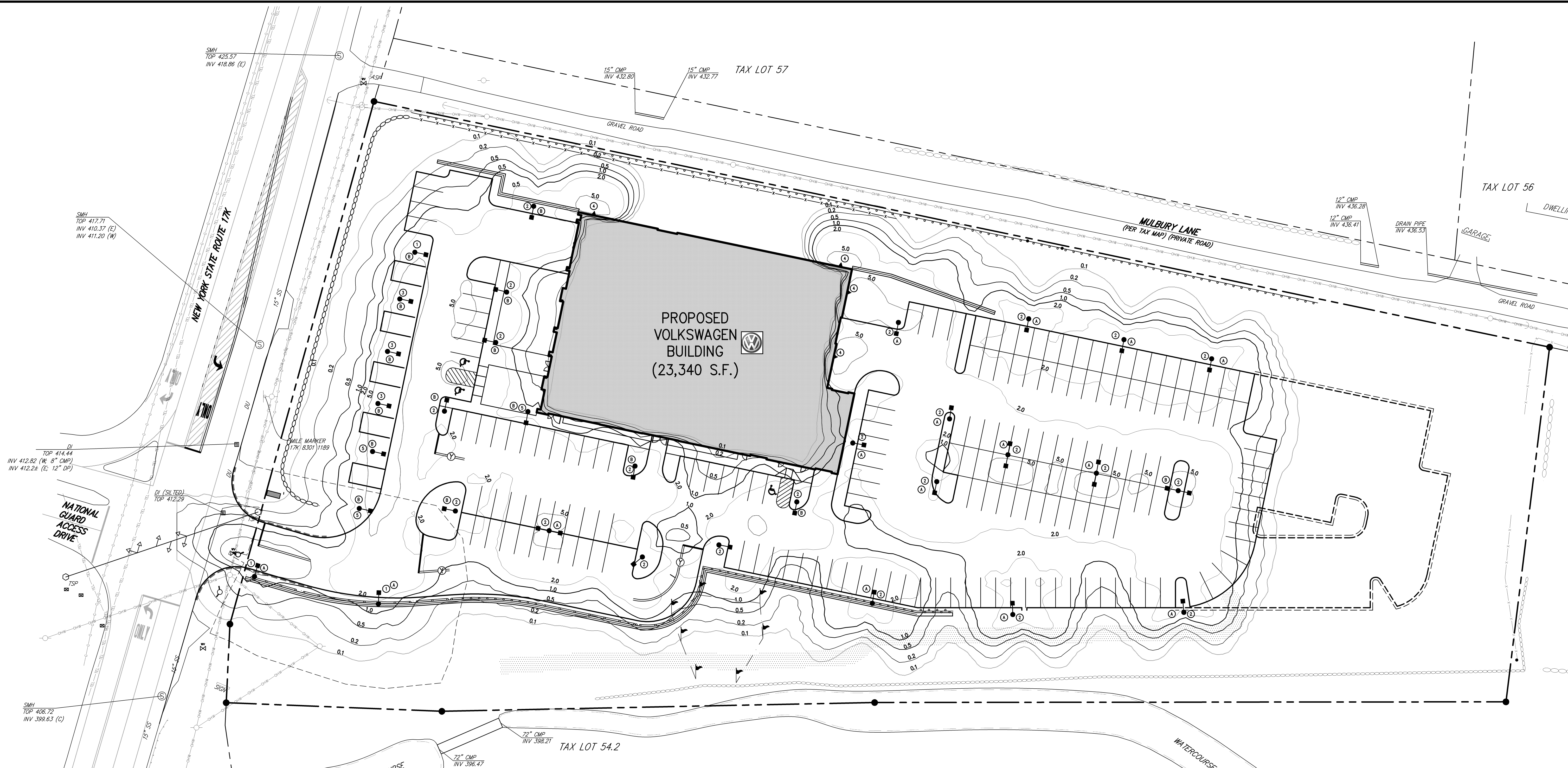
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**SITE SEDIMENT & EROSION CONTROL PLAN**  
**VOLKSWAGEN OF NEWBURGH**  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK

**SP-6**







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DRW.	TAB.	SOP.
1300-LIGHTING	LIGHTING	LIGHTING

**ROUTE 17 CARS, LLC**  
 1143 DUTCHESS TURNPIKE  
 POUGHKEEPSIE, NEW YORK 12603

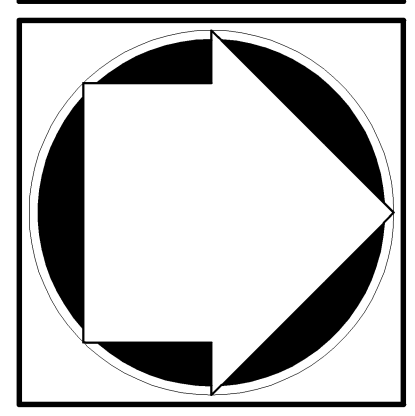
**CLARIS CONSTRUCTION INC.**  
 153 SOUTH MAIN STREET  
 NEWTOWN, CONNECTICUT 06470

120 Bedford Road  
 Armonk, NY 10504  
 voice 914.273.5225 • fax 914.273.1102  
 www.johmeyerconsulting.com

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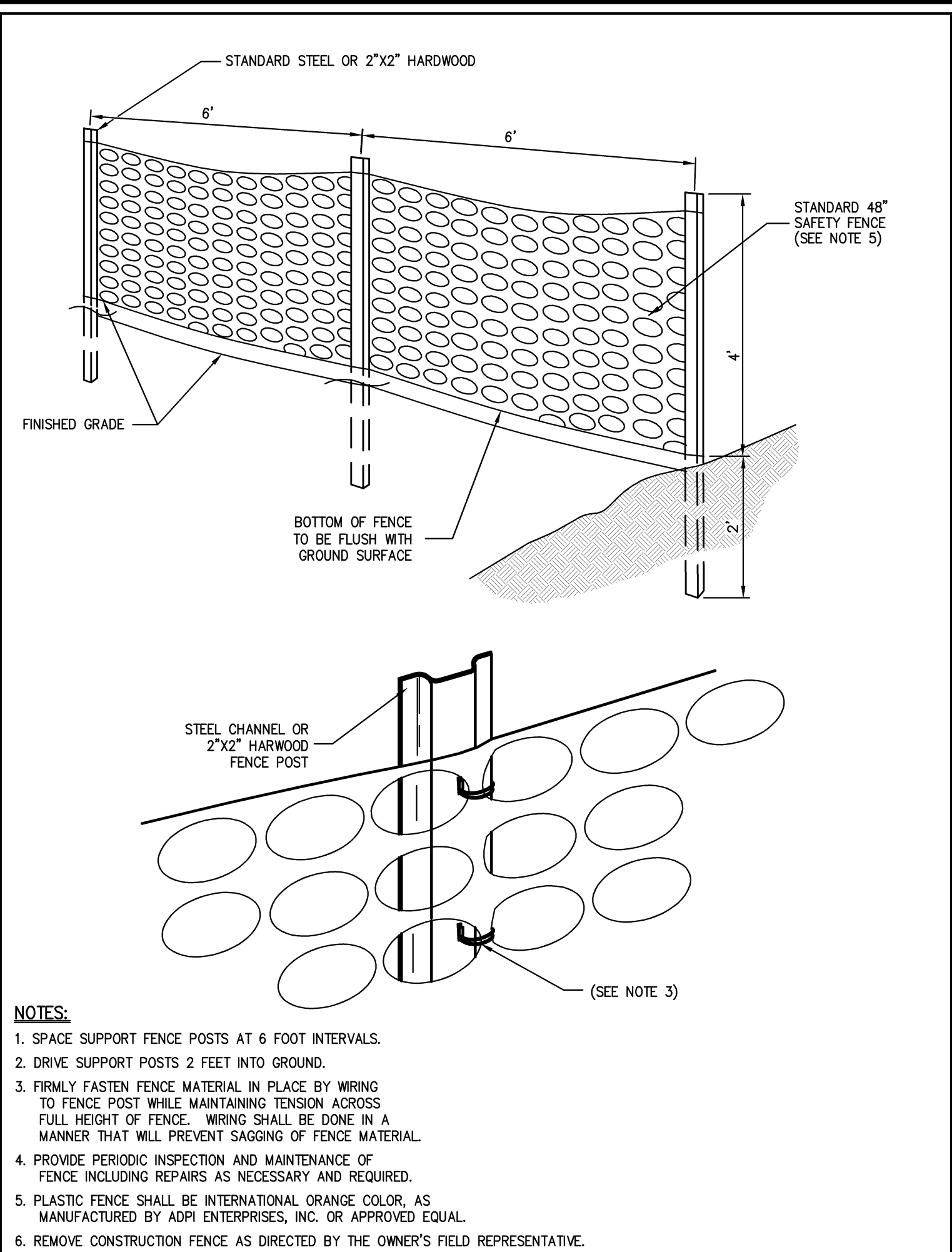
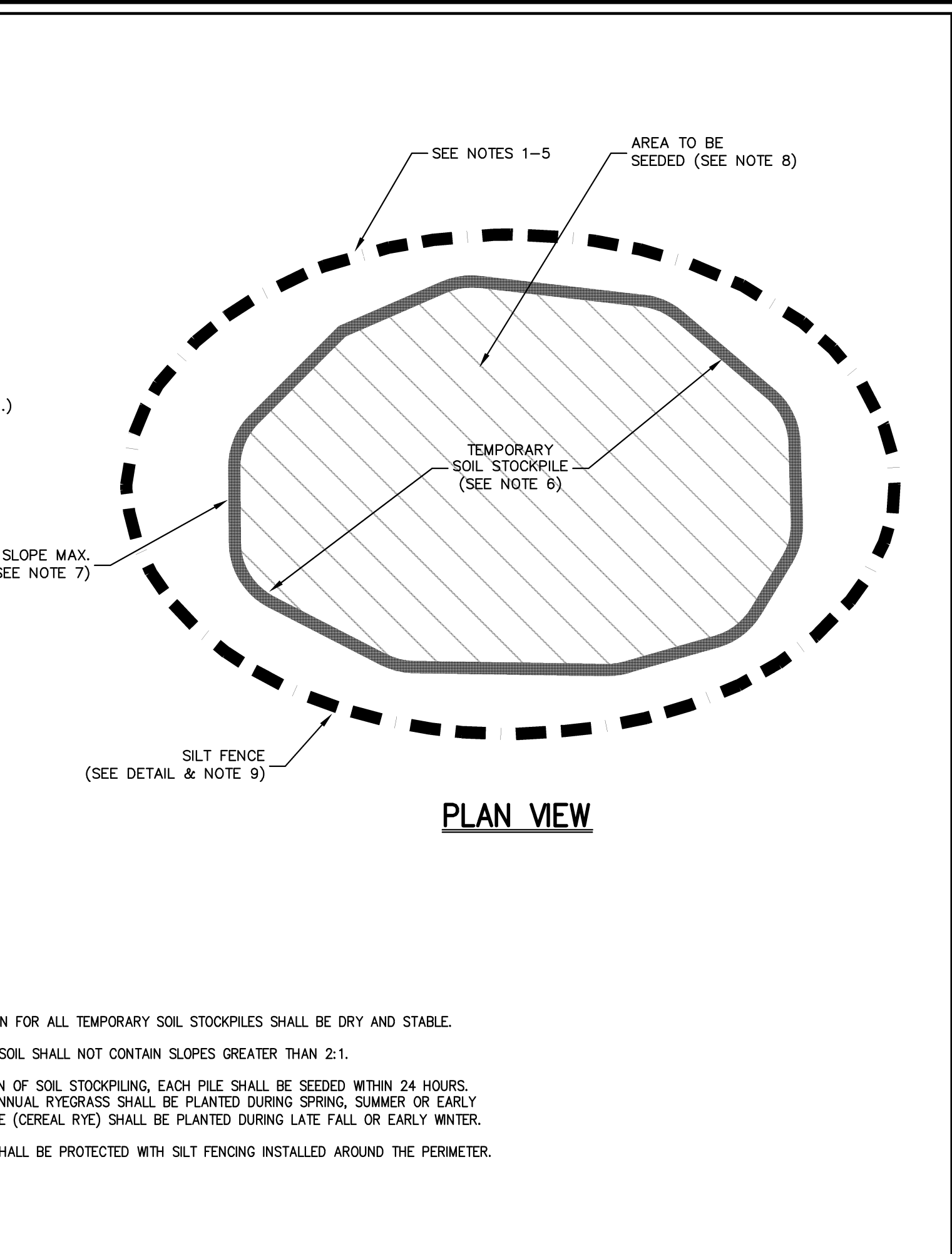
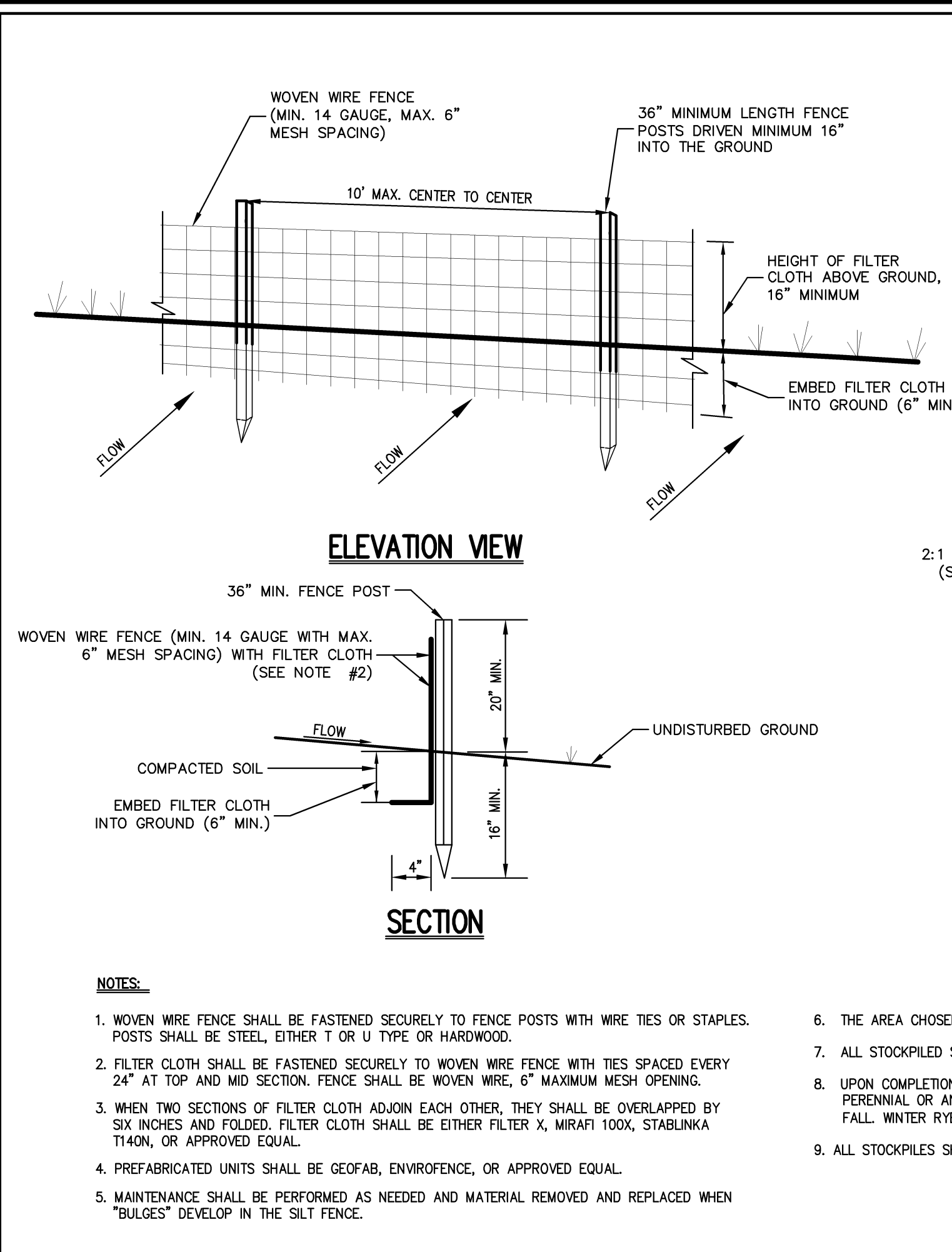
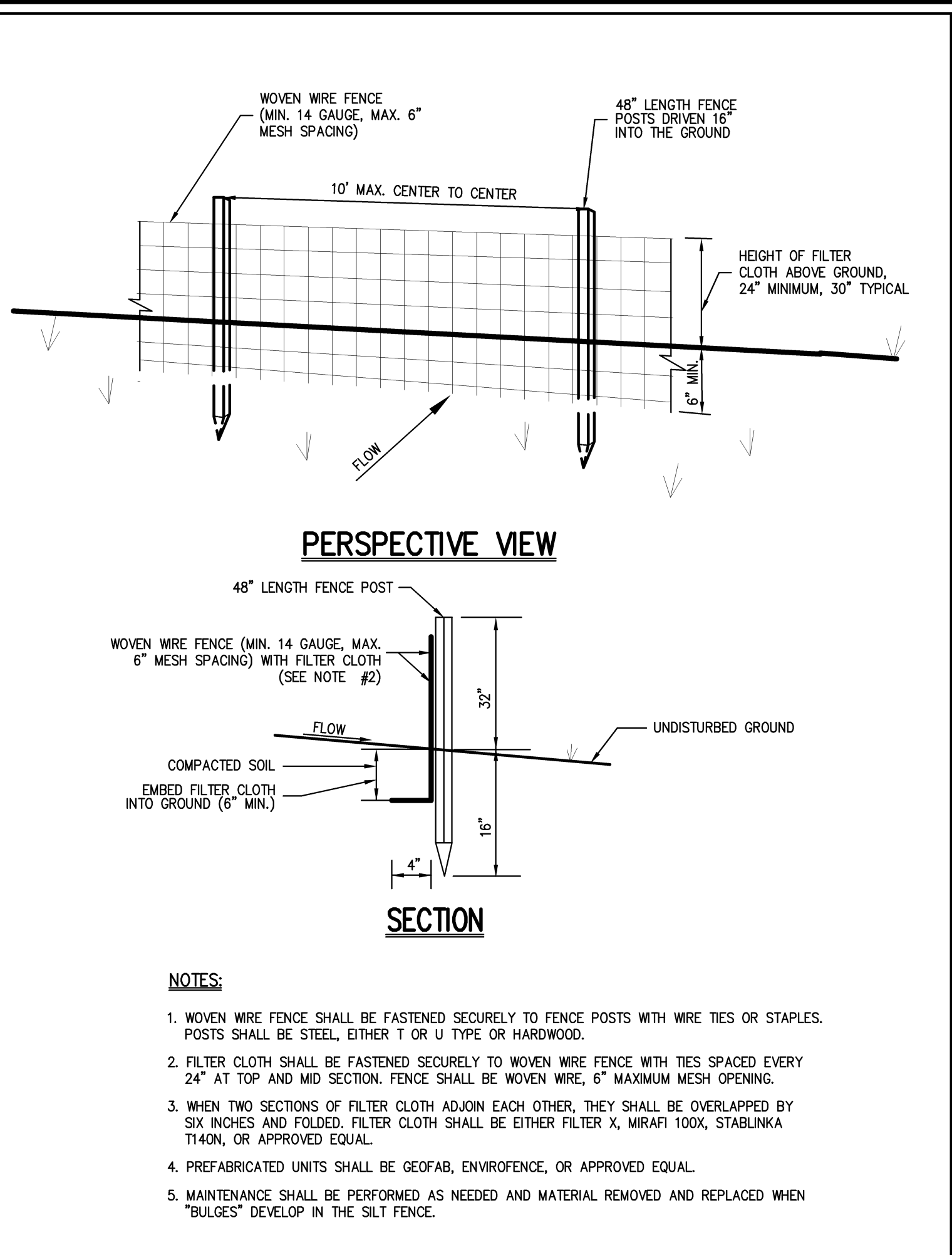
**SITE LIGHTING PLAN**

**VOLKSWAGEN OF NEWBURGH**  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK



DRW: JE APPROVED: RR  
 SCALE: 1"=30"  
 DATE: 11/07/2013  
 PROJECT NO: 13021  
 DRW: 1300-LIGHTING TAB: LIGHTING SOP: LIGHTING  
**SP-8**

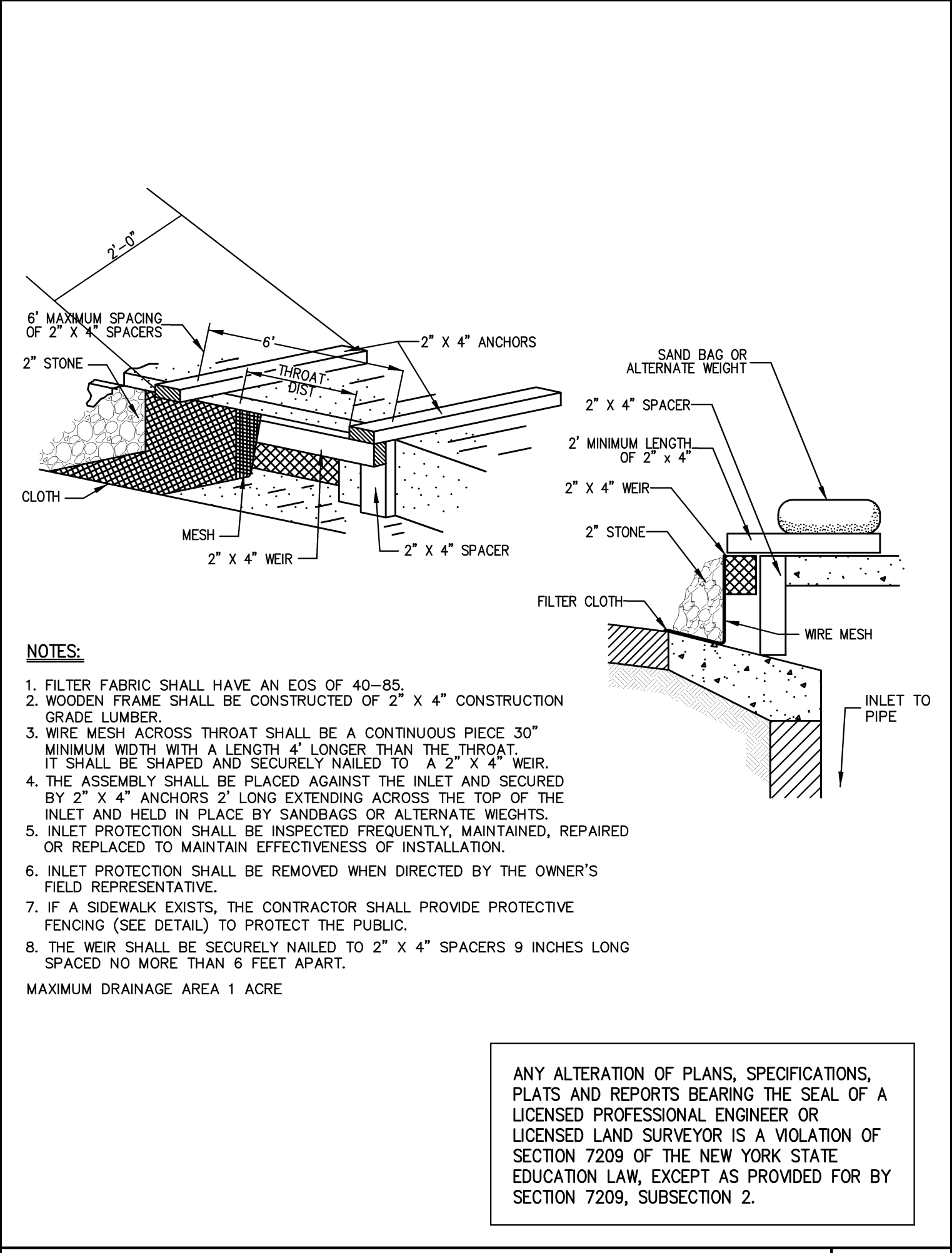
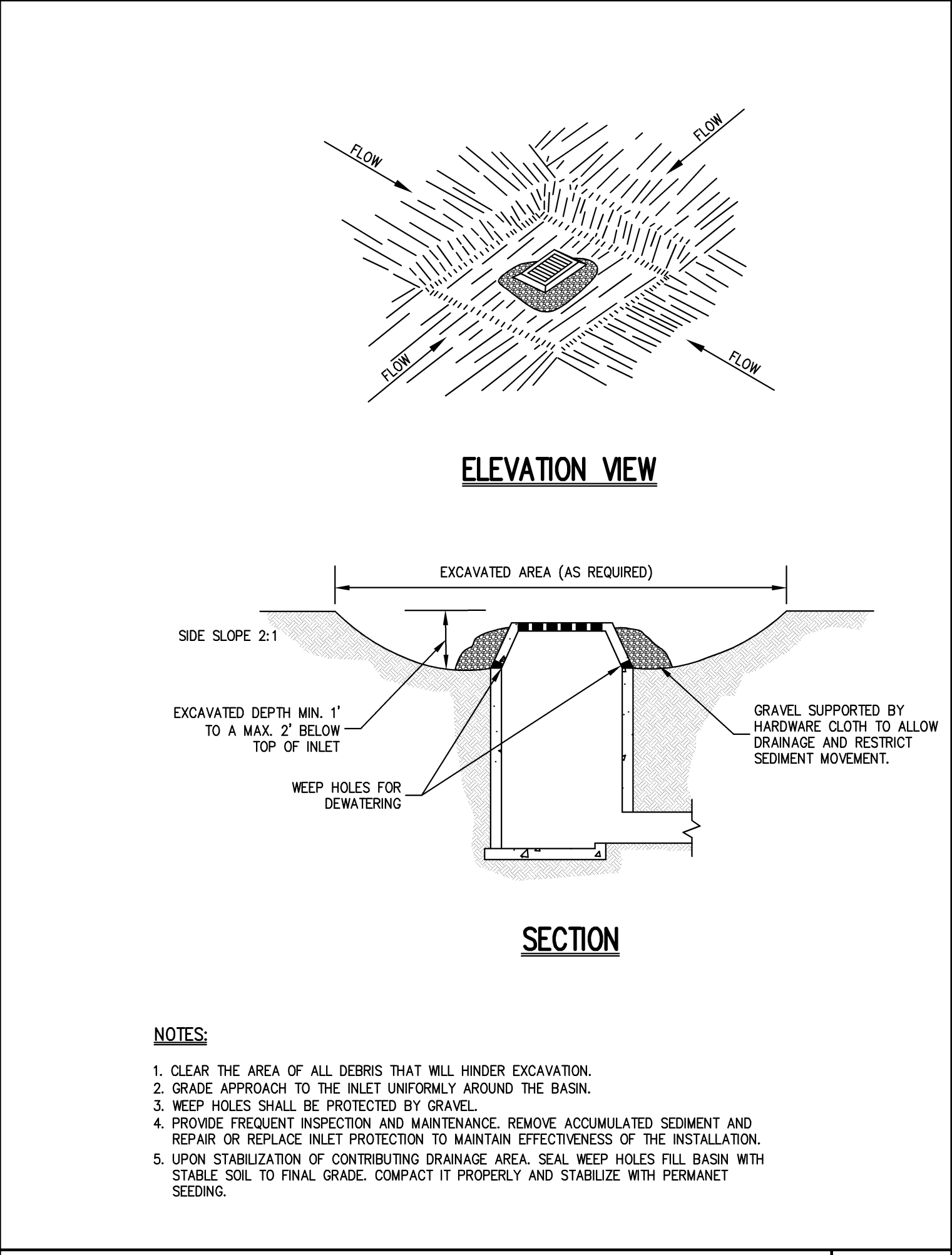
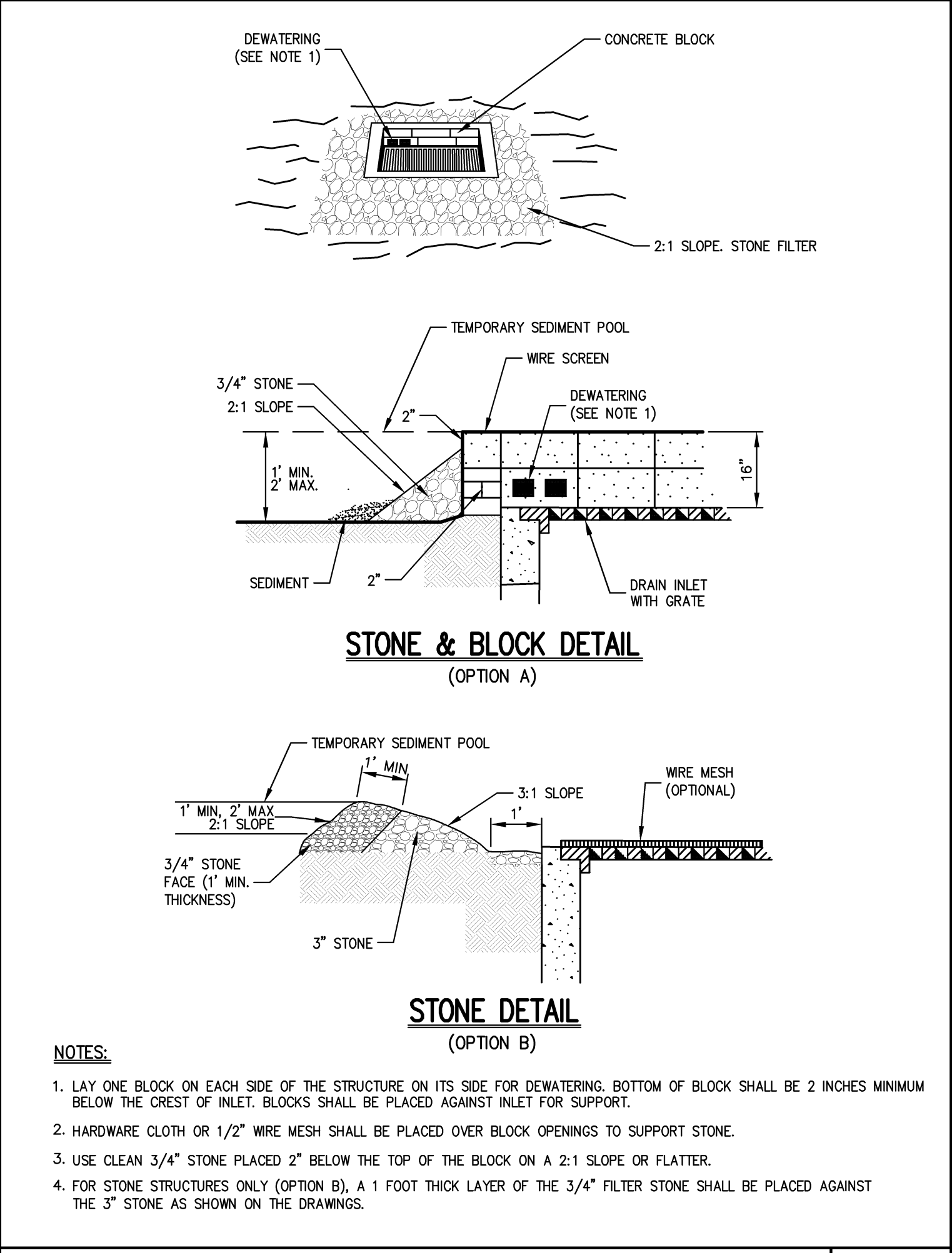
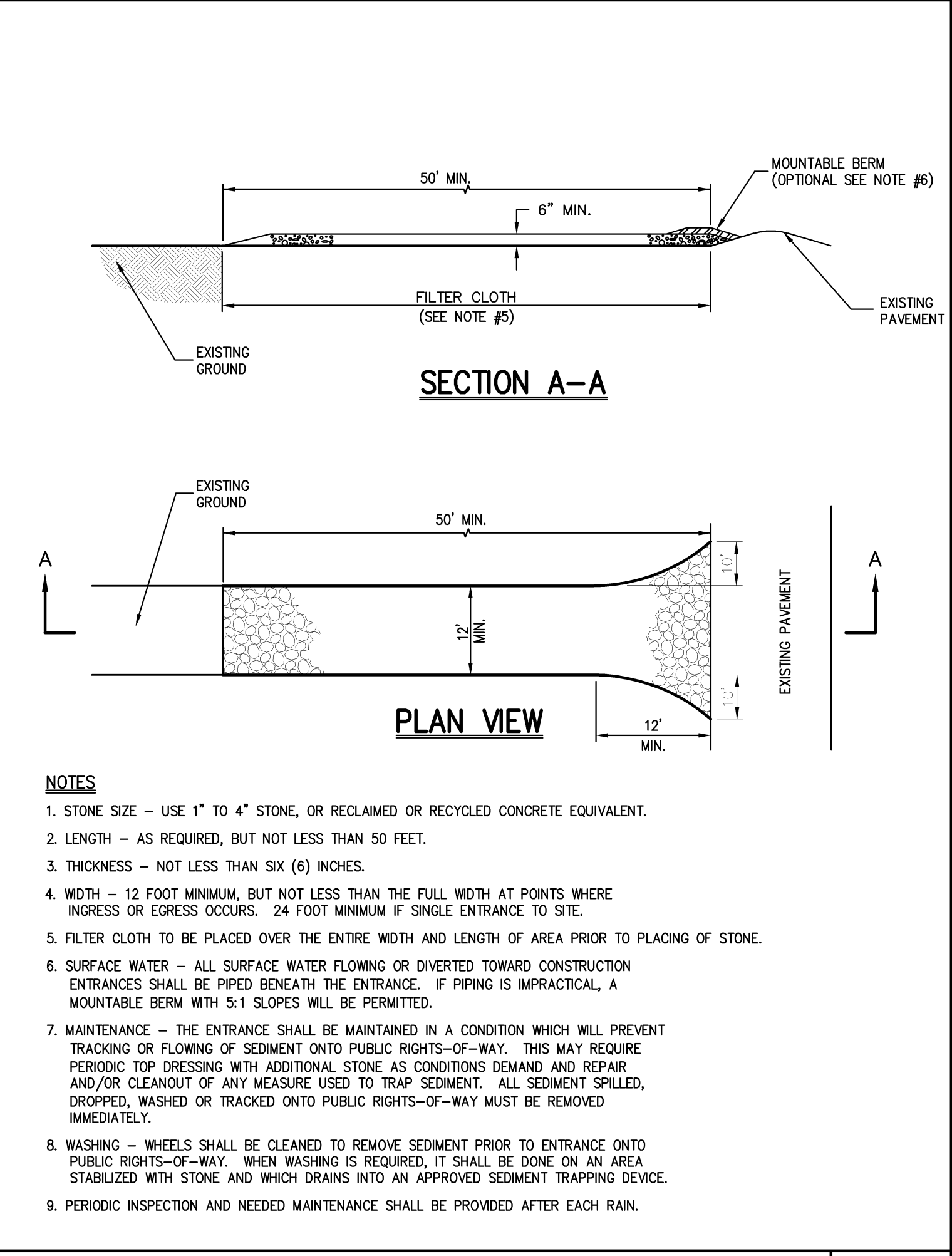
Previous Editions Obsolete



**SILT FENCE** **1**

**TEMPORARY SOIL STOCKPILE WITH SILT FENCE** **2**

**CONSTRUCTION FENCE** **3**



**STABILIZED CONSTRUCTION ENTRANCE** **4**

**STONE & BLOCK DRAIN INLET PROTECTION** **5**

**EXCAVATED DRAIN INLET PROTECTION** **6**

**CURB DROP INLET PROTECTION STRUCTURE** **7**

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NO.	1. REVISED

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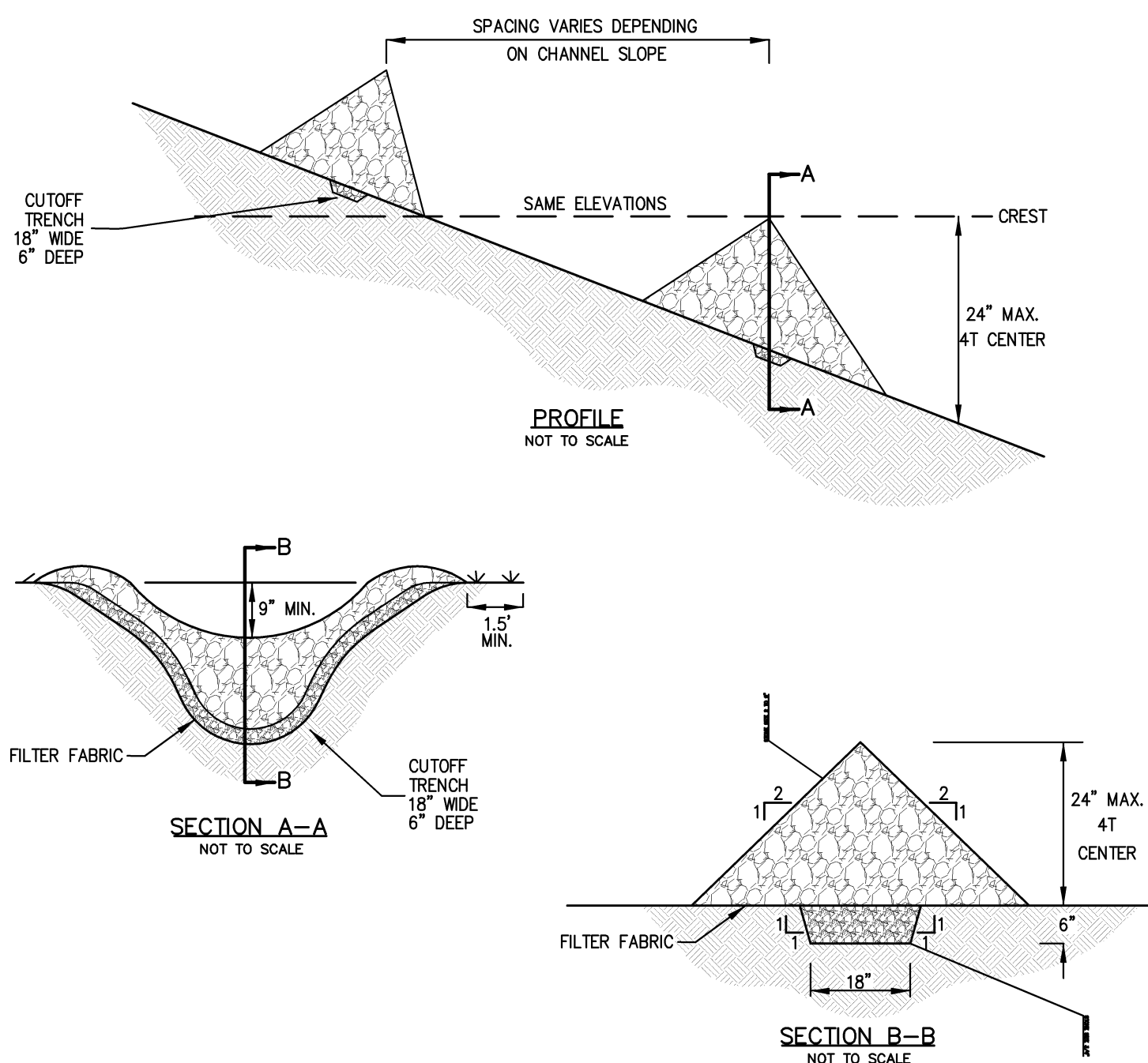
120 Bedford Road  
Armonk, NY 10504  
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www.johnmeyerconsulting.com

**CONSTRUCTION DETAILS**

**VOLKSWAGEN OF NEWBURGH**  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

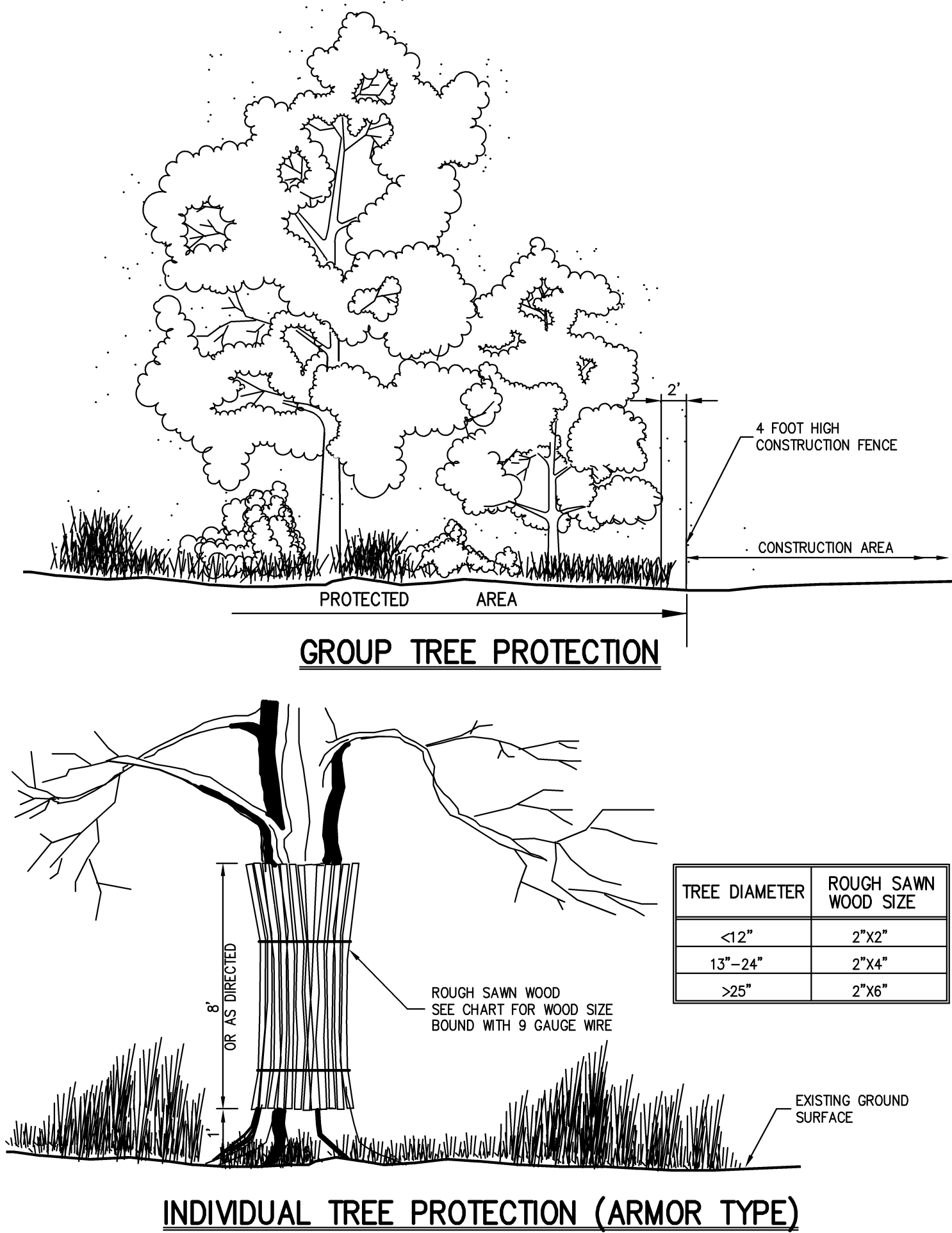
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DATE	N.T.S. 11/07/2013		
PROJECT NO.	13021		
DWG. 1301-DETAILS	TAB	SP-09	LS
DRAWING NO.	SP-9		

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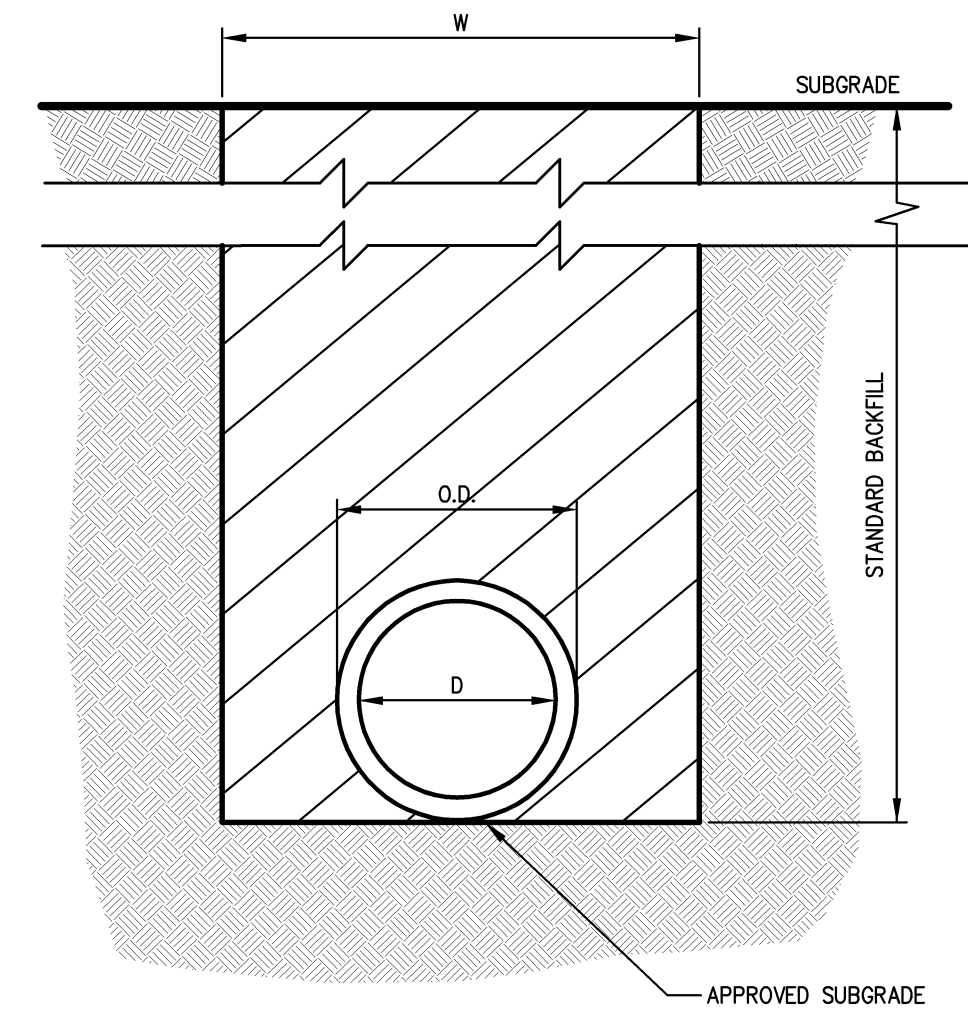


- CONSTRUCTION SPECIFICATIONS:**
- STONE WILL BE PLACED ON A FILTER FABRIC FOUNDATION TO THE LINES, GRADES AND LOCATIONS SHOWN IN THE PLAN.
  - SET SPACING OF CHECK DAMS TO ASSURE THAT THE ELEVATIONS OF THE CREST OF THE DOWNSTREAM DAM IS AT THE SAME ELEVATION OF THE TOE OF THE UPSTREAM DAM.
  - EXTEND THE STONE A MINIMUM OF 1.5 FEET BEYOND THE DITCH BANK TO PREVENT CUTTING AROUND THE DAM.
  - PROTECT THE CHANNEL DOWNSTREAM OF THE LOWEST CHECK DAM FROM SCOUR AND EROSION WITH STONE OR LINER AS APPROPRIATE.
  - ENSURE THAT CHANNEL APPURTENANCES SUCH AS CULVERT ENTRIES BELOW CHECK DAMS ARE NOT SUBJECT TO DAMAGE OR BLOCKAGE FROM DISPLACED STONES.

MAXIMUM DRAINAGE AREA 2 ACRES.



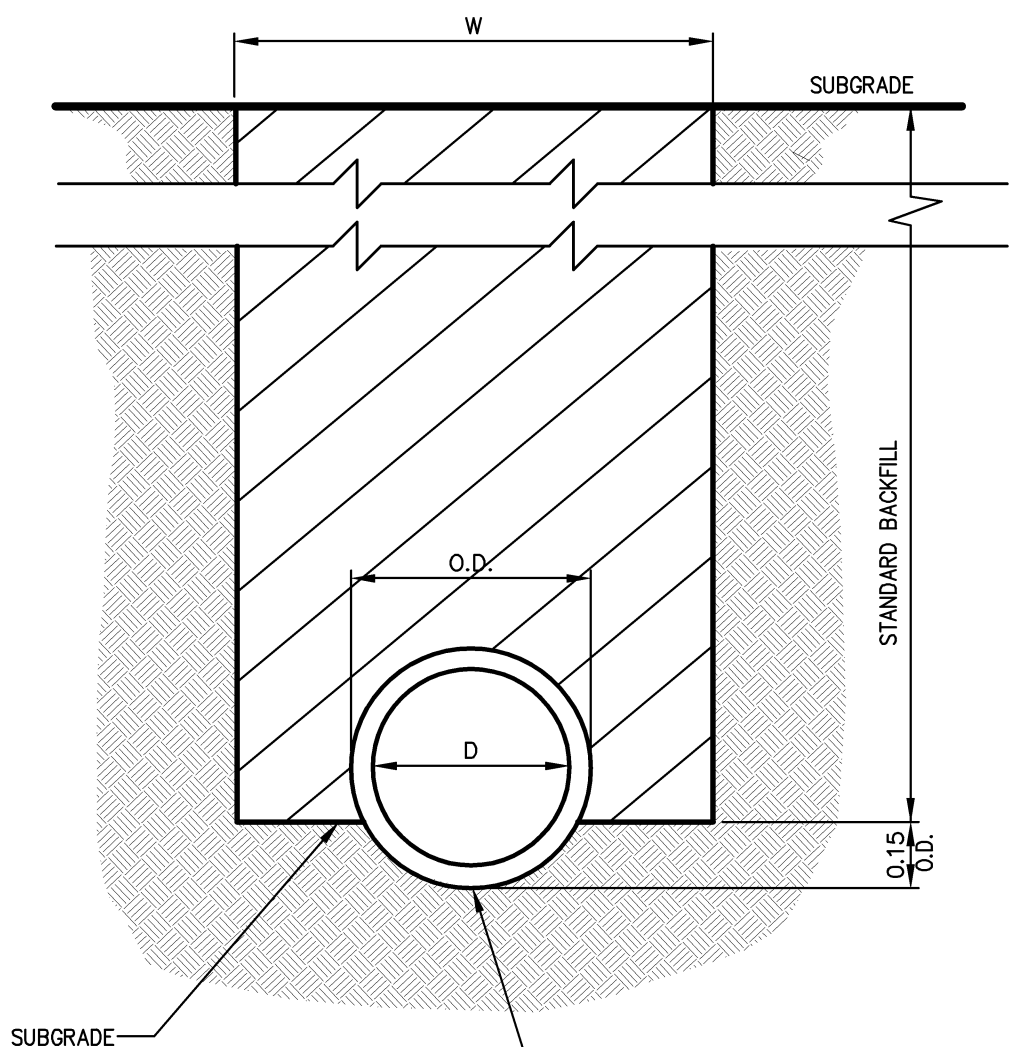
INDIVIDUAL TREE PROTECTION (ARMOR TYPE)



D=INSIDE DIAMETER, SPAN, OR RISE  
 O.D.=OUTSIDE BARREL DIAMETER, SPAN OR RISE  
 H.D.=OUTSIDE DIAMETER, SPAN, OR RISE @ BELL OR BAND  
 W=H.D. + 2.0' - FOR 48" OR SMALLER DIAMETER, SPAN, OR RISE  
 W=H.D. + 2.5' - FOR GREATER THAN 48" DIAMETER, SPAN, OR RISE

**NOTES:**

- IN FILL AREAS, EMBANKMENTS SHALL BE CONSTRUCTED TO A MAXIMUM OF 2 FEET ABOVE TOP OF PIPE BEFORE EXCAVATING TRENCH.
- FOR CORRUGATED POLYETHYLENE DRAIN PIPE (CPDP) INSTALLATION AND POLYVINYL CHLORIDE (PVC) PIPE AND CONDUIT INSTALLATION, USE TYPE II TRENCH.
- BACKFILL FOR PIPE SHALL BE PLACED EVENLY AND CAREFULLY AROUND AND OVER THE PIPE OR CONDUIT IN SIX (6) INCH MAXIMUM LAYERS. EACH LAYER SHALL BE THOROUGHLY AND CAREFULLY COMPACTED UNTIL TWELVE (12) INCHES OF COVER EXISTS OVER THE PIPE OR CONDUIT. THE REMAINDER OF THE BACKFILL MAY THEN BE PLACED AND COMPACTED IN A MAXIMUM OF TWELVE (12) INCH LAYERS. EACH LAYER SHALL BE COMPACTED BY APPROVED MECHANICAL TAMPING MACHINES. UNLESS OTHERWISE SPECIFIED BACKFILL SHALL BE COMPACTED TO NOT LESS THAN [92%] MAXIMUM MODIFIED DENSITY IN ACCORDANCE WITH ASTM DESIGNATION D-1557 IN THE MANNER HEREIN DESCRIBED. BACKFILL SHALL PROCEED UP TO THE LINES AND GRADES AS SHOWN ON THE DRAWINGS.



D=INSIDE DIAMETER, SPAN, OR RISE  
 O.D.=OUTSIDE BARREL DIAMETER, SPAN OR RISE  
 H.D.=OUTSIDE DIAMETER, SPAN, OR RISE @ BELL OR BAND  
 W=H.D. + 2.0' - FOR 48" OR SMALLER DIAMETER, SPAN, OR RISE  
 W=H.D. + 2.5' - FOR GREATER THAN 48" DIAMETER, SPAN, OR RISE

**NOTES:**

- IN FILL AREAS, EMBANKMENTS SHALL BE CONSTRUCTED TO A MAXIMUM OF 2 FEET ABOVE TOP OF PIPE BEFORE EXCAVATING TRENCH.
- FOR CORRUGATED POLYETHYLENE DRAIN PIPE (CPDP) INSTALLATION AND POLYVINYL CHLORIDE (PVC) PIPE AND CONDUIT INSTALLATION, USE TYPE II TRENCH.
- BACKFILL FOR PIPE SHALL BE PLACED EVENLY AND CAREFULLY AROUND AND OVER THE PIPE OR CONDUIT IN SIX (6) INCH MAXIMUM LAYERS. EACH LAYER SHALL BE THOROUGHLY AND CAREFULLY COMPACTED UNTIL TWELVE (12) INCHES OF COVER EXISTS OVER THE PIPE OR CONDUIT. THE REMAINDER OF THE BACKFILL MAY THEN BE PLACED AND COMPACTED IN A MAXIMUM OF TWELVE (12) INCH LAYERS. EACH LAYER SHALL BE COMPACTED BY APPROVED MECHANICAL TAMPING MACHINES. UNLESS OTHERWISE SPECIFIED BACKFILL SHALL BE COMPACTED TO NOT LESS THAN [92%] MAXIMUM MODIFIED DENSITY IN ACCORDANCE WITH ASTM DESIGNATION D-1557 IN THE MANNER HEREIN DESCRIBED. BACKFILL SHALL PROCEED UP TO THE LINES AND GRADES AS SHOWN ON THE DRAWINGS.

STONE CHECK DAM

8

TREE PROTECTION

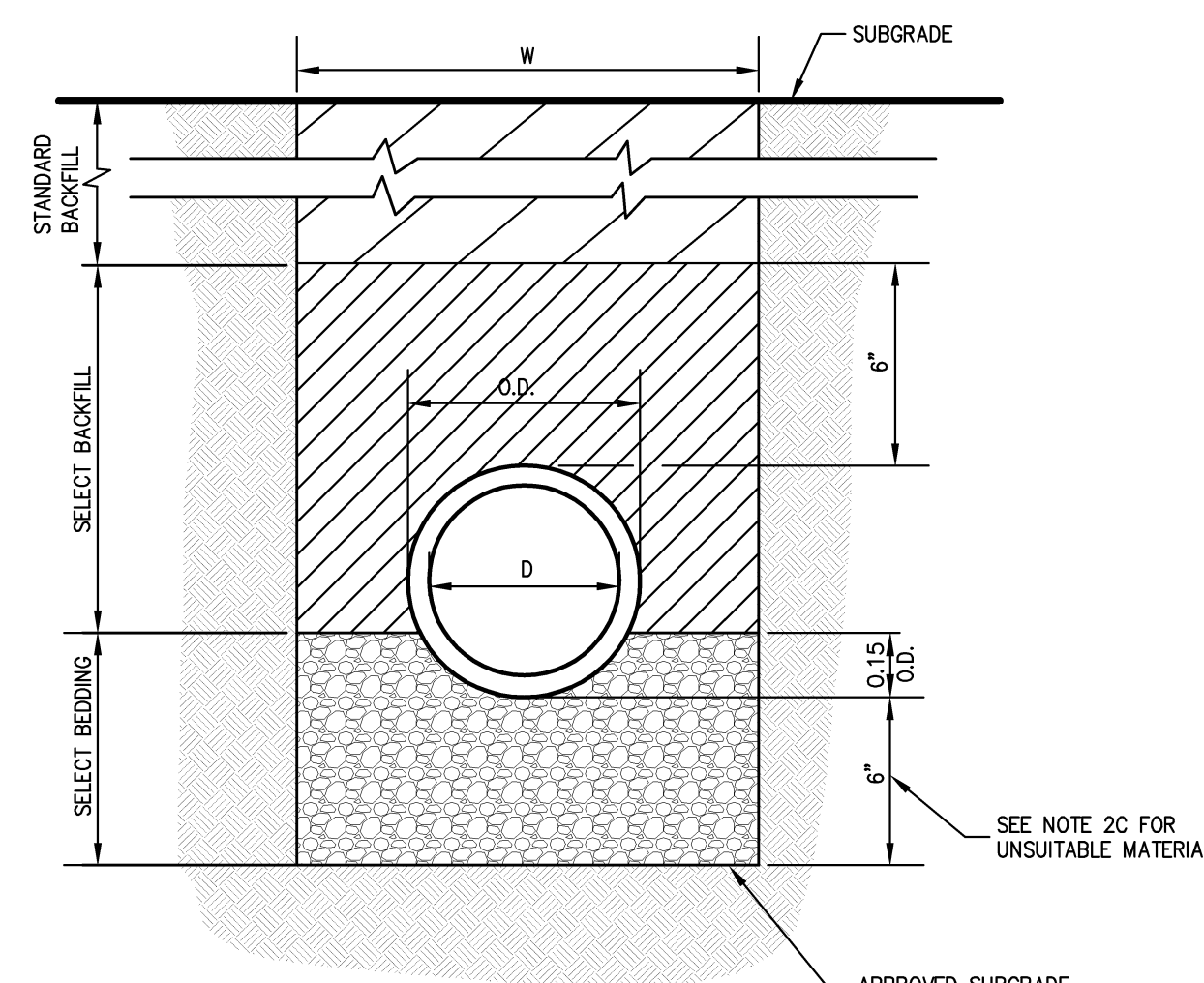
9

TYPE I TRENCH (FLAT BOTTOM)

10

TYPE I TRENCH (SHAPED BOTTOM)

11



D=INSIDE DIAMETER, SPAN, OR RISE  
 O.D.=OUTSIDE BARREL DIAMETER, SPAN OR RISE  
 H.D.=OUTSIDE DIAMETER, SPAN, OR RISE @ BELL OR BAND  
 W=H.D. + 2.0' - FOR 48" OR SMALLER DIAMETER, SPAN, OR RISE  
 W=H.D. + 2.5' - FOR GREATER THAN 48" DIAMETER, SPAN, OR RISE

**NOTES:**

- FOR TYPE II TRENCH, MATERIAL FOR SELECT BEDDING AND SELECT BACKFILL SHALL BE: A. EITHER SAND OR CRUSHED STONE IF NO WATER IS ENCOUNTERED IN TRENCH. B. 3/4" CRUSHED STONE IF WATER IS ENCOUNTERED IN TRENCH.
- TYPE II TRENCH SHALL BE USED IN ALL OF THE FOLLOWING CASES: A. FOR ALL CORRUGATED POLYETHYLENE DRAIN PIPE (CPDP) AND PVC PIPE AND CONDUIT INSTALLATION. B. WHEN ROCK OR HARDPAN IS ENCOUNTERED IN BOTTOM OF TRENCH. C. WHEN UNSUITABLE MATERIAL IS ENCOUNTERED IN BOTTOM OF TRENCH. IN SUCH CASE DEPTH OF UNDERCUTTING SHALL BE AS DIRECTED BY THE ENGINEER WITH 6" MINIMUM.
- FOR ALL TRENCH EXCAVATION IN FILL AREAS, ALL EMBANKMENTS SHALL BE CONSTRUCTED TO A MINIMUM OF 2 FEET ABOVE THE OUTSIDE TOP (AT THE BELL) OF THE PIPE PRIOR TO BEGINNING ANY TRENCH EXCAVATION.
- BACKFILL FOR PIPE AND CONDUIT SHALL BE PLACED EVENLY AND CAREFULLY AROUND AND OVER THE PIPE OR CONDUIT IN SIX (6) INCH MAXIMUM LAYERS. EACH LAYER SHALL BE THOROUGHLY AND CAREFULLY COMPACTED UNTIL TWELVE (12) INCHES OF COVER EXISTS OVER THE PIPE OR CONDUIT. THE REMAINDER OF THE BACKFILL MAY THEN BE PLACED AND COMPACTED IN A MAXIMUM OF TWELVE (12) INCH LAYERS. EACH LAYER SHALL BE COMPACTED BY APPROVED MECHANICAL TAMPING MACHINES. UNLESS OTHERWISE SPECIFIED BACKFILL SHALL BE COMPACTED TO NOT LESS THAN [92%] MAXIMUM MODIFIED DENSITY IN ACCORDANCE WITH ASTM DESIGNATION D-1557 IN THE MANNER HEREIN DESCRIBED. BACKFILL SHALL PROCEED UP TO THE LINES AND GRADES AS SHOWN ON THE DRAWINGS.

**NOTES PERTAINING TO DRAIN INLETS**

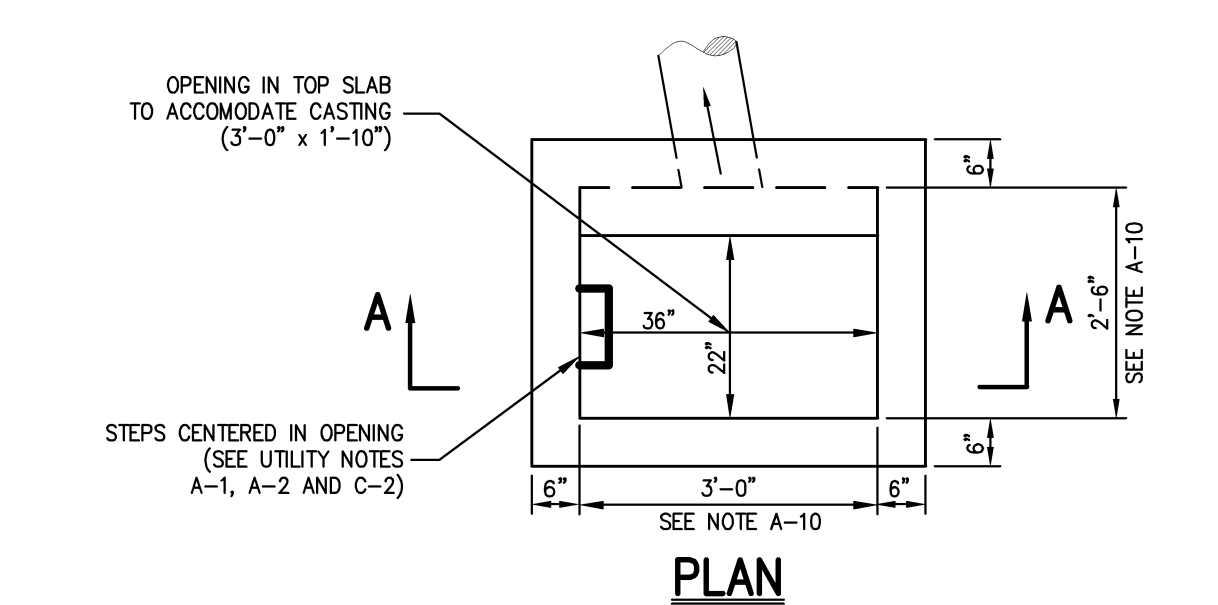
- STEPS WILL NOT BE REQUIRED IN INLETS LESS THAN FOUR (4) FEET IN DEPTH. STEPS WILL BE REQUIRED IN INLETS FOUR (4) FEET OR GREATER IN DEPTH. DEPTHS FOR DRAIN INLETS SHALL BE MEASURED FROM FINISHED GRADE TO INSIDE BOTTOM OF STRUCTURE (INCLUDING SUMP AS APPLICABLE).
- WHEN STEPS ARE REQUIRED, STEPS SHALL COMPLY WITH THE SAME REQUIREMENTS OF ASTM STANDARD C-478, ARTICLE 13 ENTITLED "MANHOLE STEPS & LADDERS".
- FOR MASONRY STRUCTURES, THE FIRST COURSE OF MASONRY SHALL BE SET IN THE CONCRETE FOUNDATION BEFORE THE CONCRETE HAS SET. CONCRETE FOUNDATION SHALL BE CLASS "A"(4000 psi) CONCRETE, TWELVE (12) INCHES THICK AND SHALL EXTEND SIX (6) INCHES BEYOND THE OUTSIDE FACE THE STRUCTURE.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH AND CONSTRUCT THE PROPER SIZE STRUCTURE INCLUDING THE NECESSARY OPENINGS TO ACCOMMODATE THE WORK AS SHOWN ON THE PLANS OR ORDERED BY THE ENGINEER, AT NO ADDITIONAL COST TO THE OWNER.
- ALL NECESSARY PATCHING FOR DRAIN STRUCTURES SHALL BE ACCOMPLISHED WITH NON-SHRINKING CEMENT MORTAR GROUT, APPROVED EQUAL TO SIKKA-SET AS MANUFACTURED BY THE SIKKA CHEMICAL CORP.
- FOUNDATIONS FOR PRECAST CONCRETE STRUCTURES SHALL BE SET ON A COMPACTED LAYER OF APPROVED CRUSHED STONE HAVING A MINIMUM COMPACTED THICKNESS OF EIGHT (8) INCHES.
- ALL PIPES SHALL BE CUT FLUSH WITH THE INSIDE WALL OF THE STRUCTURE.
- PROVIDE REINFORCED CONCRETE TOP SLAB FOR OVERSIZED DRAIN INLETS WITH PROPER SIZE OPENING TO ACCOMMODATE INSTALLATION OF FRAME & GRATE.
- FOR MASONRY STRUCTURES GREATER THAN TEN (10) FEET IN DEPTH, THICKNESS OF MASONRY WALLS SHALL BE INCREASED TO TWELVE (12) INCHES.
- FOR ALL STRUCTURES GREATER THAN 10 FEET IN DEPTH, STRUCTURES SHALL PROVIDE MINIMUM INSIDE DIMENSIONS OF 4 FEET X 4 FEET.

**NOTES PERTAINING TO MANHOLES**

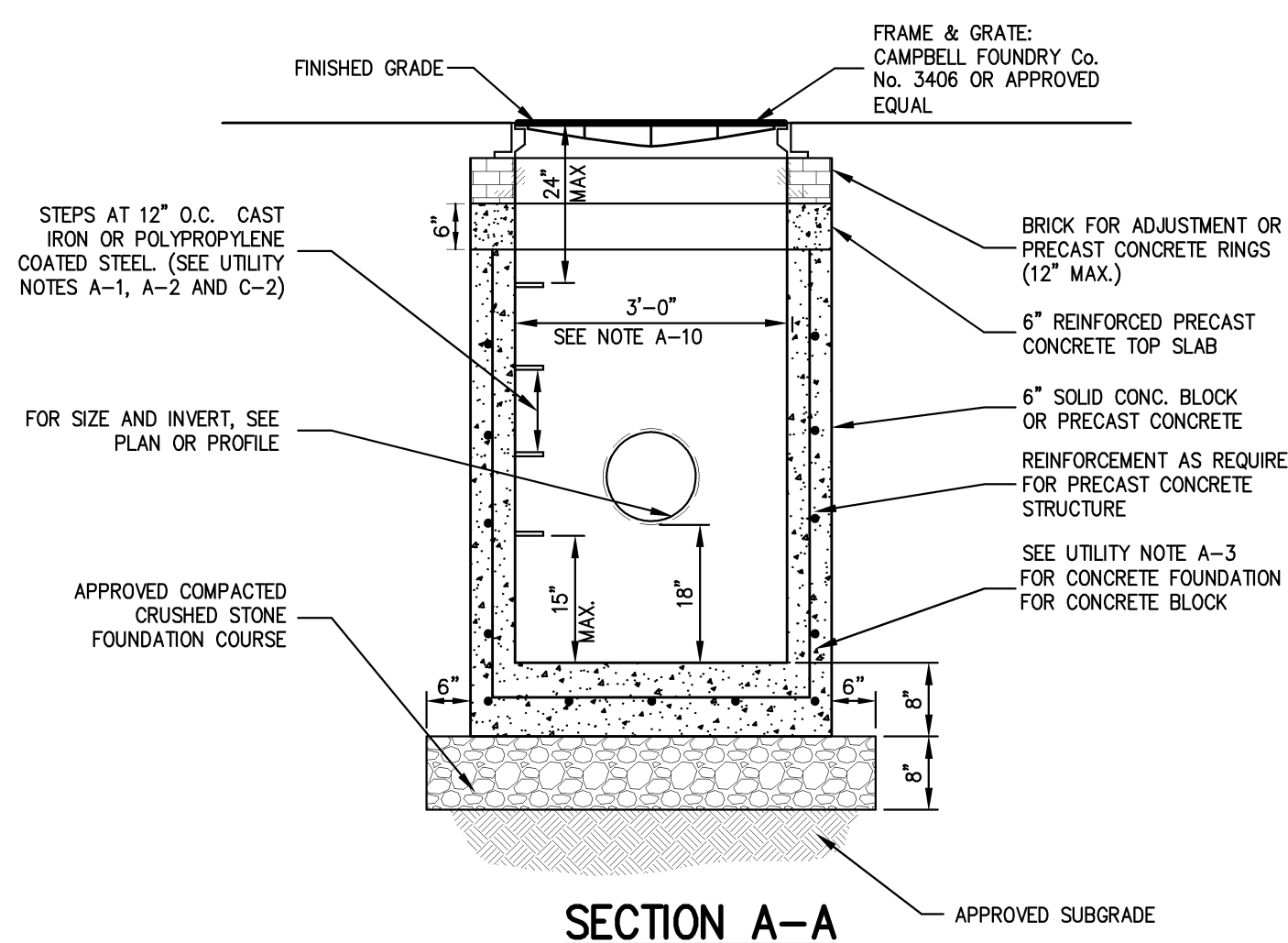
- PRECAST CONCRETE MANHOLES SHALL COMPLY WITH ASTM STANDARD C-478. MANHOLE JOINTS SHALL COMPLY WITH ASTM STANDARD C-443.
- FOR PRECAST CONCRETE MANHOLES FIVE (5) FEET OR LESS IN HEIGHT, TOP CONE SECTION SHALL BE REPLACED WITH PRECAST REINFORCED CONCRETE SLAB (6" MIN. THICKNESS) WITH OPENING OF SUFFICIENT SIZE TO ACCOMMODATE MANHOLE CASTING.
- FOR MANHOLES 10 FEET OR MORE IN DEPTH, MANHOLE DIAMETER SHALL BE FIVE (5) FEET.
- TERMINAL MANHOLE FLOORS SHALL BE SLOPED TOWARD OUTFALL PIPE.
- INVERT CHANNELS FOR PRECAST CONCRETE MANHOLES SHALL BE CONSTRUCTED OF CONCRETE.
- NOTES A-1, A-2, A-4, A-5, A-6 & A-7 UNDER "NOTES PERTAINING TO DRAIN INLETS" ABOVE SHALL APPLY TO MANHOLES.

**NOTES PERTAINING TO PRECAST CONCRETE STRUCTURES FOR STORM DRAINS, SANITARY SEWERS AND WATER LINES**

- ALL PRECAST CONCRETE STRUCTURES SHALL BE DESIGNED TO ACCOMMODATE AN H-20 DESIGN LOAD.
- STEPS SHALL BE LOCATED WITHIN STRUCTURE TO AVOID PLACEMENT OVER PIPES WHEN PRACTICABLE.



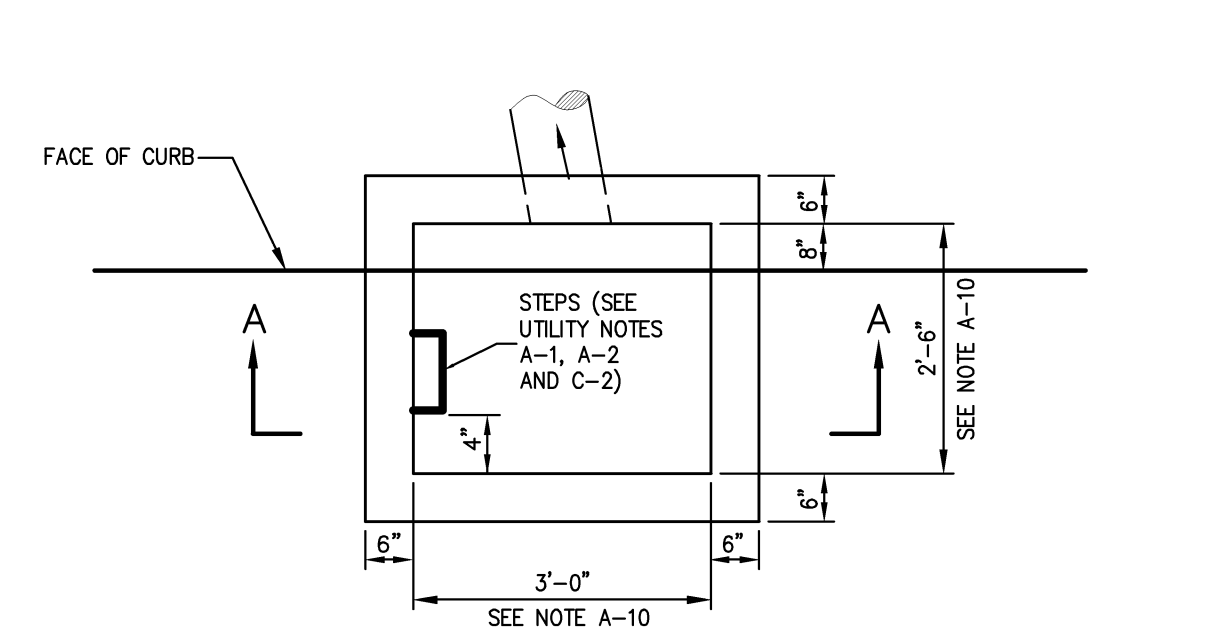
PLAN



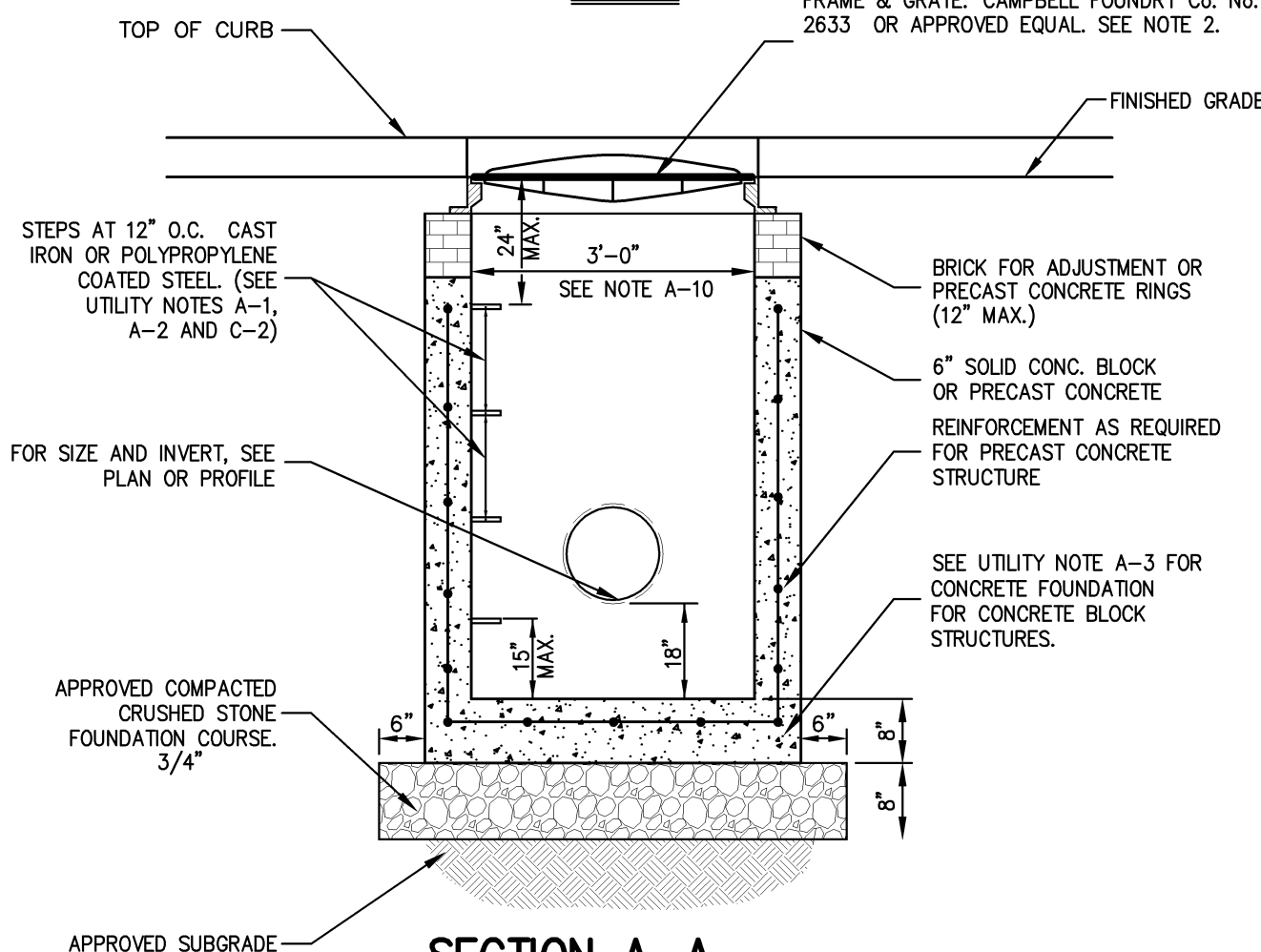
SECTION A-A

**NOTE:**

- REINFORCE PRECAST CONCRETE TOP SLAB AND REINFORCE PRECAST CONCRETE STRUCTURE SHALL BE DESIGNED TO ACCOMMODATE AN H-20 DESIGN LOAD.
- SEE NOTES PERTAINING TO DRAIN INLETS UNDER UTILITY NOTES ON DRAWING SP-10



PLAN



SECTION A-A

**NOTE:**

- SEE NOTES PERTAINING TO DRAIN INLETS UNDER UTILITY NOTES ON DRAWING SP-10

TYPE II TRENCH

12

UTILITY NOTES

13

DRAIN INLET (TYPE DI)

14

DRAIN INLET (TYPE CI)

15

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REASON						

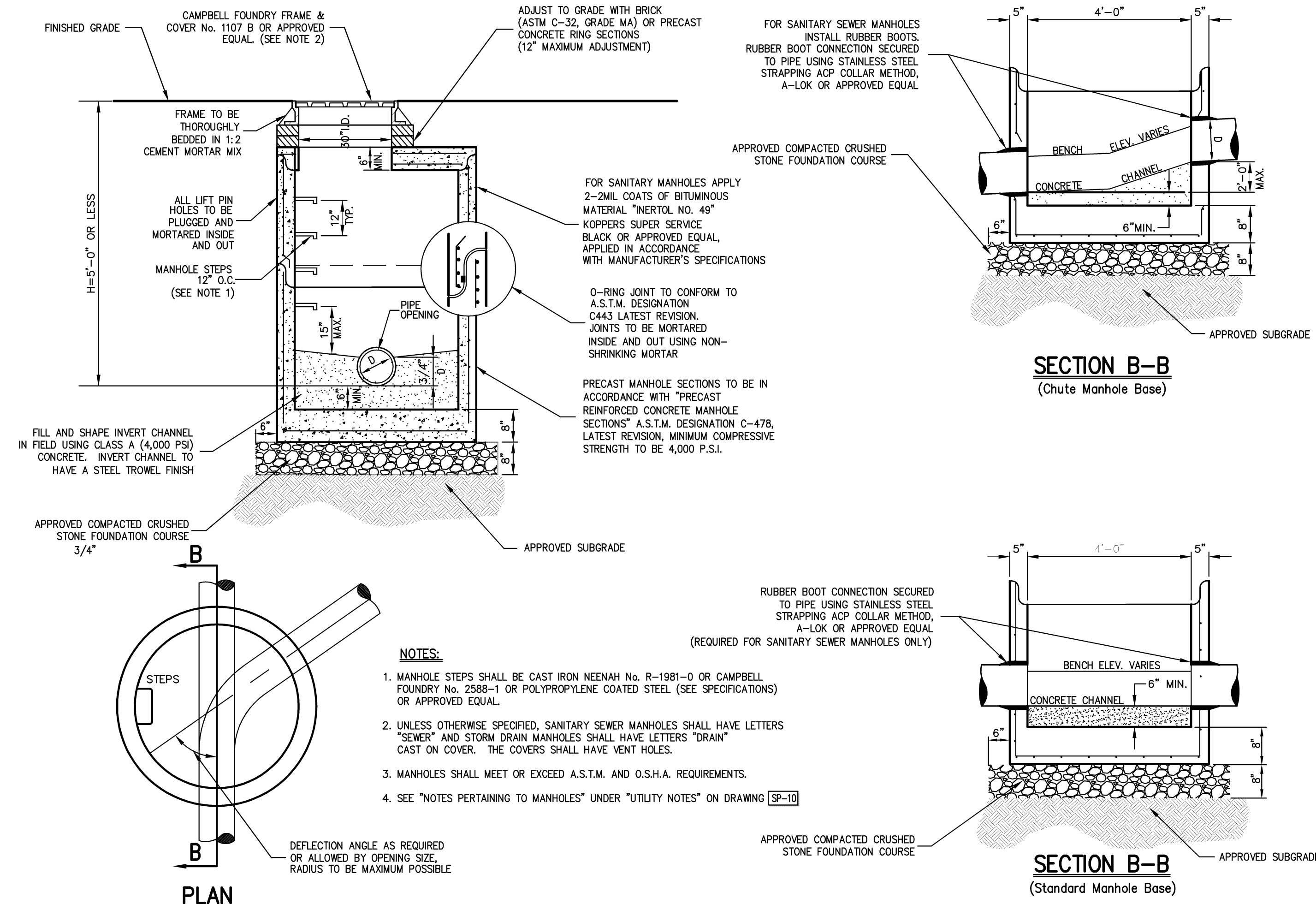
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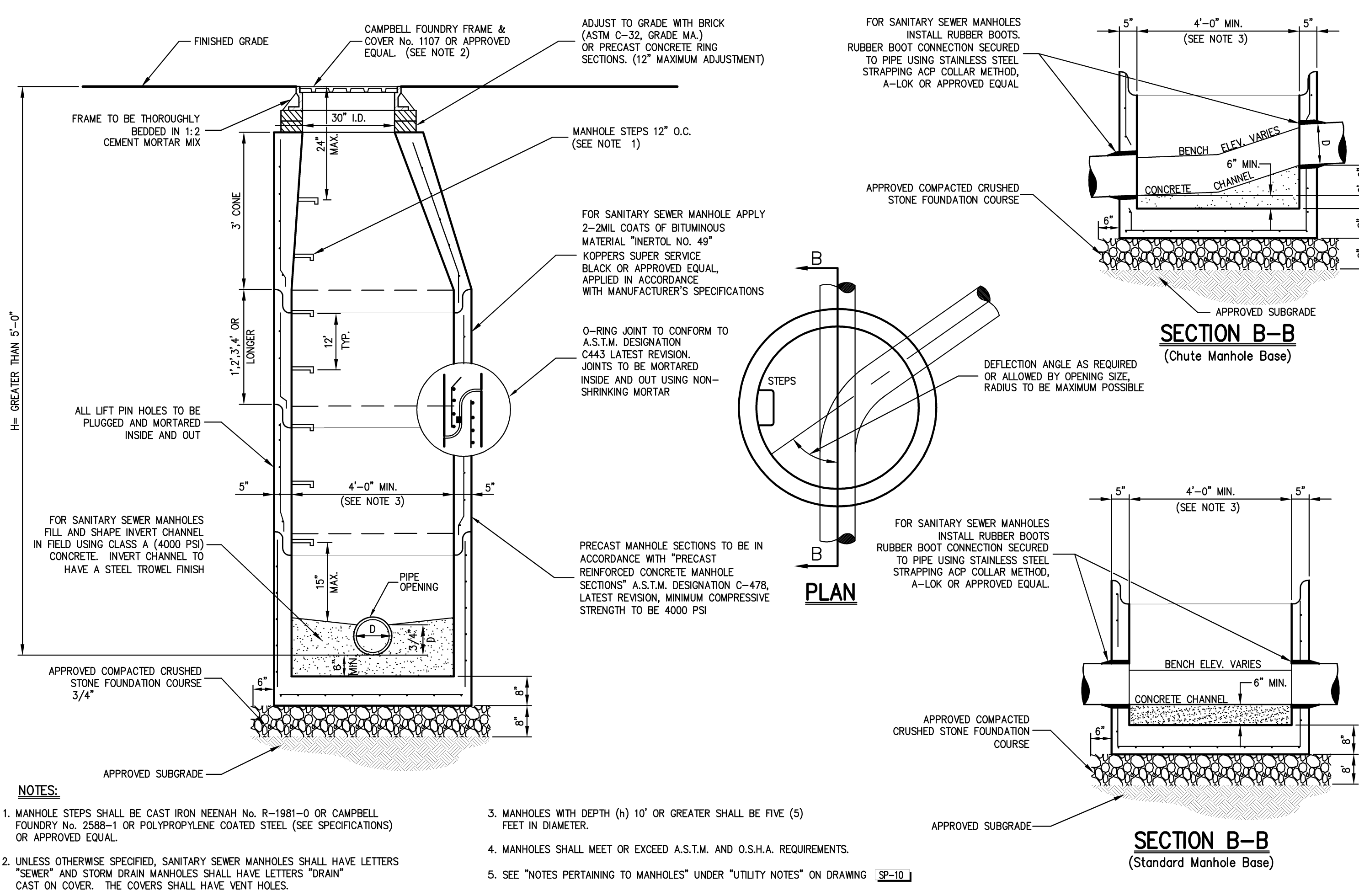
VOLKSWAGEN OF NEWBURGH  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK

DRAWN	JE	APPROVED	RR
SCALE	N.T.S.		
DATE	11/07/2013		
PROJECT NO.	13021		
DWG:	1302-DEWIS	DWG:	SP-10
DRAWING NO.	SP-10		



**MANHOLE (TYPE A)**  
(H < 5'-0")

16

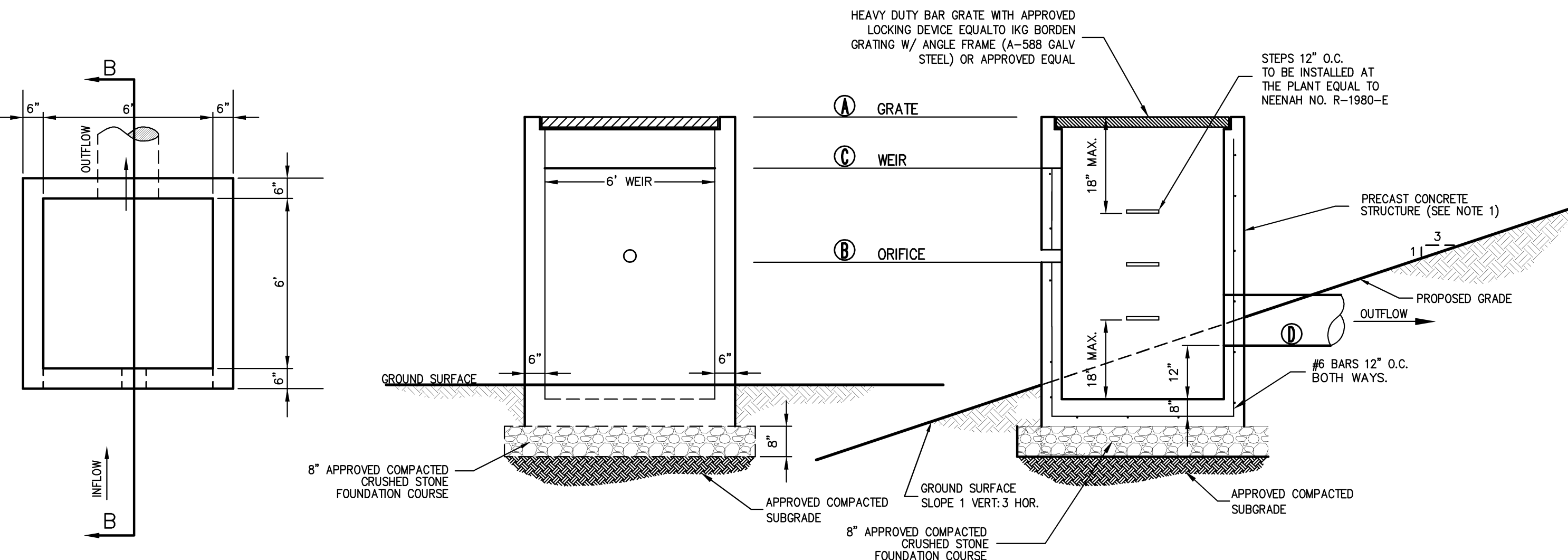


**MANHOLE (TYPE B)**  
(H > 5'-0") < 10'-0")

17

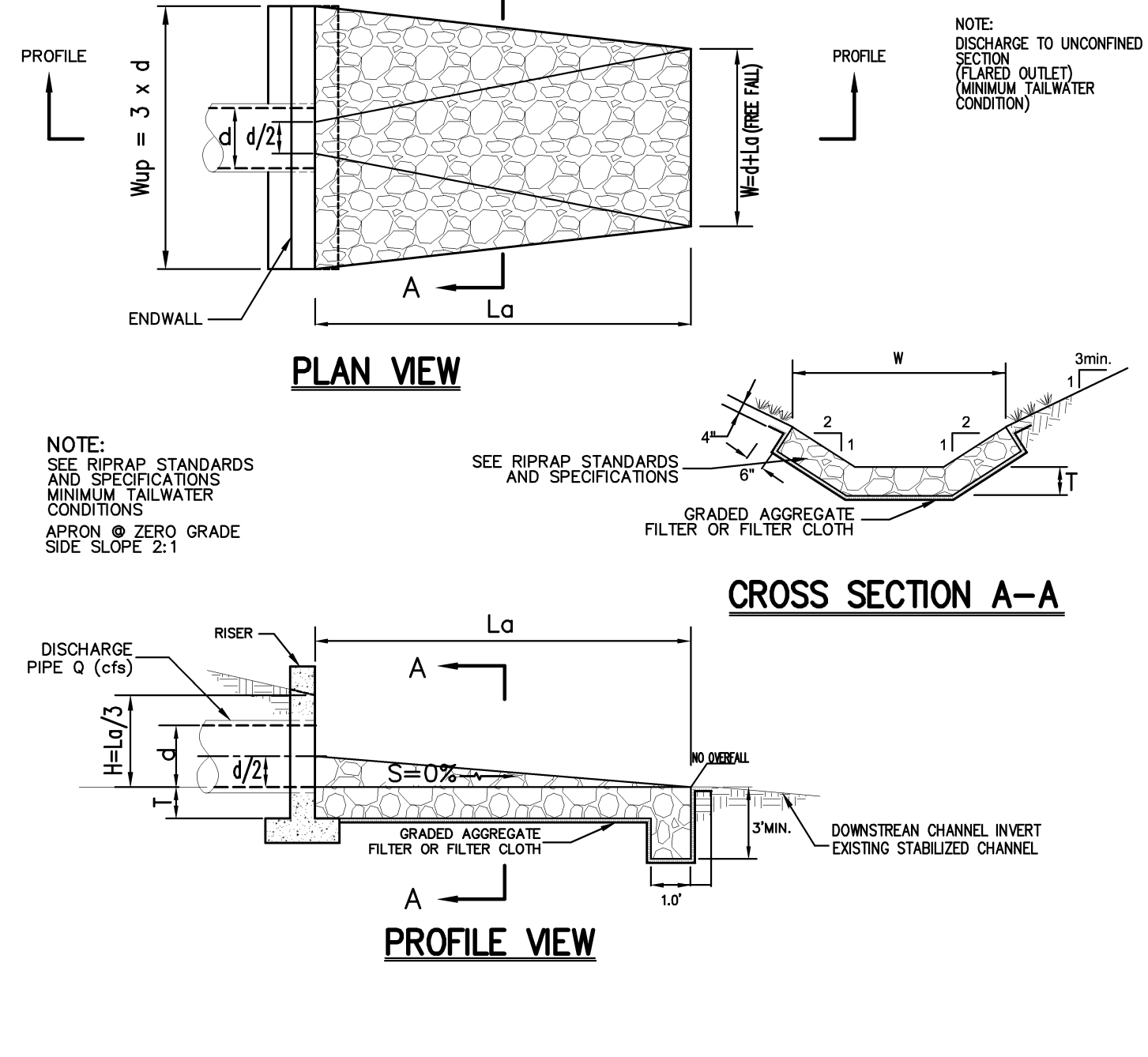
**STRUCTURE CHART**

BASIN No.	STRUCTURE No.	GRATE (A) ELEV.	ORIFICE (B) DIAMETER	WEIR (C) ELEV.	WEIR (C) SIZE	OUTFLOW (D) DIAMETER	OUTFLOW (D) ELEV.
#1	OCS-1B	411.00	12"	408.00	6"	409.50	18"



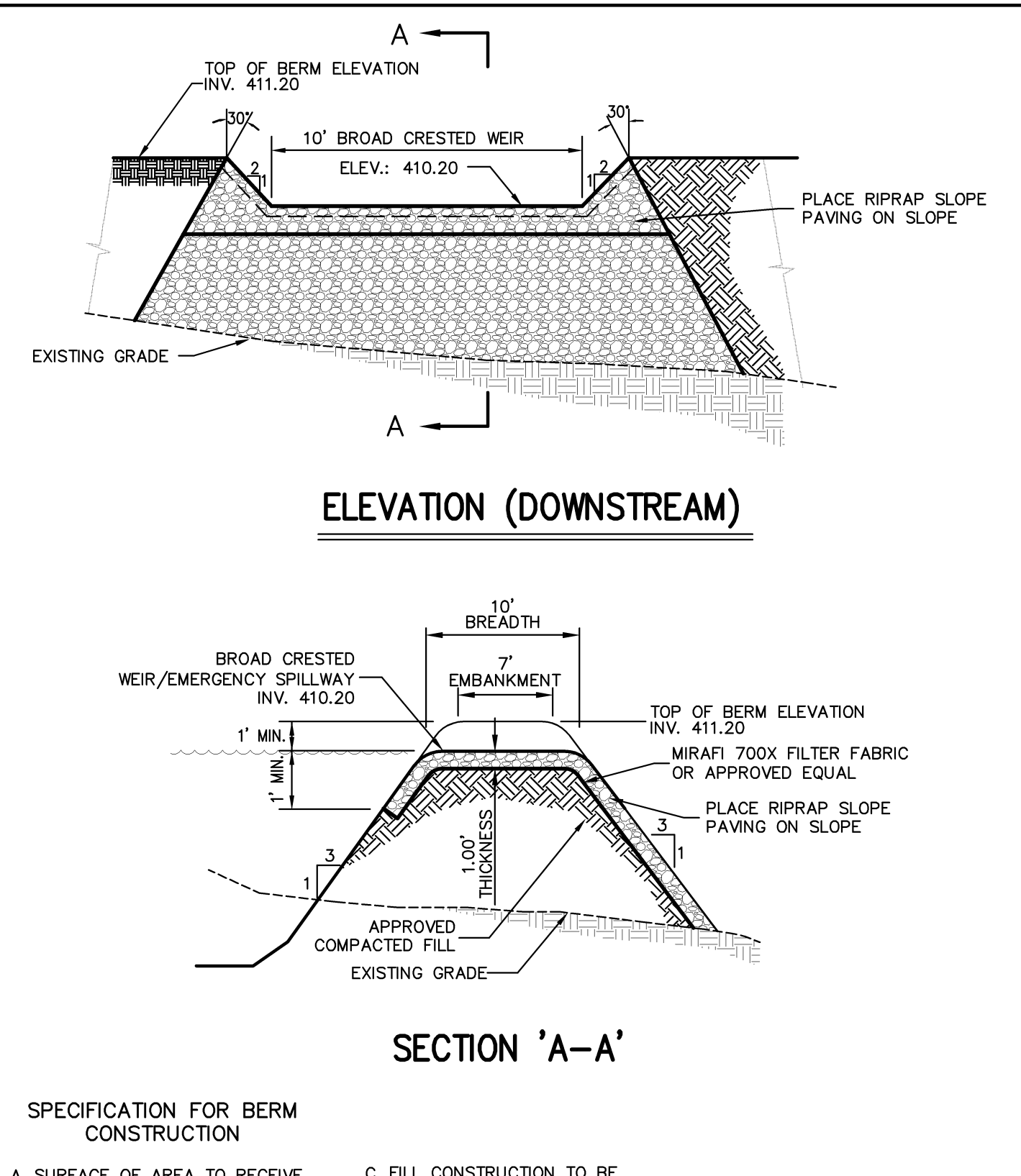
**OUTLET CONTROL STRUCTURE**

18



**RIP-RAP APRON/ENERGY DISSIPATOR**

19



**RIP-RAP EMERGENCY SPILLWAY**

20

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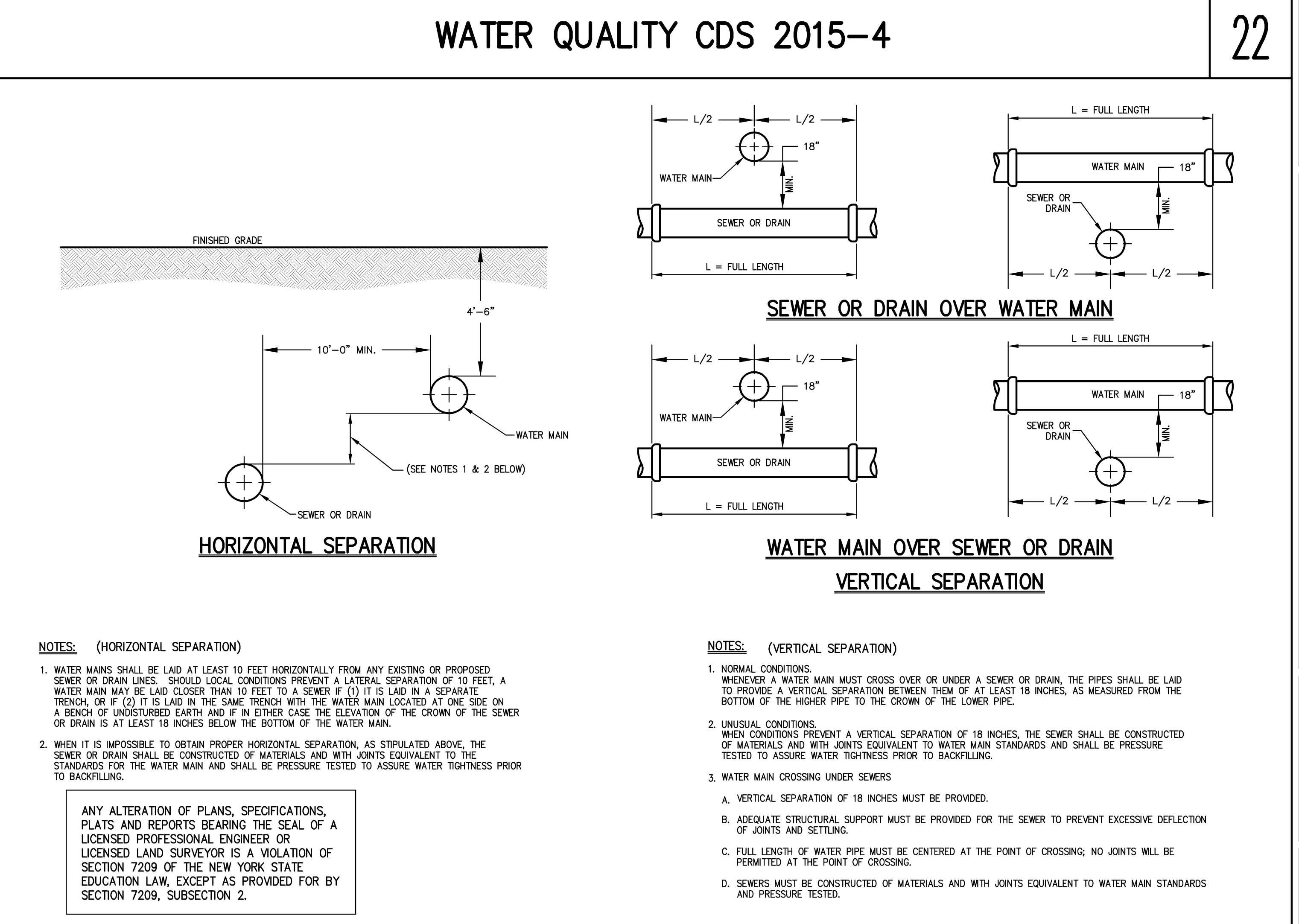
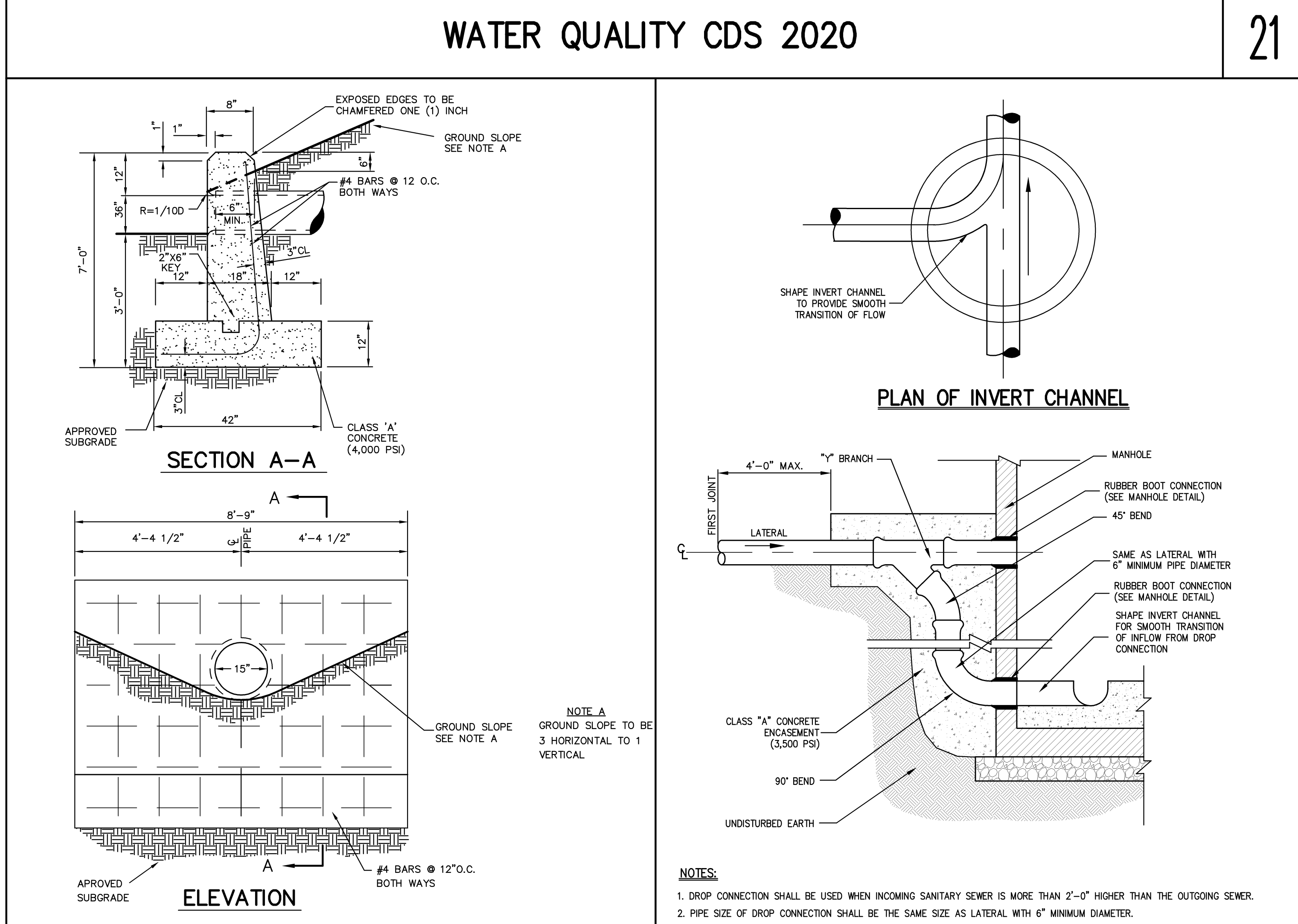
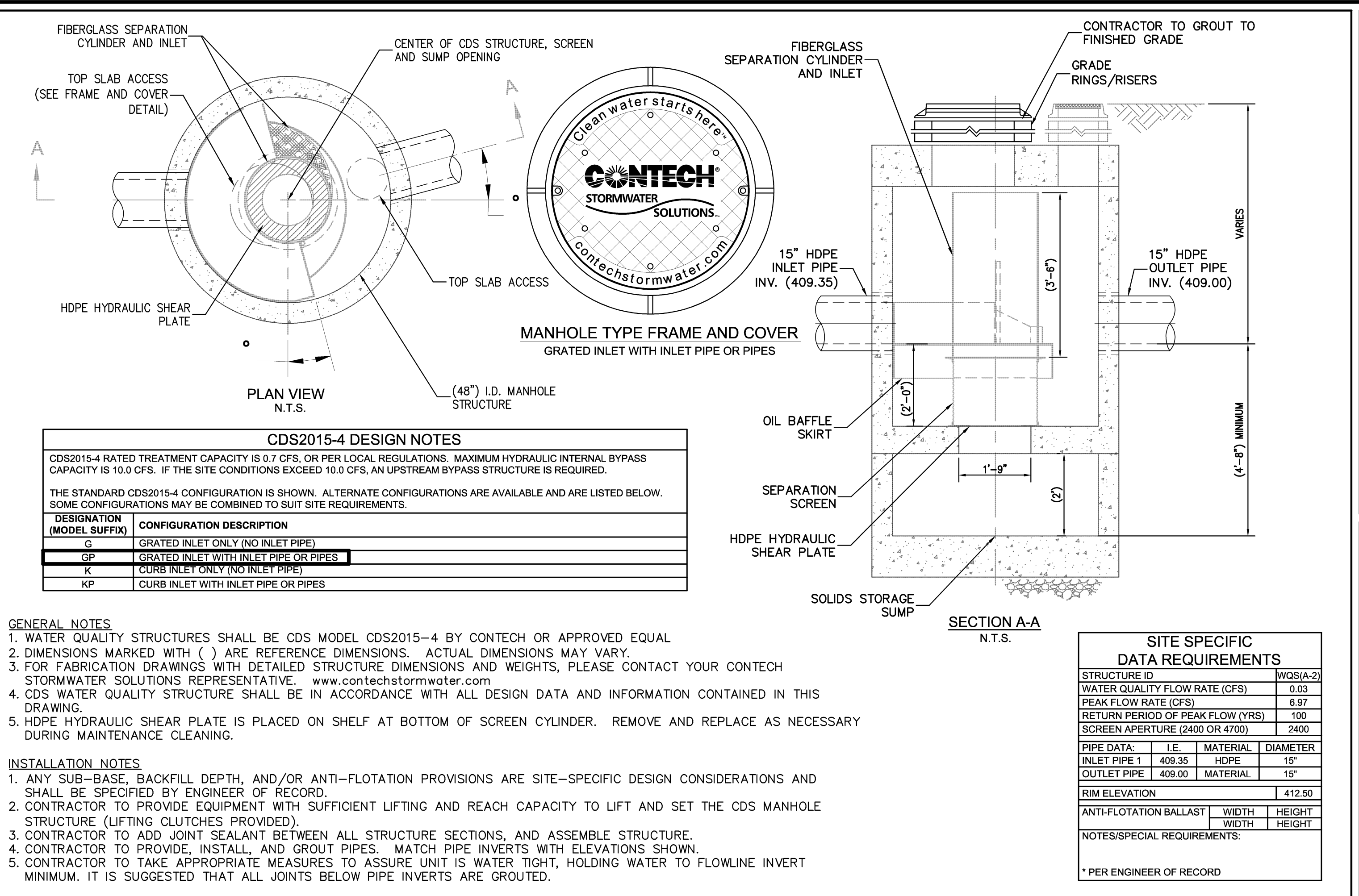
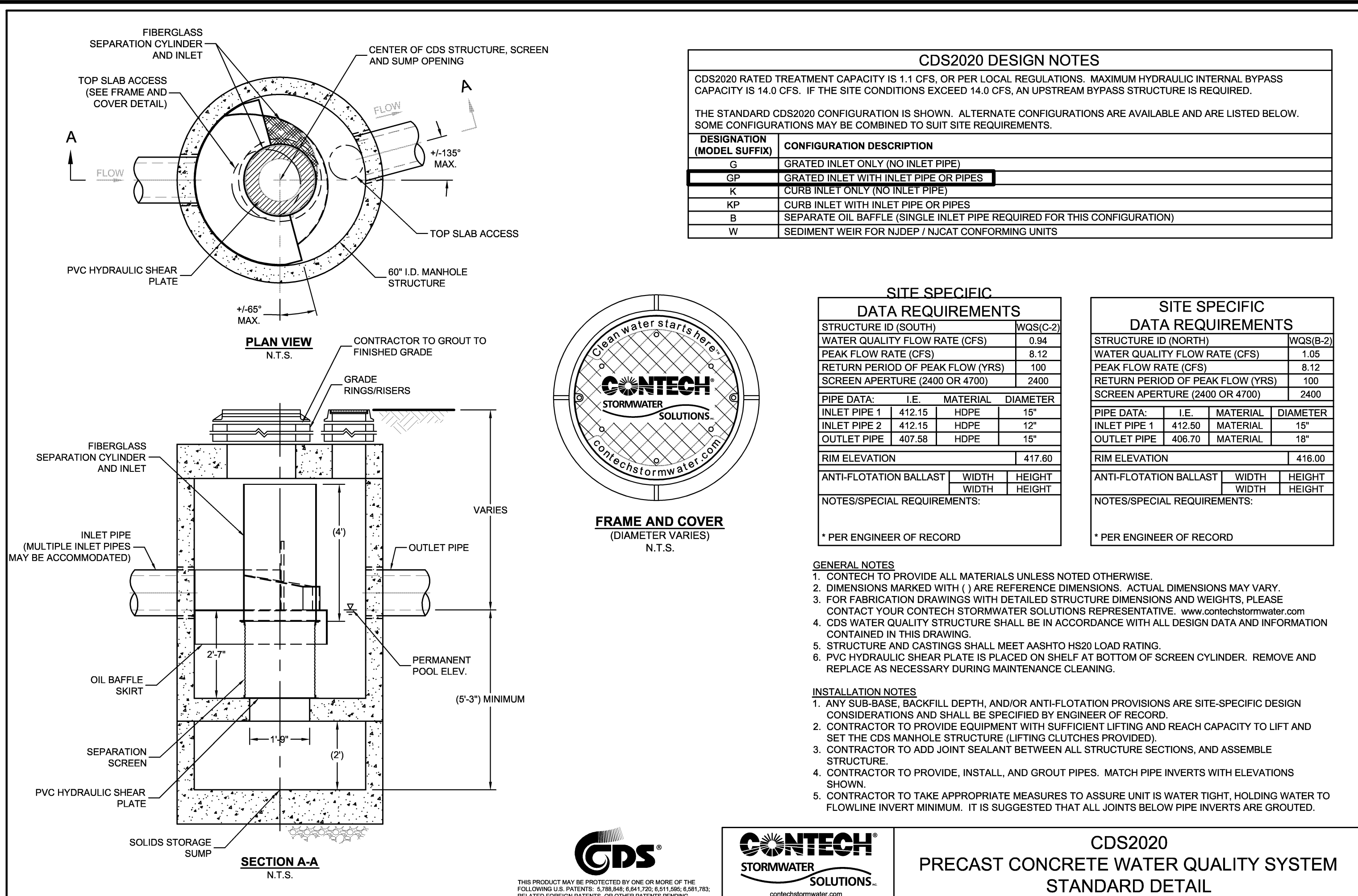
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**CONSTRUCTION DETAILS**

**VOLKSWAGEN OF NEWBURGH**  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

SCALE:	JE	APPROVED:	RR
DATE:	11/07/2013		
PROJECT No.:	13021		
DWG.:	13021-DETAILS	TAB.:	SP-11
DRAWING No.:	SP-11		



STRAIGHT CONCRETE HEADWALL 23

DROP CONNECTION AT MANHOLE 24

SEPARATION OF WATER AND SEWER/DRAIN LINES 25

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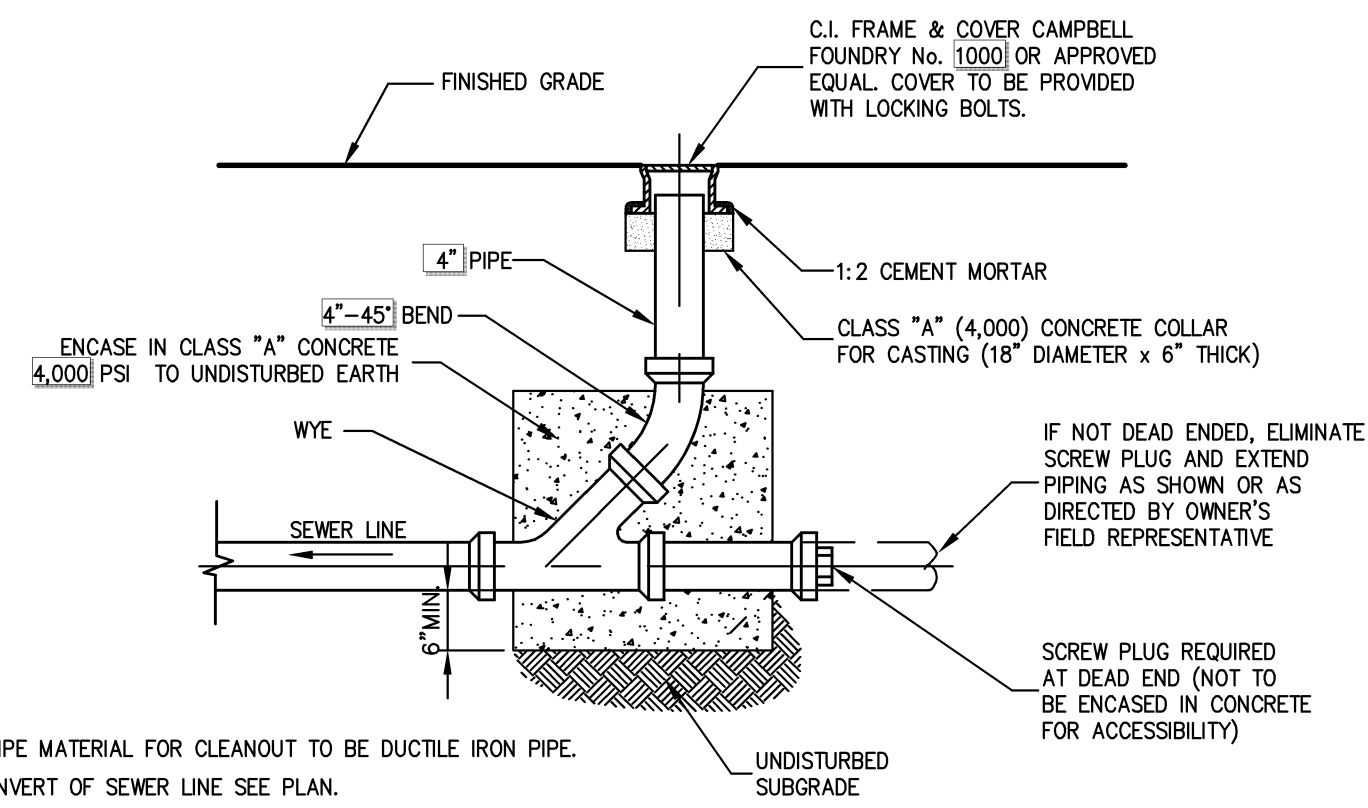
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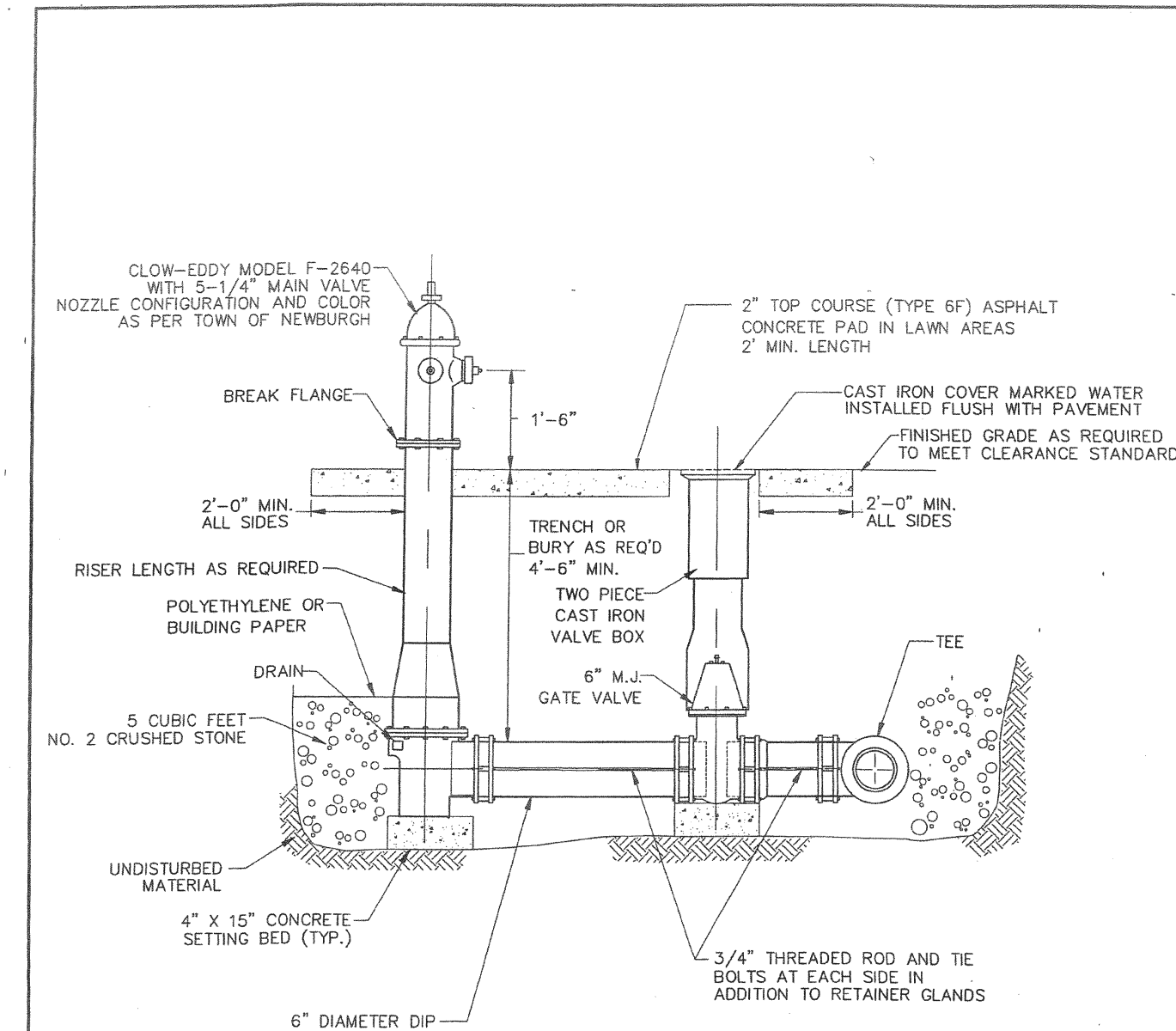
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 ROUTE 17K, VW DEALERSHIP  
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 TAB: SP-12  
 L.S.:  
 DRAWING No: SP-12



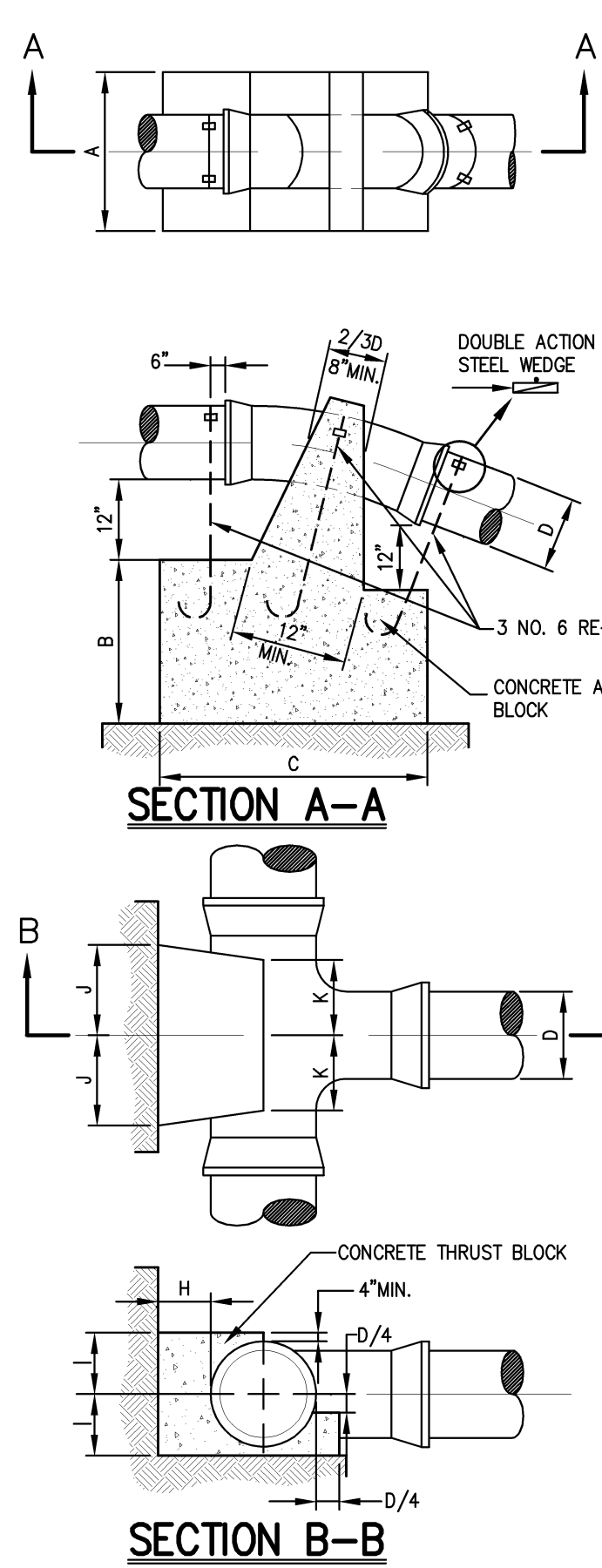
**CLEANOUT W/O MANHOLE**

**26**



**HYDRANT AND VALVE ASSEMBLY INSTALLATION**

**27**



**VERTICAL BEND**

BEND	DIAMETER (D)					
	4"	6"	8"	10"	12"	16"
1/32	A	1'-6"	1'-6"	2'-0"	2'-6"	3'-0"
	B	1'-3"	1'-3"	1'-9"	1'-3"	2'-0"
	C	2'-0"	2'-0"	2'-6"	2'-9"	3'-0"
1/16	A	2'-0"	2'-0"	3'-4"	3'-8"	4'-0"
	B	1'-9"	1'-9"	2'-5"	2'-6"	2'-6"
	C	2'-6"	2'-6"	2'-6"	4'-0"	4'-0"
1/8	A	2'-6"	2'-6"	3'-0"	4'-0"	5'-2"
	B	2'-6"	2'-6"	2'-9"	3'-0"	3'-6"
	C	3'-0"	3'-0"	4'-0"	4'-6"	6'-6"

**HORIZONTAL BEND**

BEND	DIAMETER (D)				
	4"	6"	8"	10"	12"
1/32	A	8"	8"	8"	10"
	B	7"	7"	8"	9"
	C	7"	7"	7"	8"
1/16	A	9"	9"	12"	1'-6"
	B	7"	7"	8"	9"
	C	8"	8"	9"	10"
1/8	A	1'-3"	1'-3"	1'-8"	2'-1"
	B	7"	7"	8"	9"
	C	8"	8"	8"	10"
1/4	A	2'-0"	2'-0"	2'-6"	3'-0"
	B	7"	7"	9"	12"
	C	2'-0"	2'-0"	1'-9"	1'-8"

**TEE**

D	4"	6"	8"	10"	12"
H	6"	7"	9"	10"	12"
I	8"	8"	10"	12"	1'-3"
J	7"	7"	9"	12"	1'-2"
K	6"	6"	8"	8"	8"

**CAP**

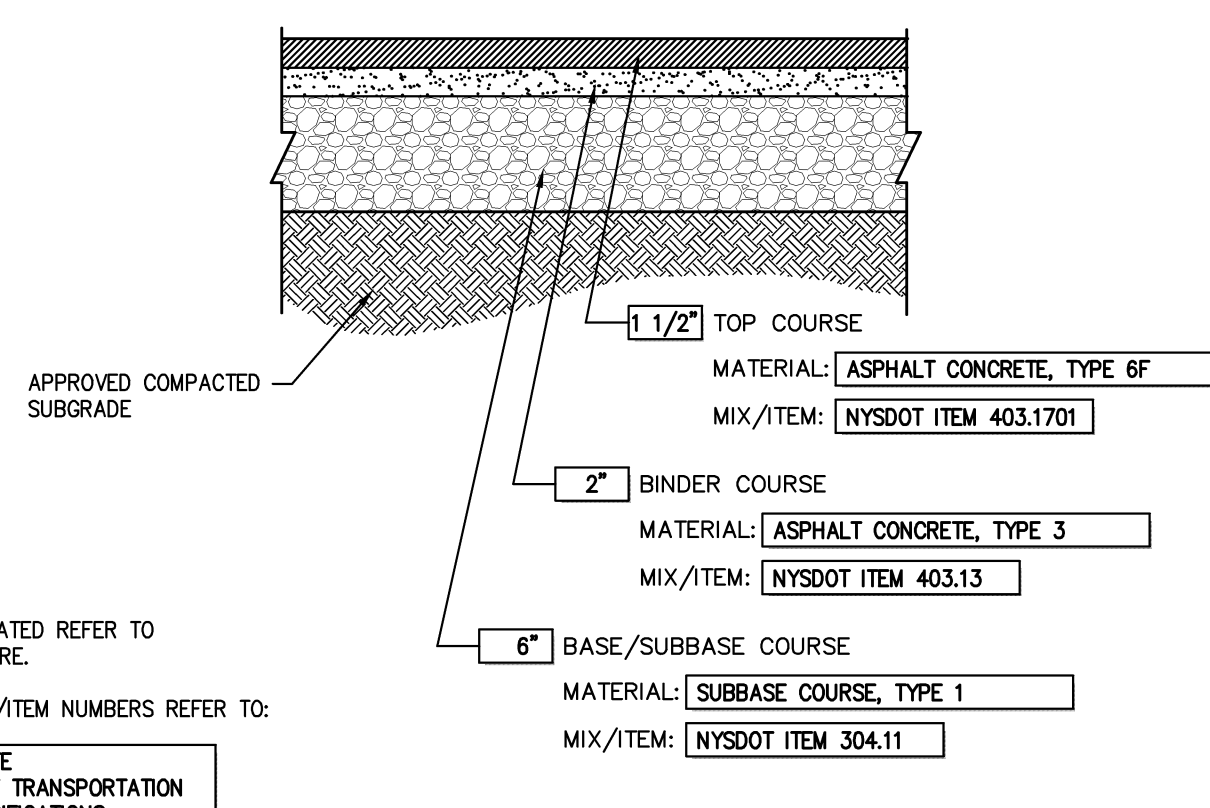
D	4"	6"	8"	10"	12"
E	6"	6"	8"	8"	10"
F	12"	12"	1'-4"	1'-8"	2'-0"
G	1'-5"	1'-5"	2'-0"	2'-5"	3'-0"

- NOTES:**
- ALL CONCRETE TO BE CLASS A (4,000 PSI).
  - ALL ANCHOR AND THRUST BLOCKS TO BEAR ON UNDISTURBED EARTH.
  - IN ALL CASES SHOWN, MEASUREMENT "D" REFERS TO THE INSIDE PIPE DIAMETER.
  - ANCHOR AND THRUST BLOCK DIMENSIONS SHOWN ARE MINIMUM FOR 100 PSI WATER PRESSURE AND SOIL RESISTANCE OF 2 KIPS PER SQ FT. CONTRACTOR SHALL ADJUST FOR OTHER CONDITIONS AND SUBMIT FOR APPROVAL.

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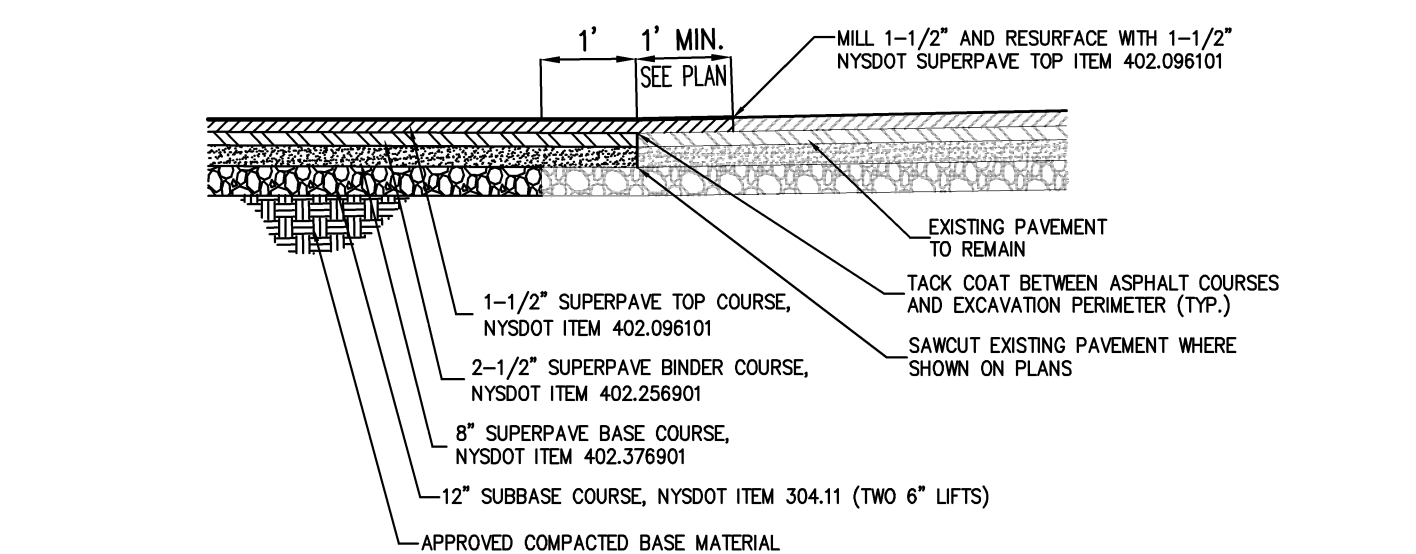
**ANCHOR AND THRUST BLOCKS**

**28**



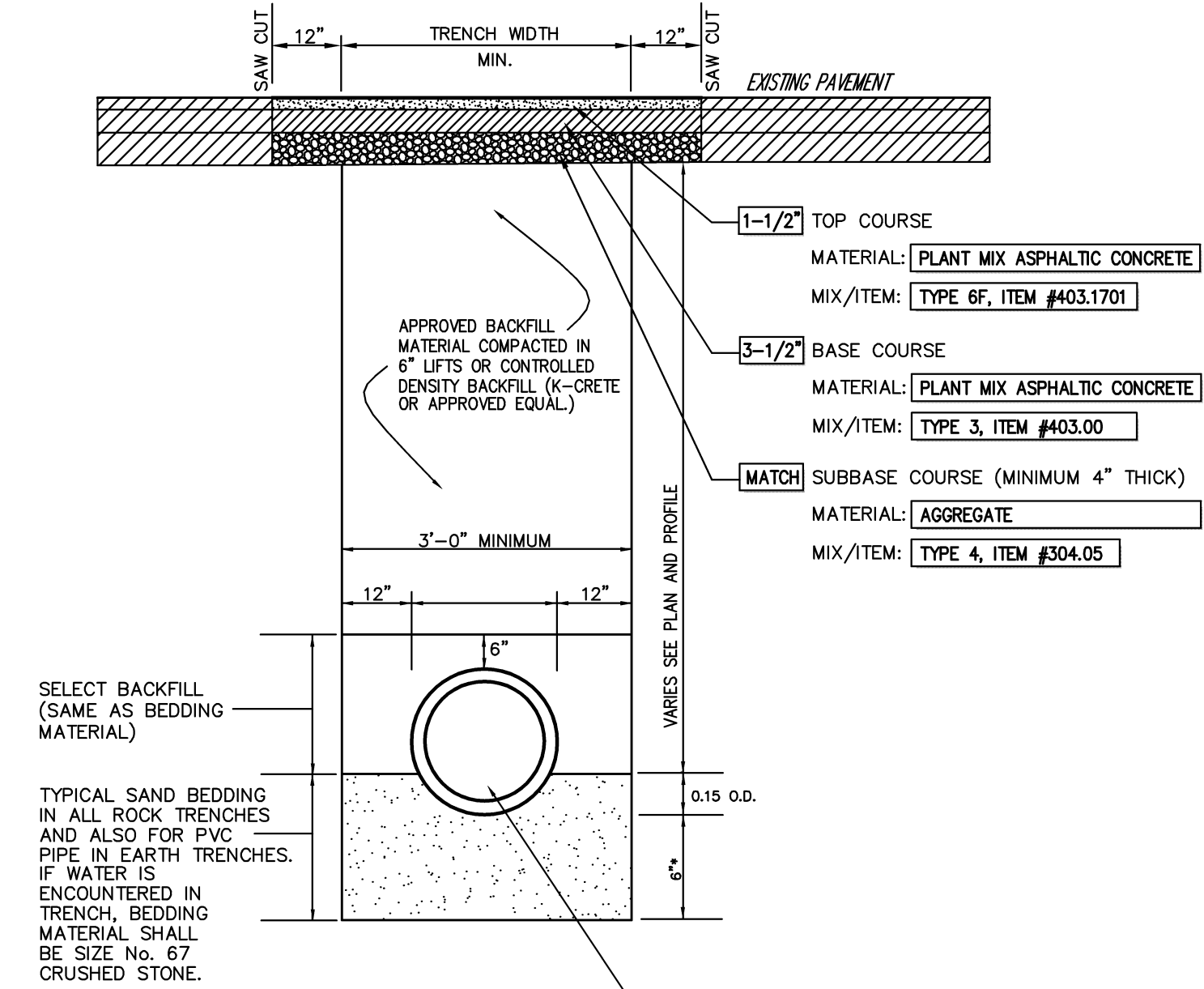
**SITE PAVEMENT (Light Duty)**

**29**



**FULL DEPTH SUPERPAVE NYSDOT PAVEMENT SECTION**

- NOTES:**
- THICKNESSES INDICATED REFER TO COMPACTED MEASURE.
  - MATERIAL AND MIX/ITEM NUMBERS REFER TO NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
  - ASPHALT (HMA) COMPACTION REQUIREMENT: NUCLEAR GAUGE MONITORING WITH CORE VERIFICATION (TOP, BINDER, BASE), SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
  - ITEM 407.01 TACKCOAT SHALL BE APPLIED TO EACH LIFT OF ASPHALT AT THE FOLLOWING RATES:
- | SURFACE TYPE             | APPLICATION RATE (LITERS/M <sup>2</sup> ) |
|--------------------------|---|
| NEW HOT MIX ASPHALT      | 0.14 - 0.28                               |
| MILLED                   | 0.21 - 0.28                               |
| EXISTING HOT MIX ASPHALT | 0.21 - 0.25                               |
| PORTLAND CEMENT CONCRETE | 0.17 - 0.30                               |
| VERTICAL SURFACES        | 0.27 - 0.32                               |



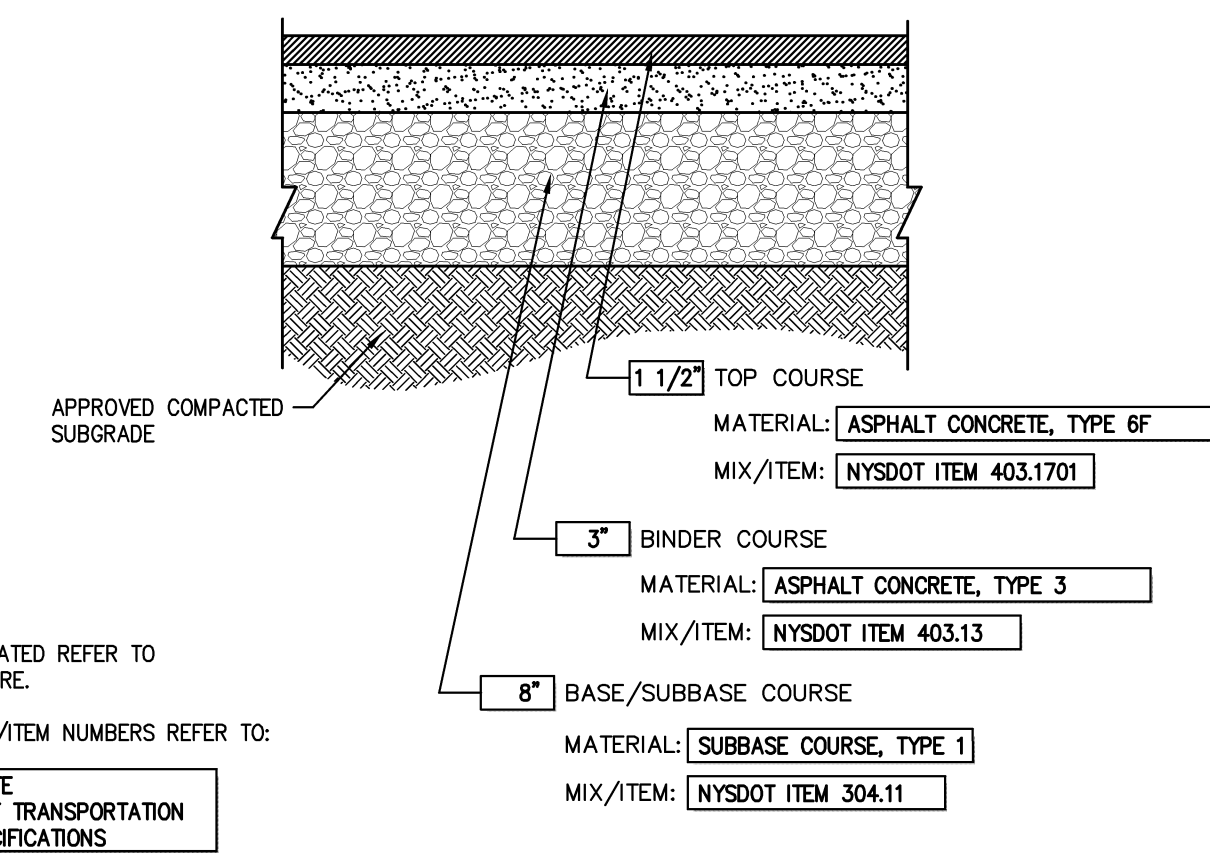
- NOTES:**
- WHERE ROCK IS ENCOUNTERED IN TRENCH BOTTOM, UNDERCUT MINIMUM OF 6"
  - WHERE UNSUITABLE MATERIAL IS ENCOUNTERED IN TRENCH BOTTOM, UNDERCUT AS DIRECTED BY OWNER'S FIELD REPRESENTATIVE.

- NOTES:**
- THICKNESS INDICATED REFERS TO COMPACTED MEASURE.

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**TRENCH PAVEMENT REPLACEMENT**

**32**

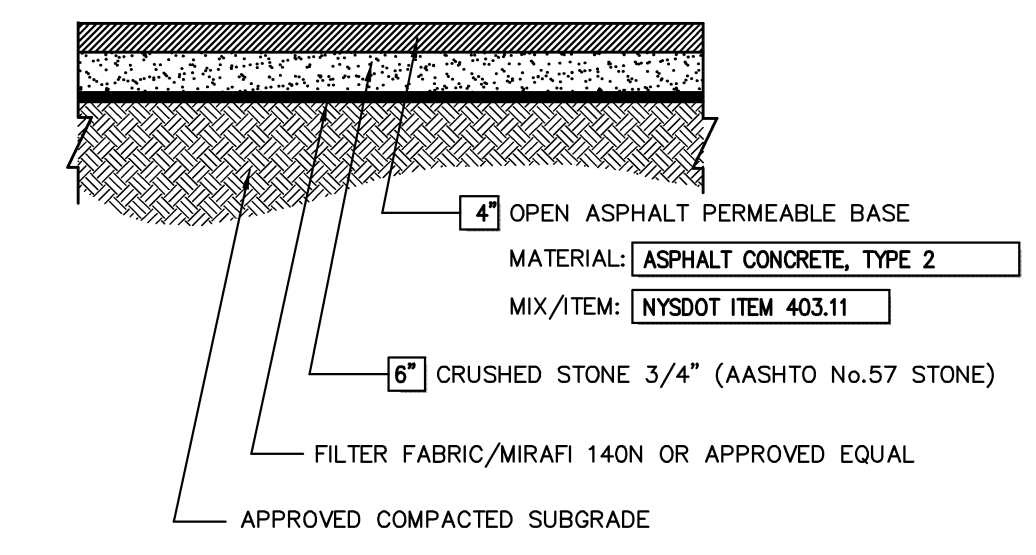


**SITE PAVEMENT (Heavy Duty)**

**30**

**NYSDOT PAVEMENT SECTION**

**31**



- NOTES:**
- THICKNESSES INDICATED REFER TO COMPACTED MEASURE.
  - MATERIAL AND MIX/ITEM NUMBERS REFER TO:
- NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS

**POROUS PAVEMENT**

**33**

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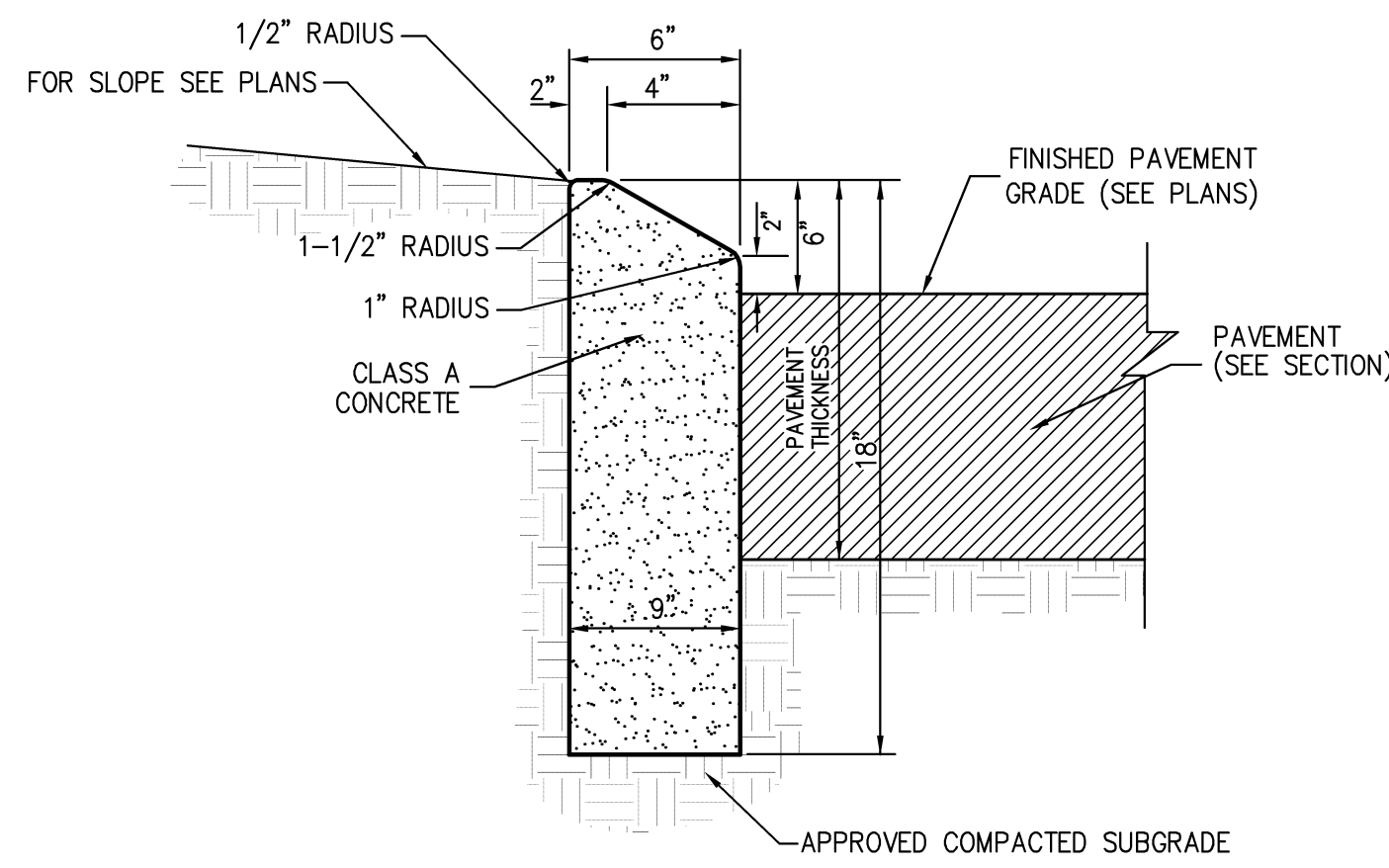
BY	JE
DATE	03/21/2014
REVISION	
NO.	1.

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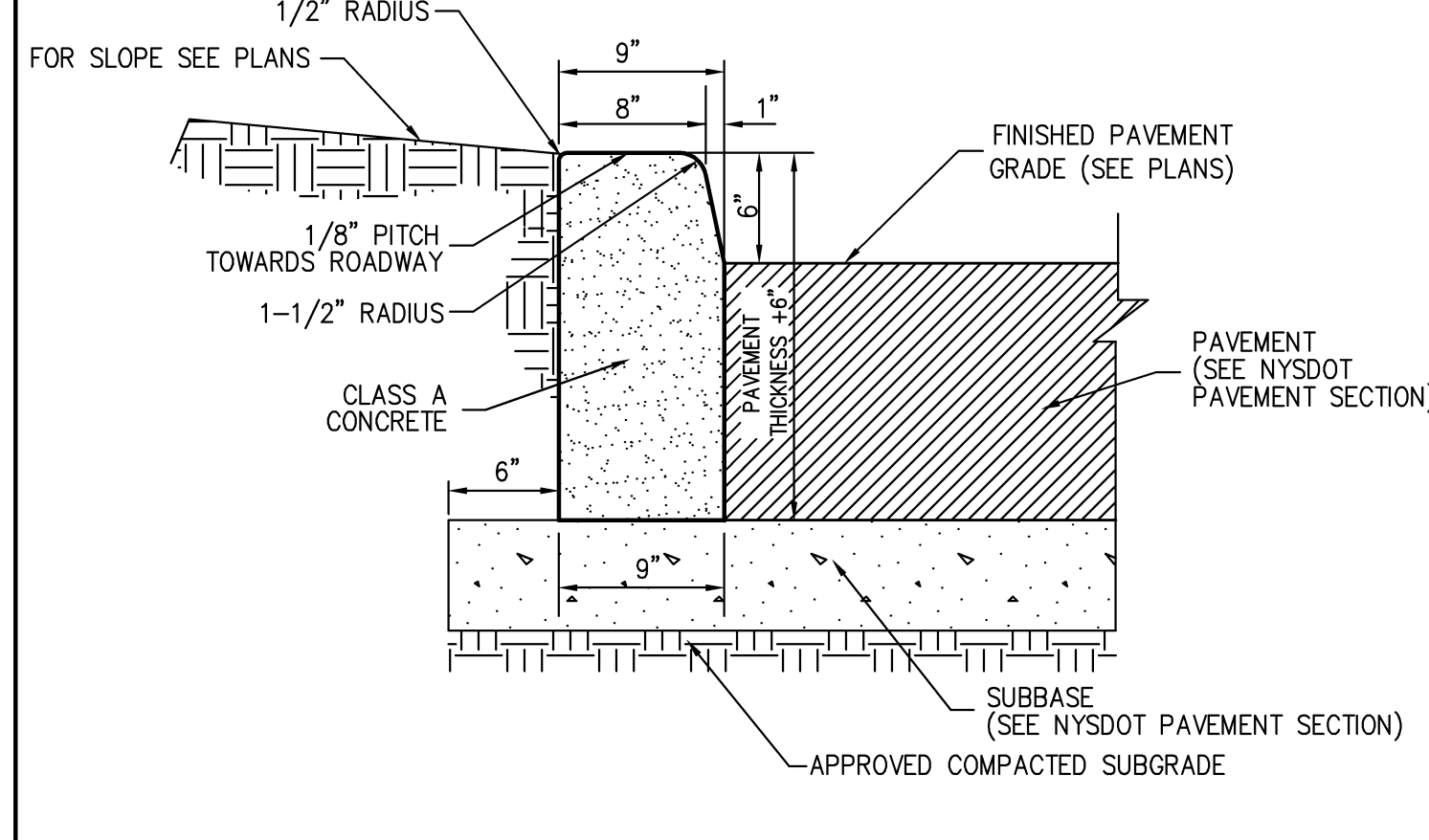
**CONSTRUCTION DETAILS**  
VOLKSWAGEN OF NEWBURGH  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

DRAWN	JE	APPROVED	RR
SCALE	N.T.S.		
DATE	11/07/2013		
PROJECT NO.	13021		
DWG. NO.	SP-13	LS	
DRAWING NO.	SP-13		



**MOUNTABLE CURB**

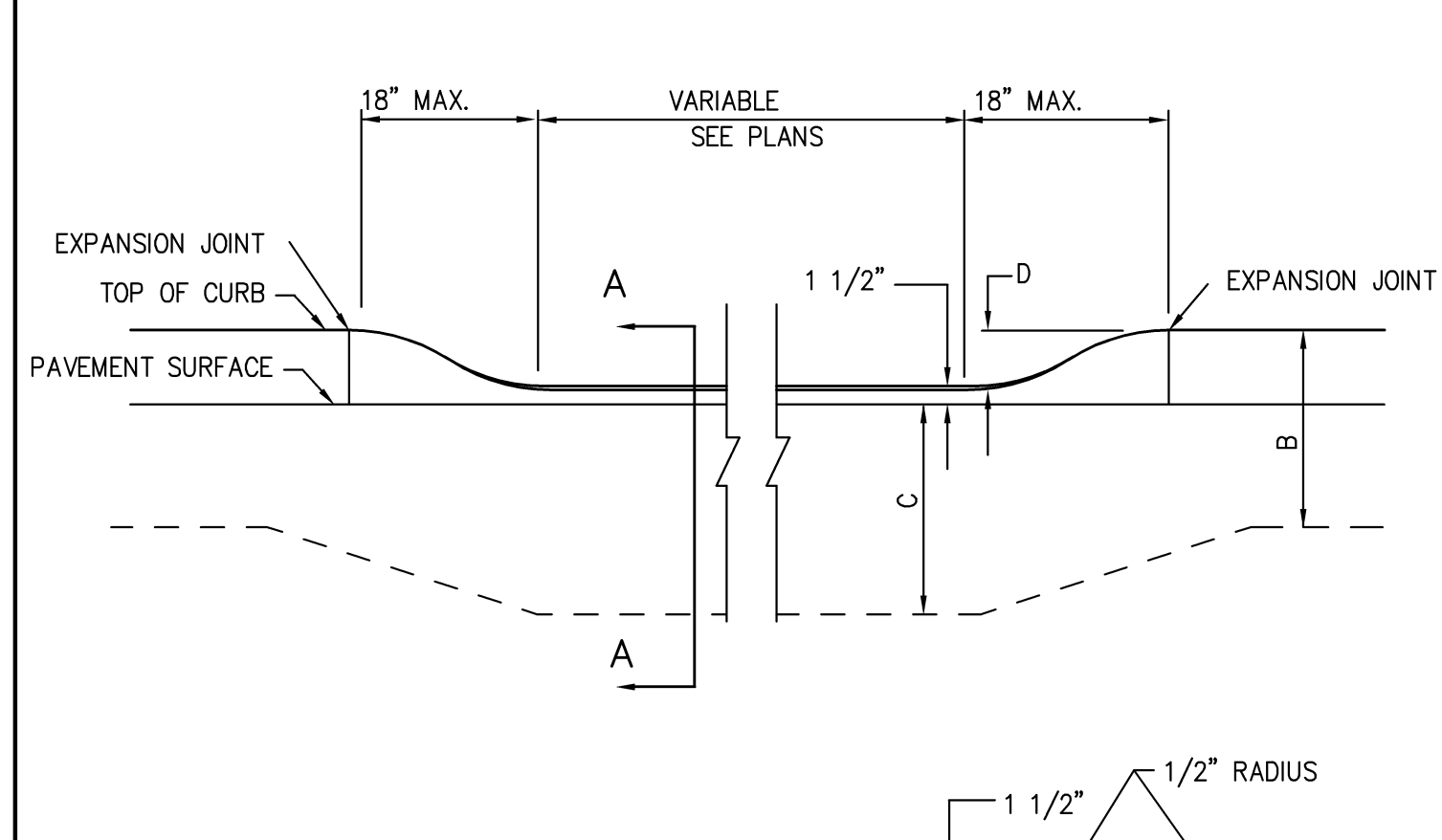
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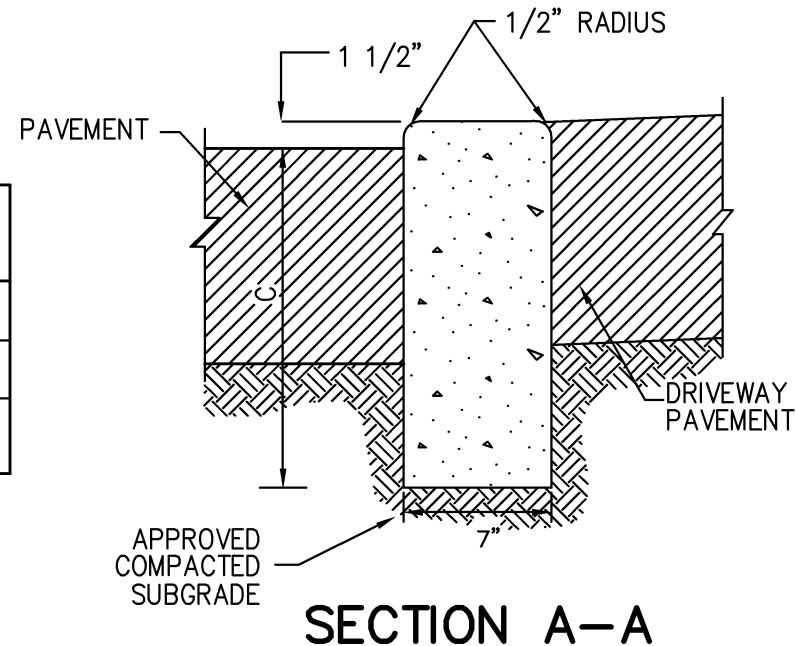
**NYSDOT TYPE VF150 CONCRETE CURB**

(ITEM 609.0401)

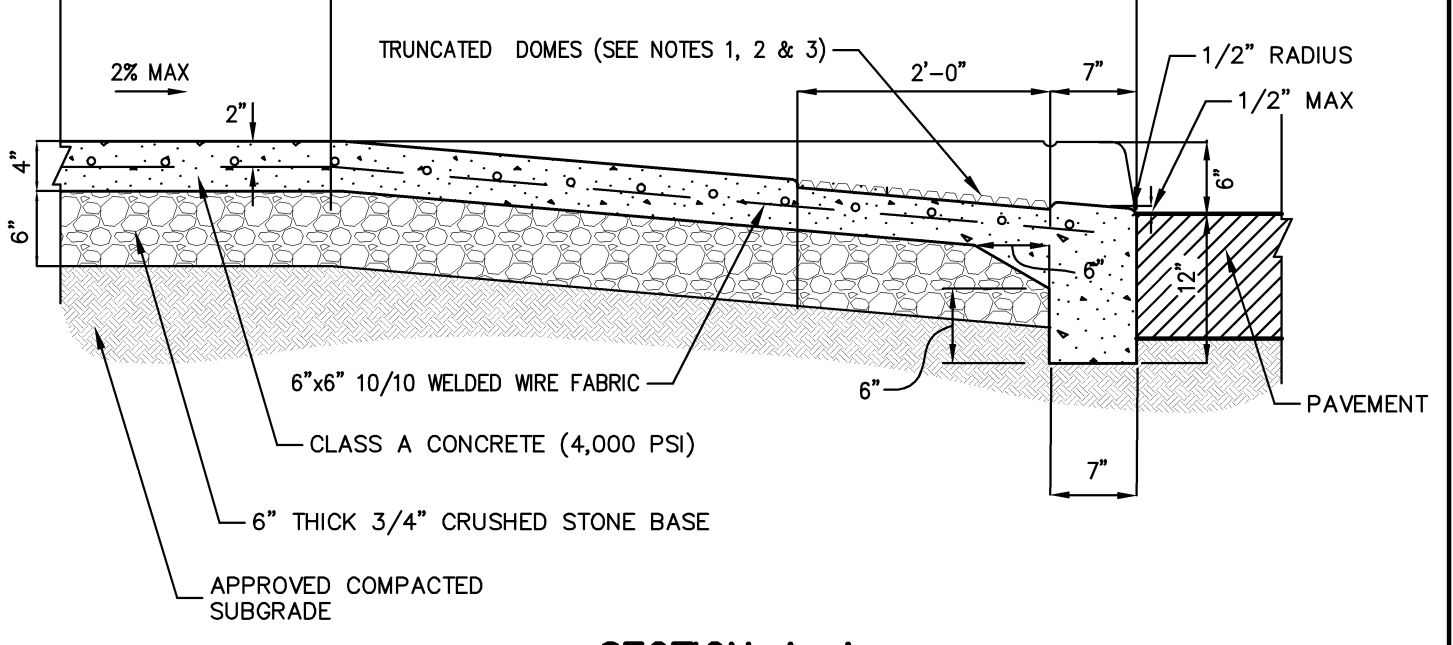
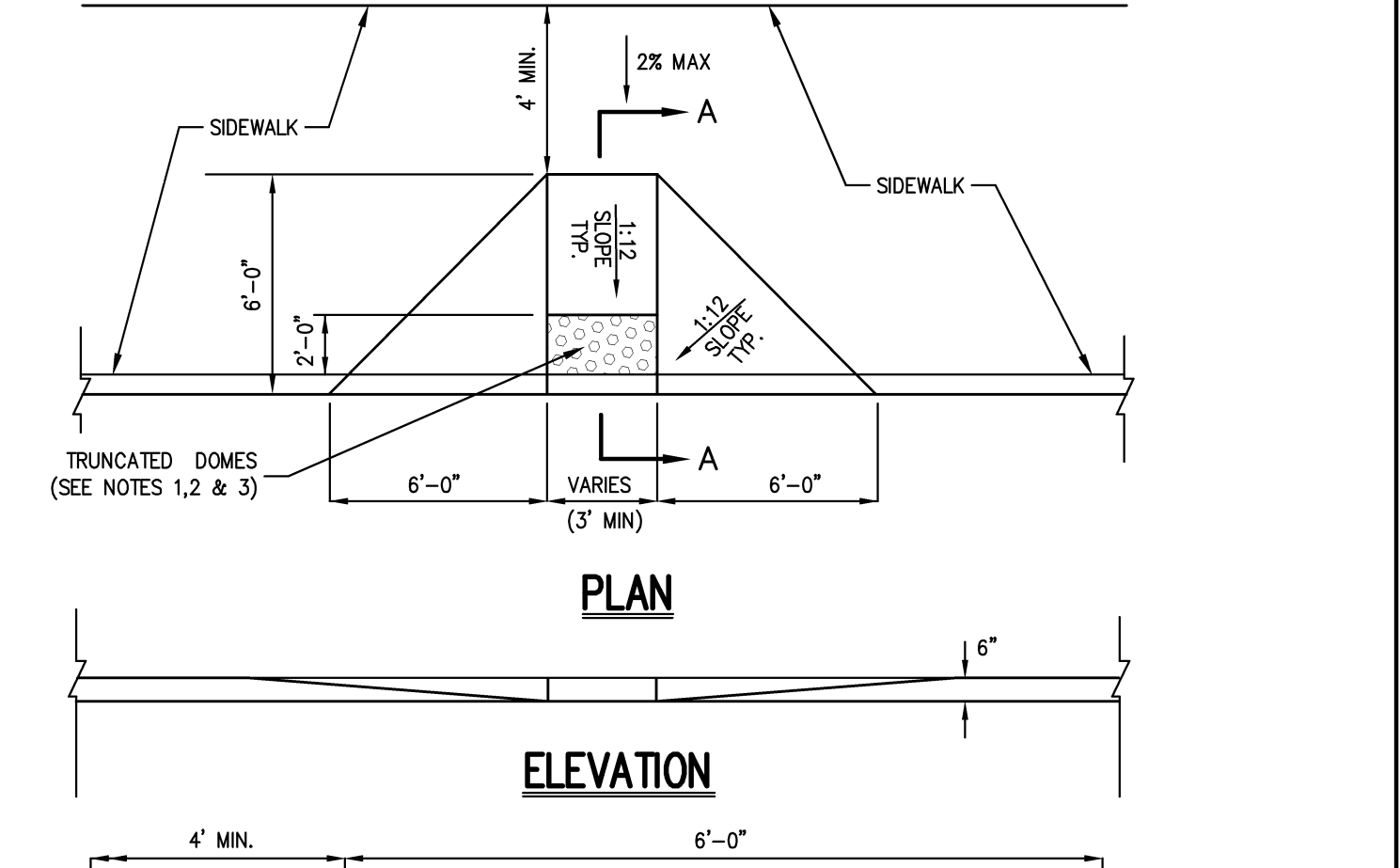
**36**



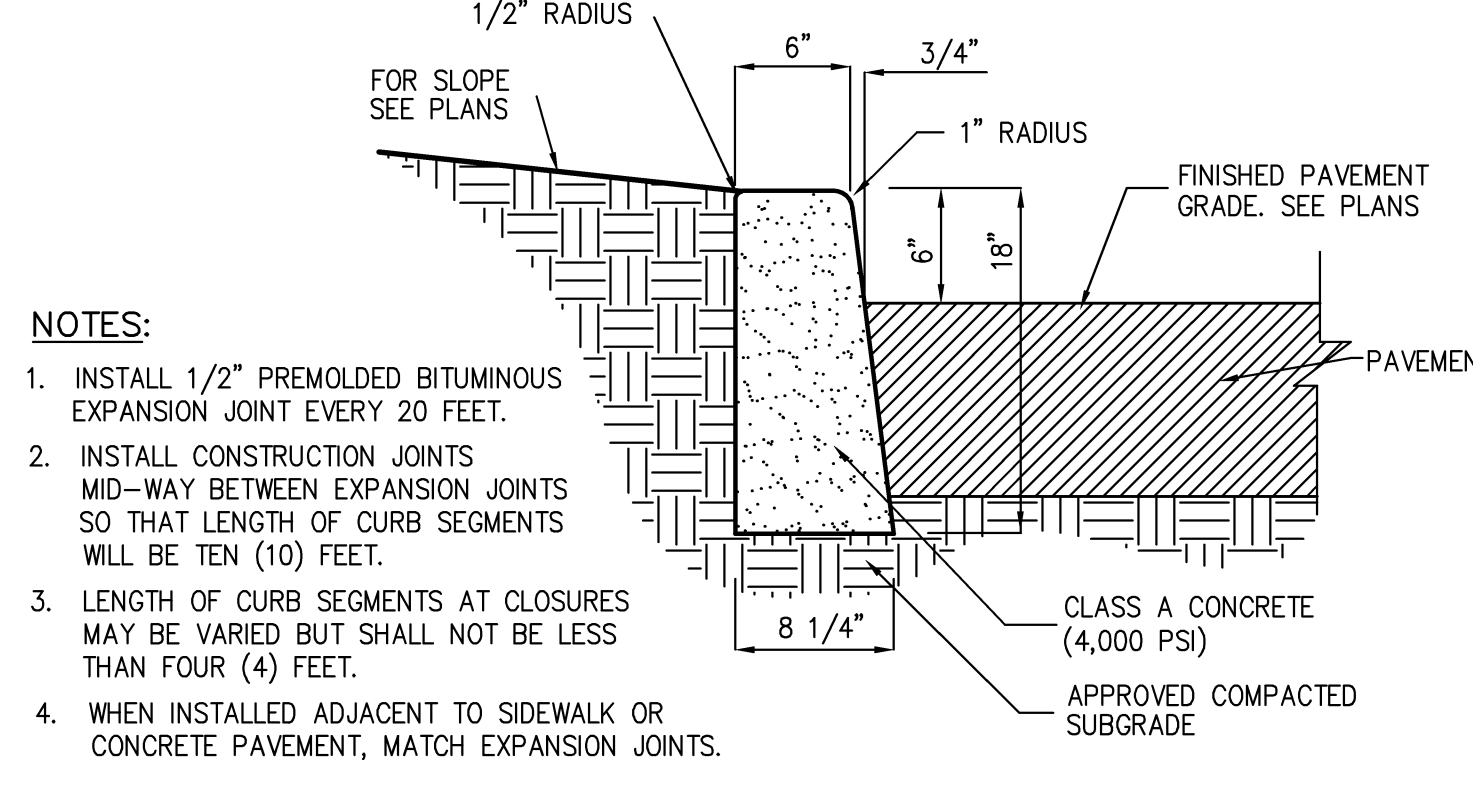
CURB SIZE	DIM. B	DIM. C	DIM. D
7" X 18"	18"	16"	4 1/2"
9" X 20"	20"	18"	4 1/2"
9" X 22"	22"	20"	4 1/2"



**METHOD OF DEPRESSING CURB AT DRIVEWAYS**



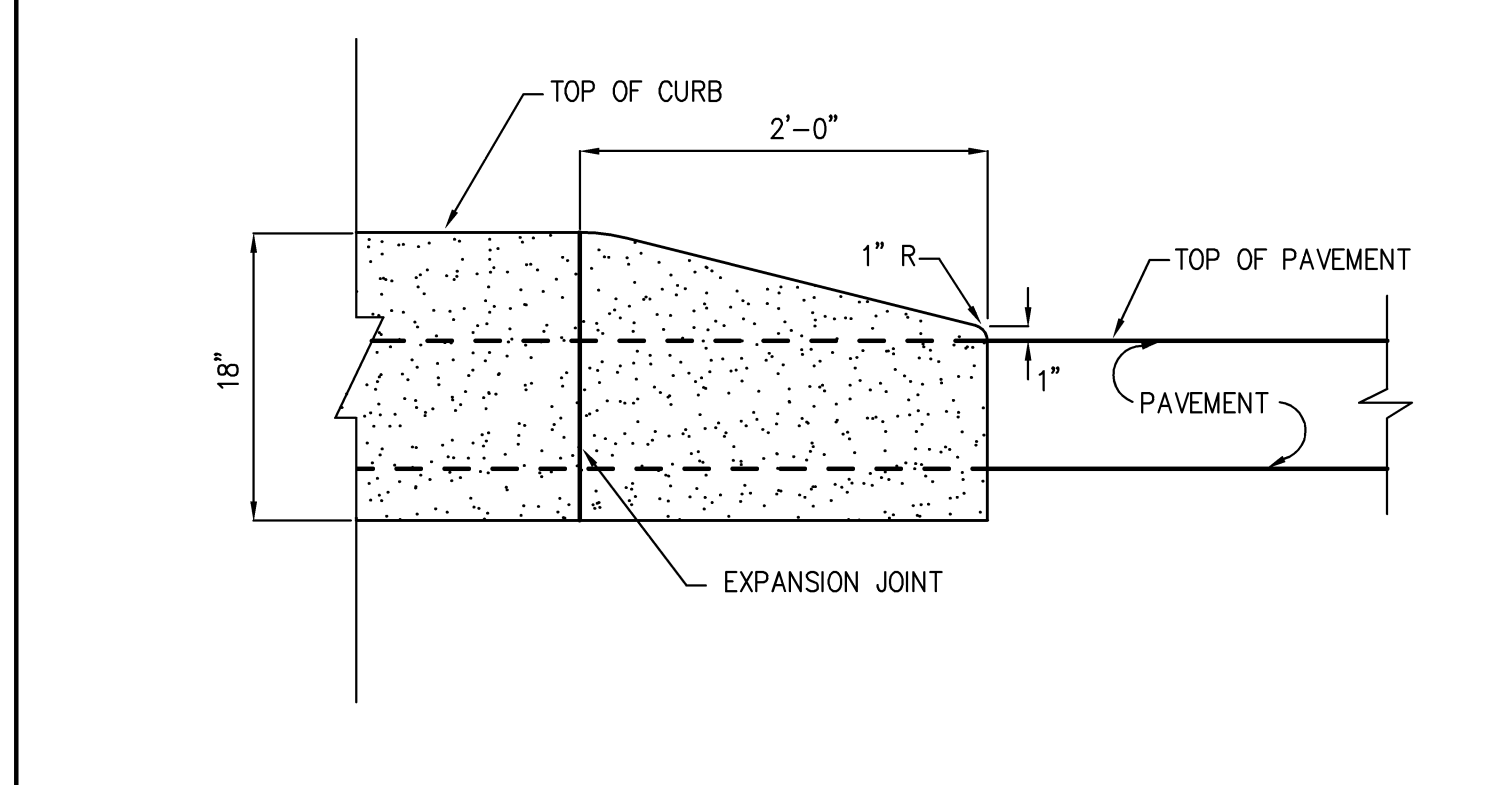
- NOTES:**
- RAMP AND SIDE RAMP SHALL HAVE DETECTABLE WARNINGS CONSISTING OF RAISED TRUNCATED DOMES WITH A DIAMETER OF NOMINAL 0.9 INCHES, A HEIGHT OF NOMINAL 0.2 INCHES AND A CENTER TO CENTER SPACING OF NOMINAL 2.35 INCHES.
  - TRUNCATED DOMES SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT IN ACCORDANCE WITH SECTION 4.29.2, FEDERAL REGISTER, VOLUME 56, NO. 144, RULES AND REGULATIONS, APPENDIX A TO PART 36 - STANDARDS FOR ACCESSIBLE DESIGN (AMERICANS WITH DISABILITIES ACT), DATED JULY 26, 1991 AND SUBSEQUENT REVISIONS.
  - 24" WIDTH OF TRUNCATED DOMES TO BE INSTALLED DIRECTLY BEHIND CURB.



- NOTES:**
- INSTALL 1/2" PREMOLDED BITUMINOUS EXPANSION JOINT EVERY 20 FEET.
  - INSTALL CONSTRUCTION JOINTS MID-WAY BETWEEN EXPANSION JOINTS SO THAT LENGTH OF CURB SEGMENTS WILL BE TEN (10) FEET.
  - LENGTH OF CURB SEGMENTS AT CLOSURES MAY BE VARIED BUT SHALL NOT BE LESS THAN FOUR (4) FEET.
  - WHEN INSTALLED ADJACENT TO SIDEWALK OR CONCRETE PAVEMENT, MATCH EXPANSION JOINTS.

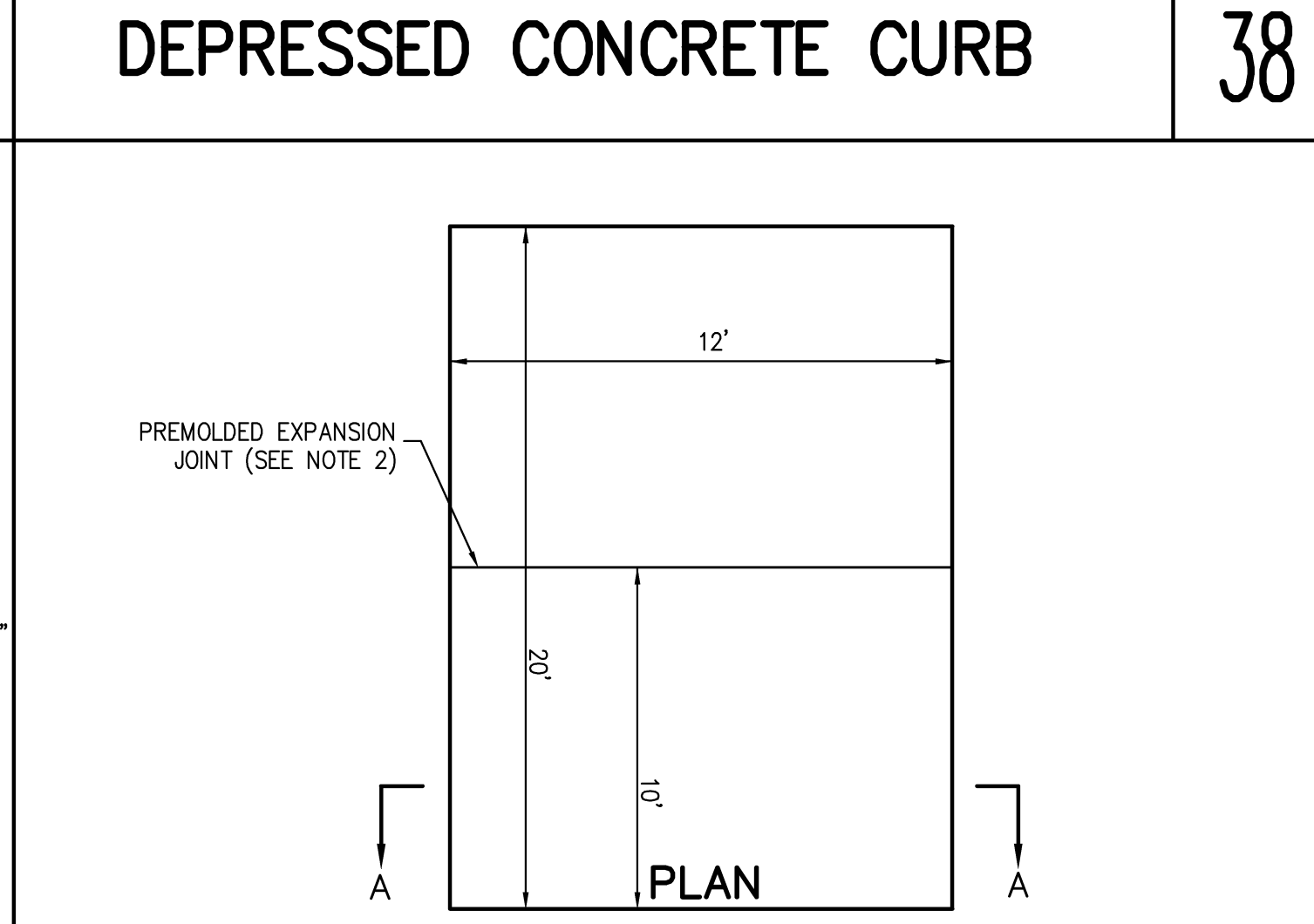
**CAST-IN-PLACE CONCRETE CURB**

**35**



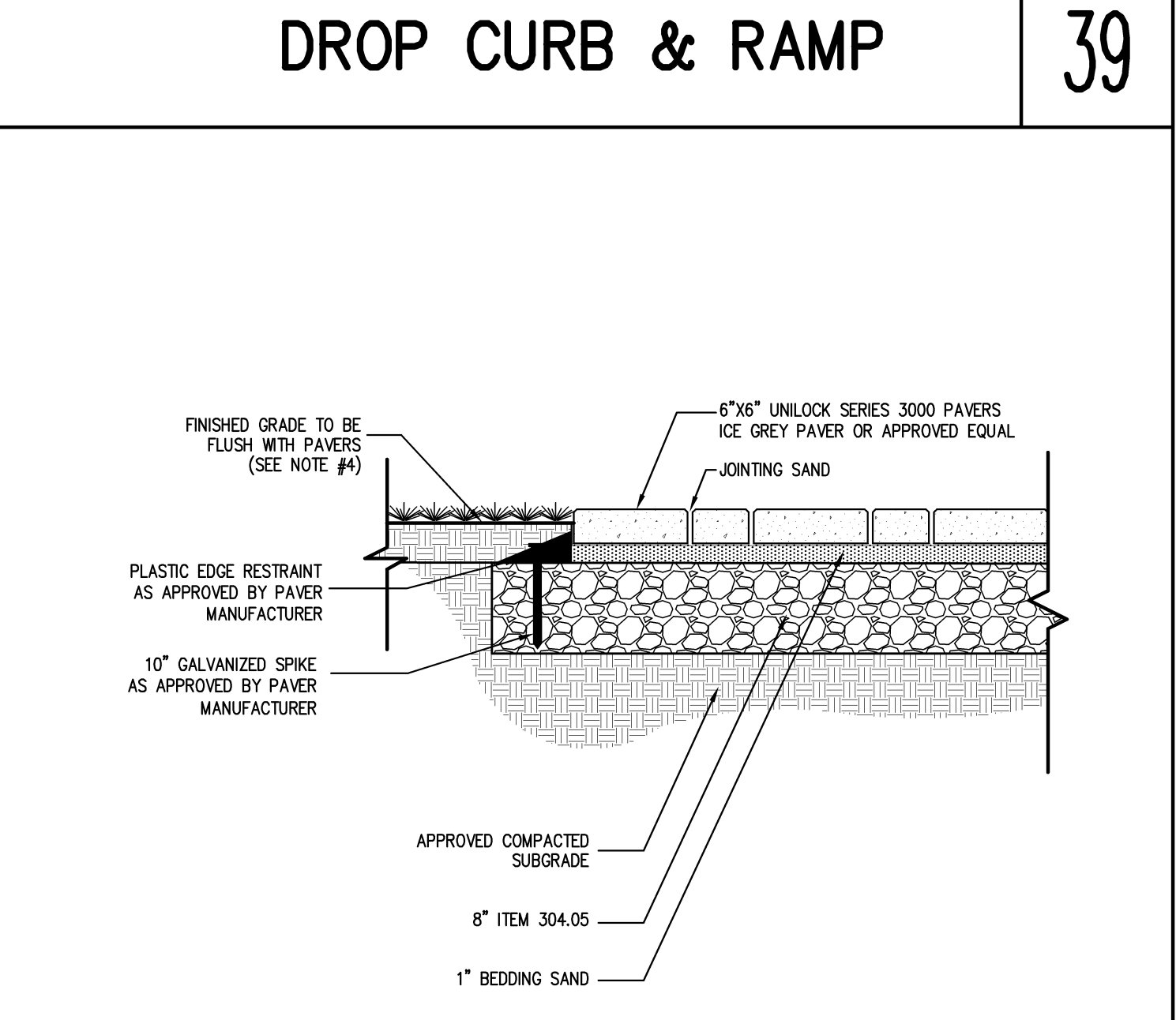
**CONCRETE CURB ENDING**

**37**



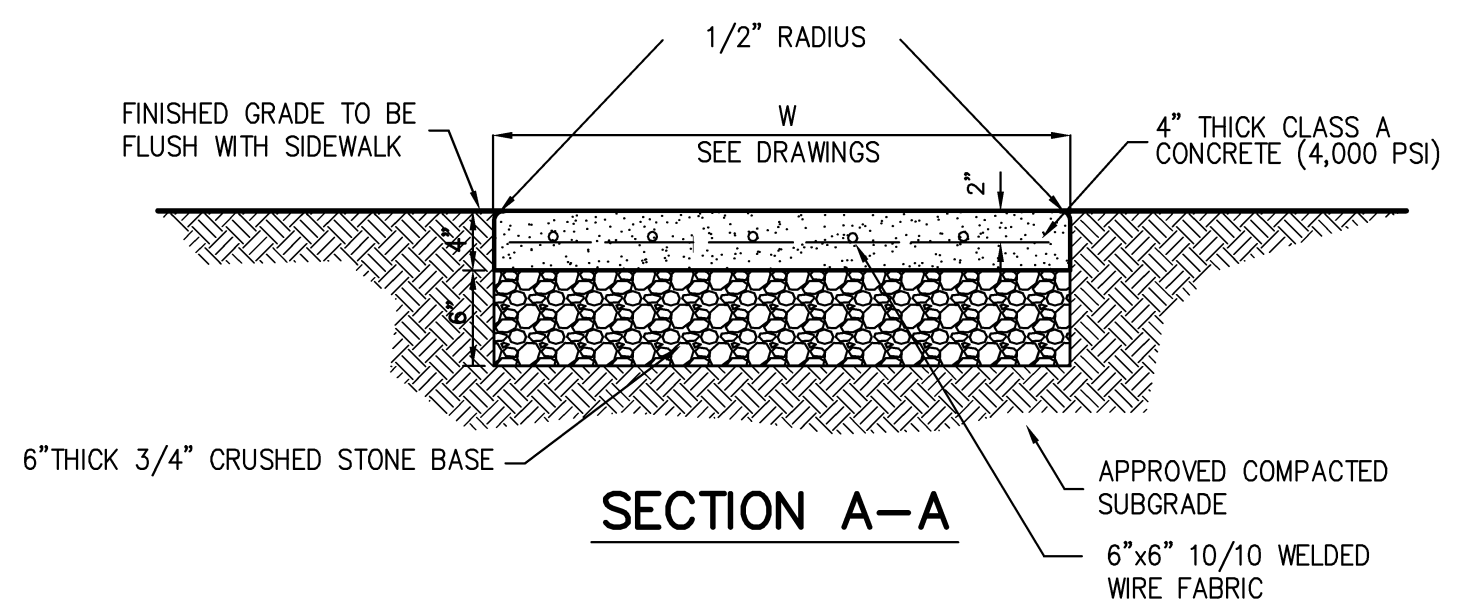
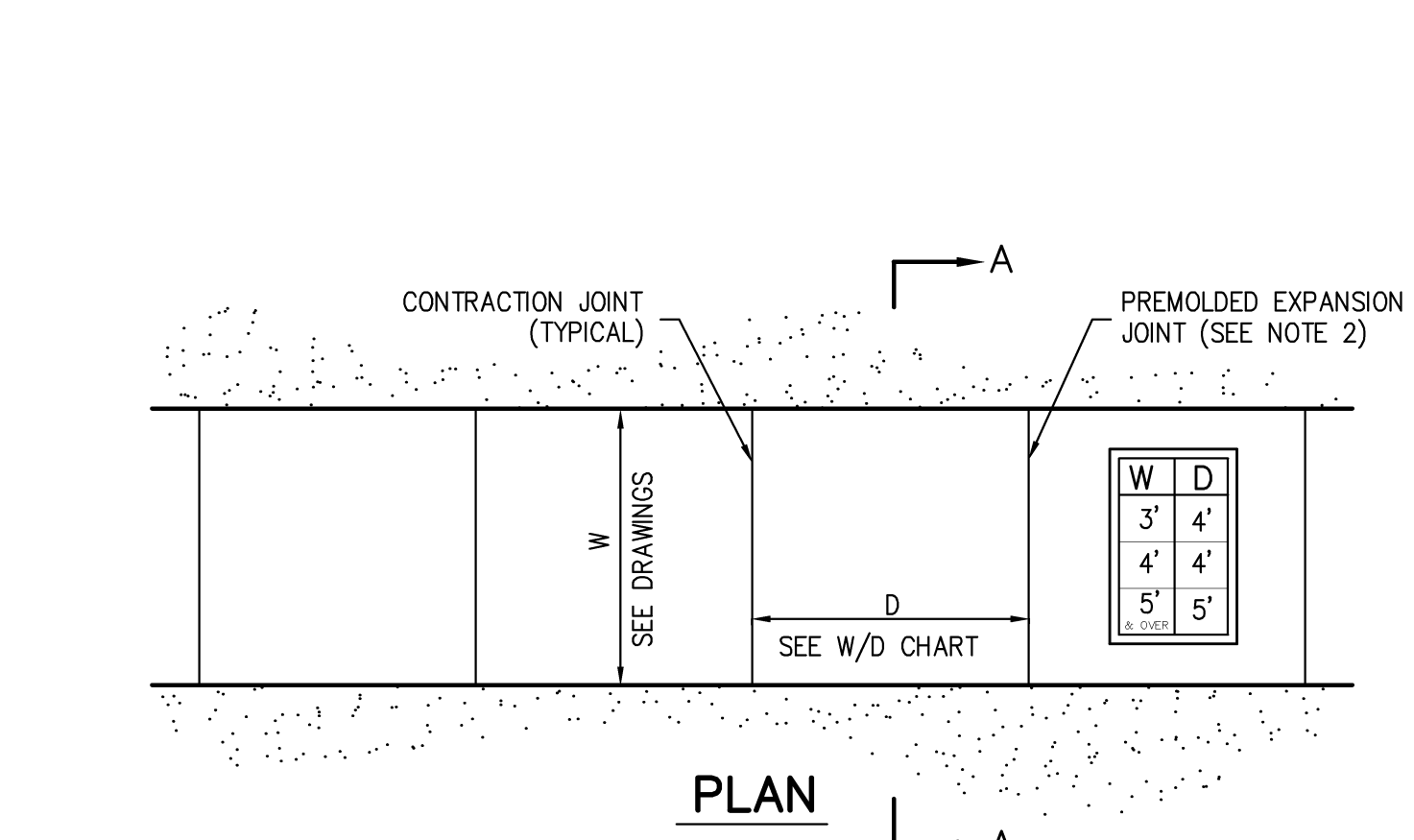
**DEPRESSED CONCRETE CURB**

**38**



**DROP CURB & RAMP**

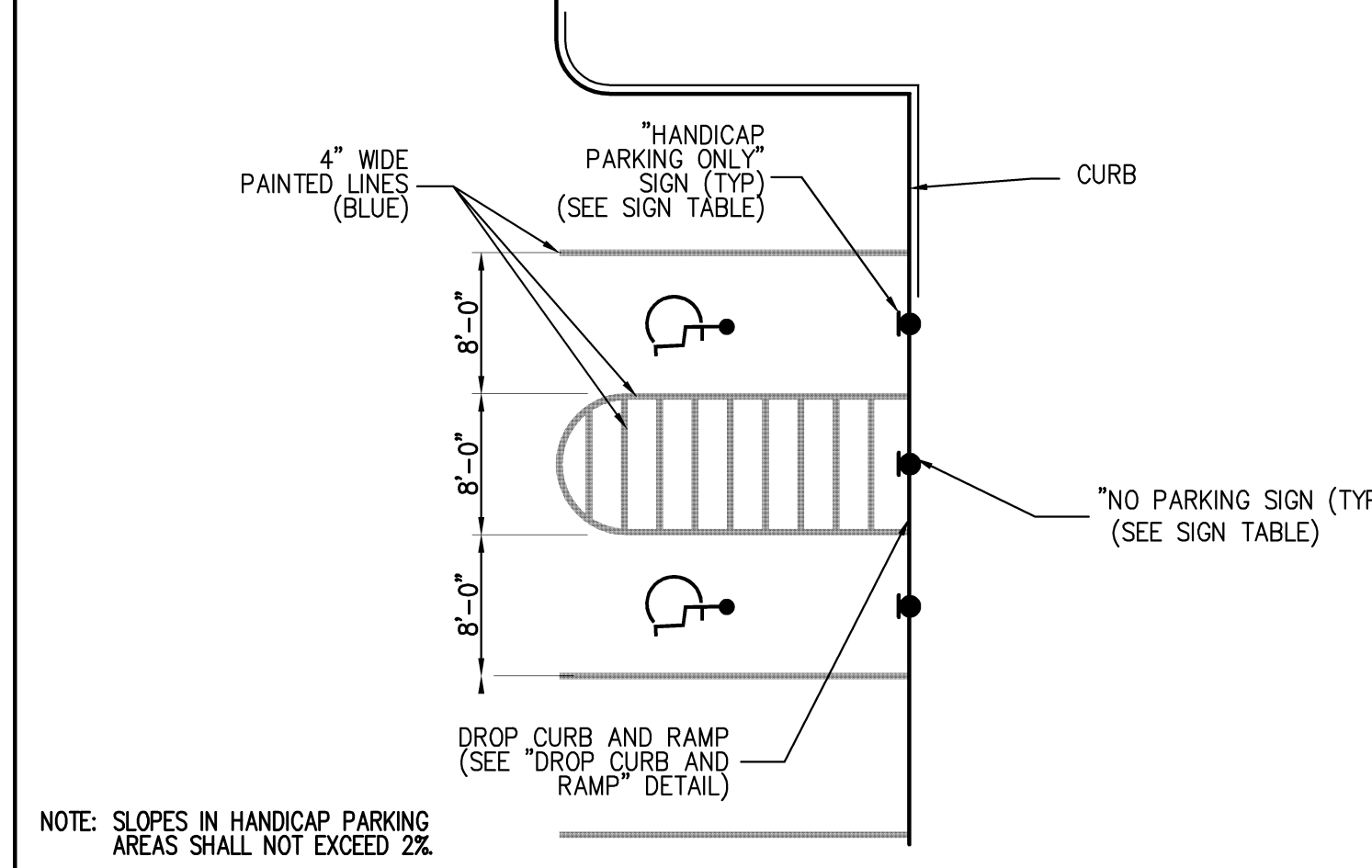
**39**



- NOTES:**
- SIDEWALK CROSS SLOPE SHALL BE 1% MIN. TO 2% MAX.
  - PROVIDE 1/2" PREMOLDED EXPANSION JOINTS AT 20' INTERVALS UNLESS OTHERWISE DIRECTED.
  - REINFORCING SHALL NOT EXTEND THROUGH EXPANSION JOINTS.
  - SIDEWALK SHALL HAVE LIGHT BROOM FINISH

**CONCRETE SIDEWALK**

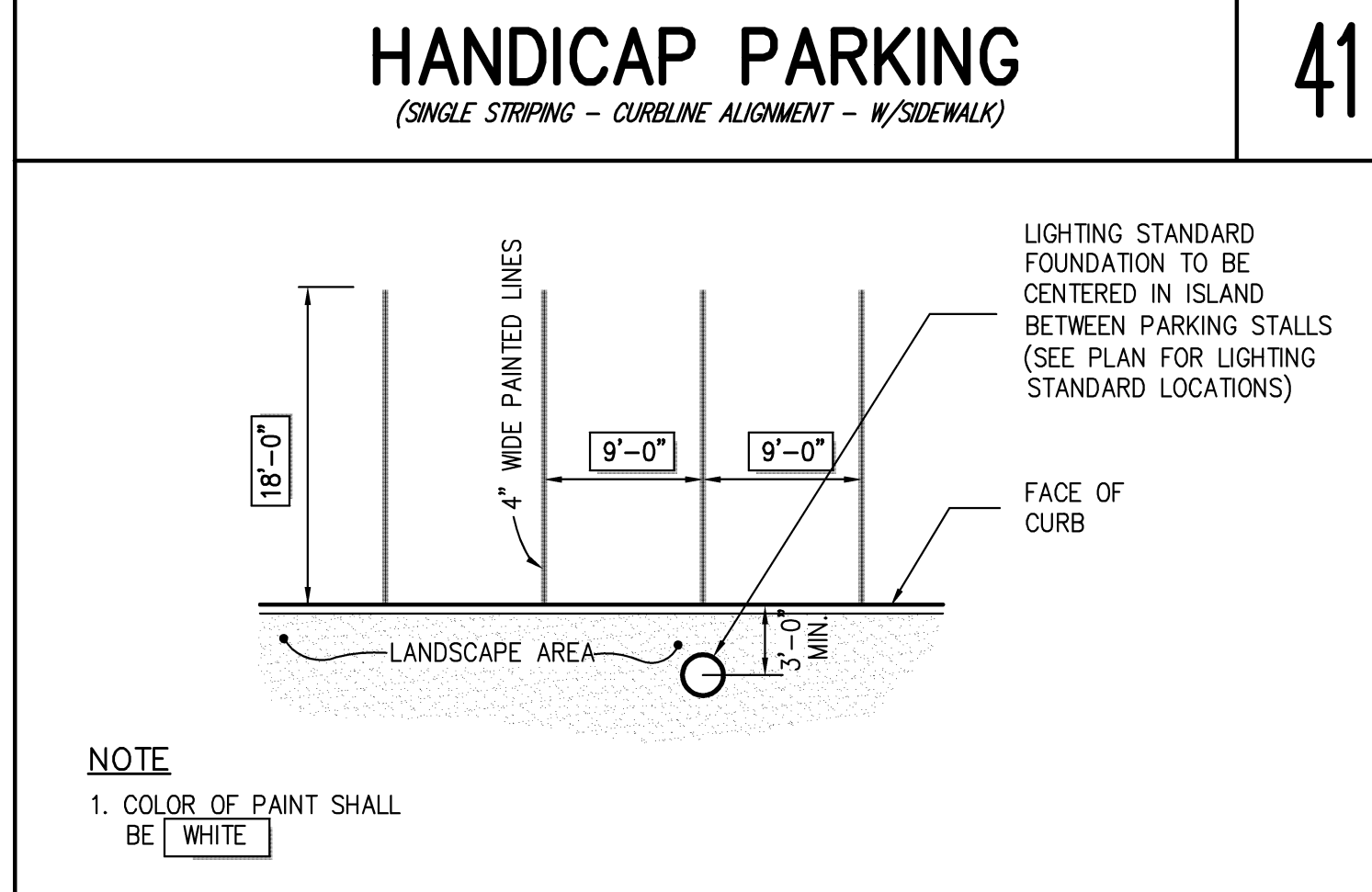
**40**



**HANDICAP PARKING**

(SINGLE STRIPING - CURBLINE ALIGNMENT - W/SIDEWALK)

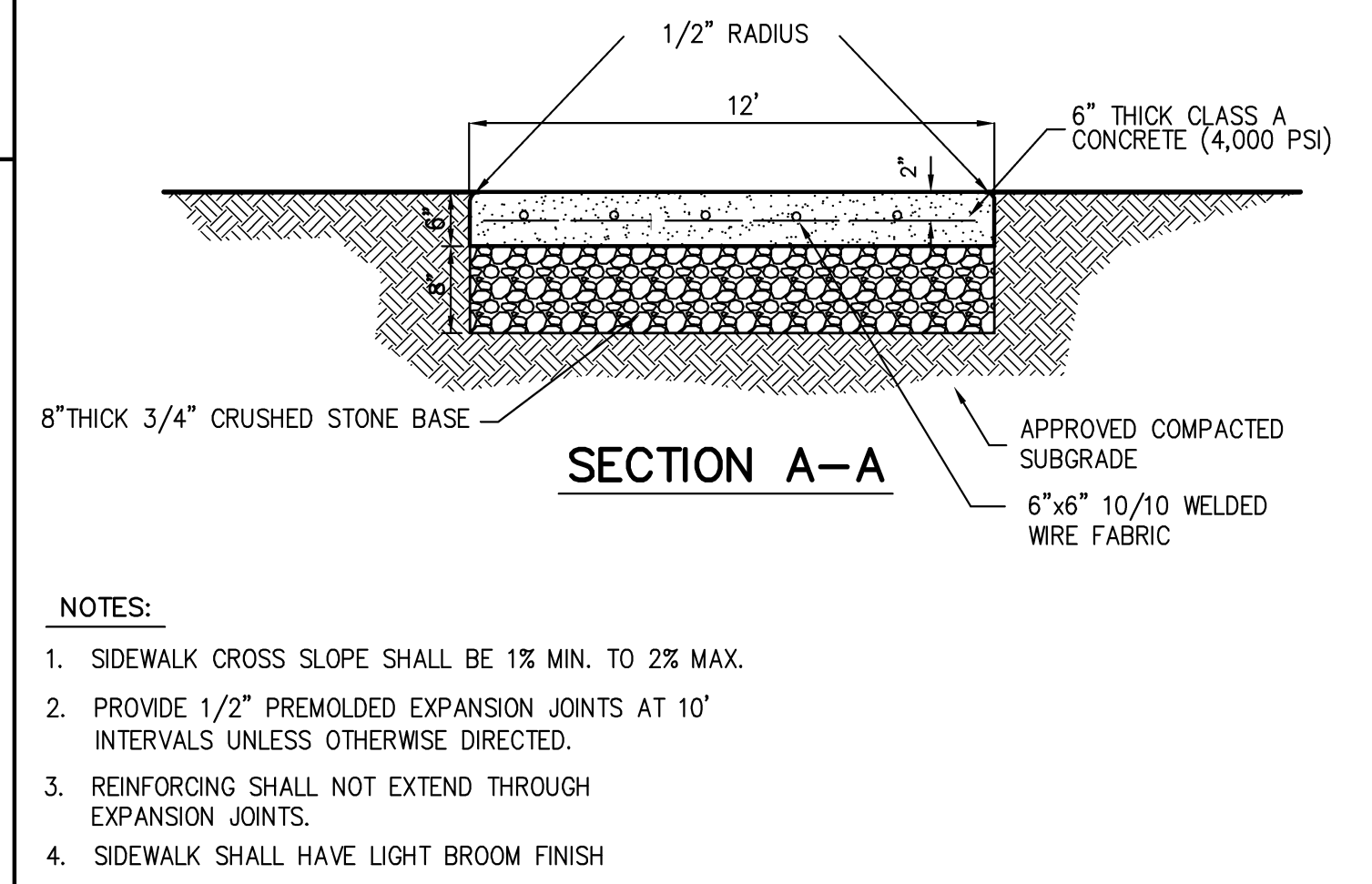
**41**



**90° PARKING**

(SINGLE STRIPING - CURBED PERIMETER)

**42**



- NOTES:**
- SIDEWALK CROSS SLOPE SHALL BE 1% MIN. TO 2% MAX.
  - PROVIDE 1/2" PREMOLDED EXPANSION JOINTS AT 10' INTERVALS UNLESS OTHERWISE DIRECTED.
  - REINFORCING SHALL NOT EXTEND THROUGH EXPANSION JOINTS.
  - SIDEWALK SHALL HAVE LIGHT BROOM FINISH

**CONCRETE DISPLAY PAD**

**43**

- NOTES:**
- MINIMUM CROSS SLOPE SHALL BE 1/4" PER FOOT UNLESS OTHERWISE INDICATED ON DRAWINGS.
  - THICKNESSES INDICATED REFER TO COMPACTED MEASURE.
  - ALL DISTURBED AREAS ADJACENT TO SIDEWALKS SHALL BE TOPSOILED (4" DEPTH) AND SEEDED WITH GRASS.

**PAVER INSTALLATION**

(PROPOSED DISPLAY AREA)

**44**

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BY	JE
DATE	03/21/2014
REVISION	
NO.	1

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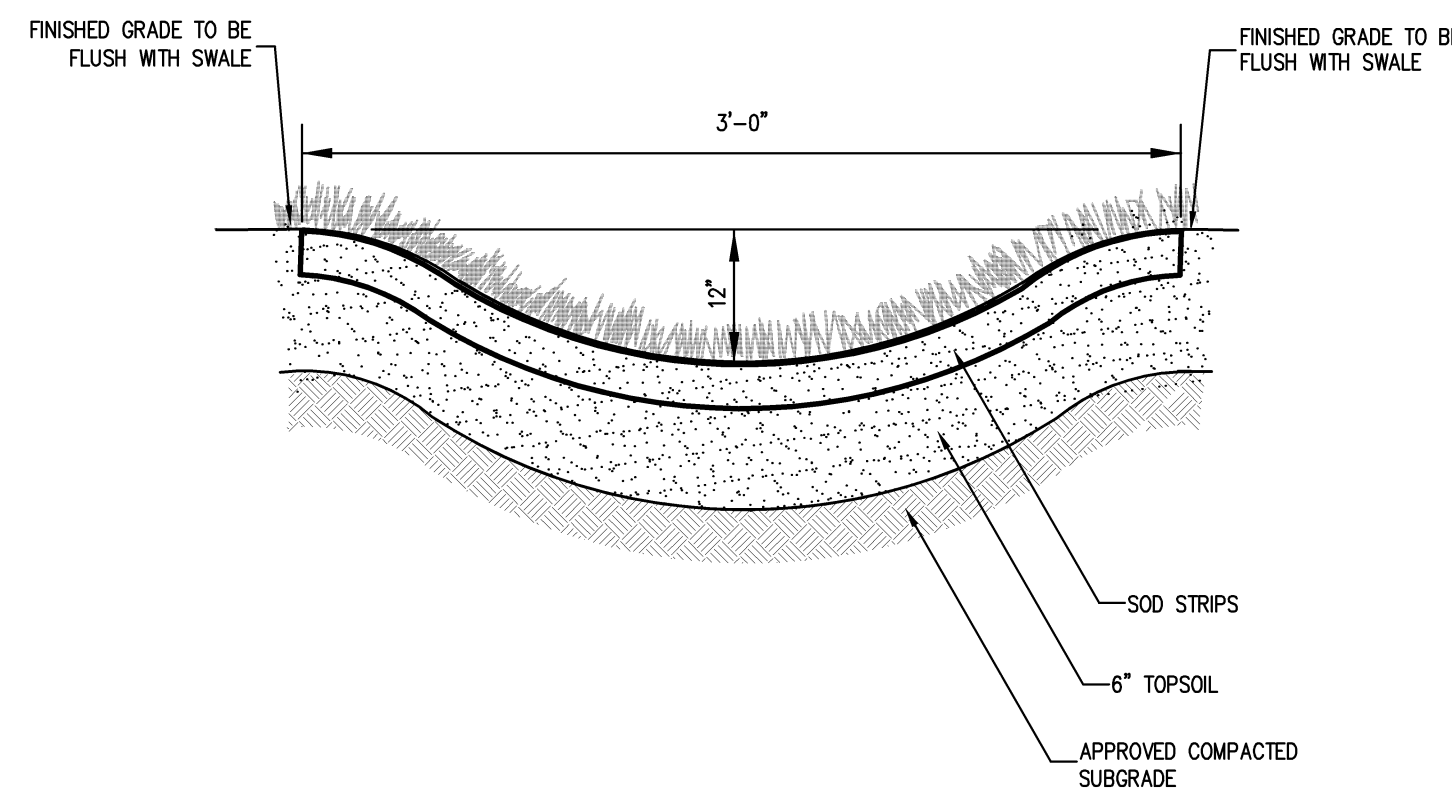
**CONSTRUCTION DETAILS**  
VOLKSWAGEN OF NEWBURGH  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

DRAWN	JE	APPROVED	RR
SCALE	N.T.S.		
DATE	11/07/2013		
PROJECT NO.	13021		
DWG. NO.	SP-14	TAB.	SP-14
DRAWING NO.	SP-14		

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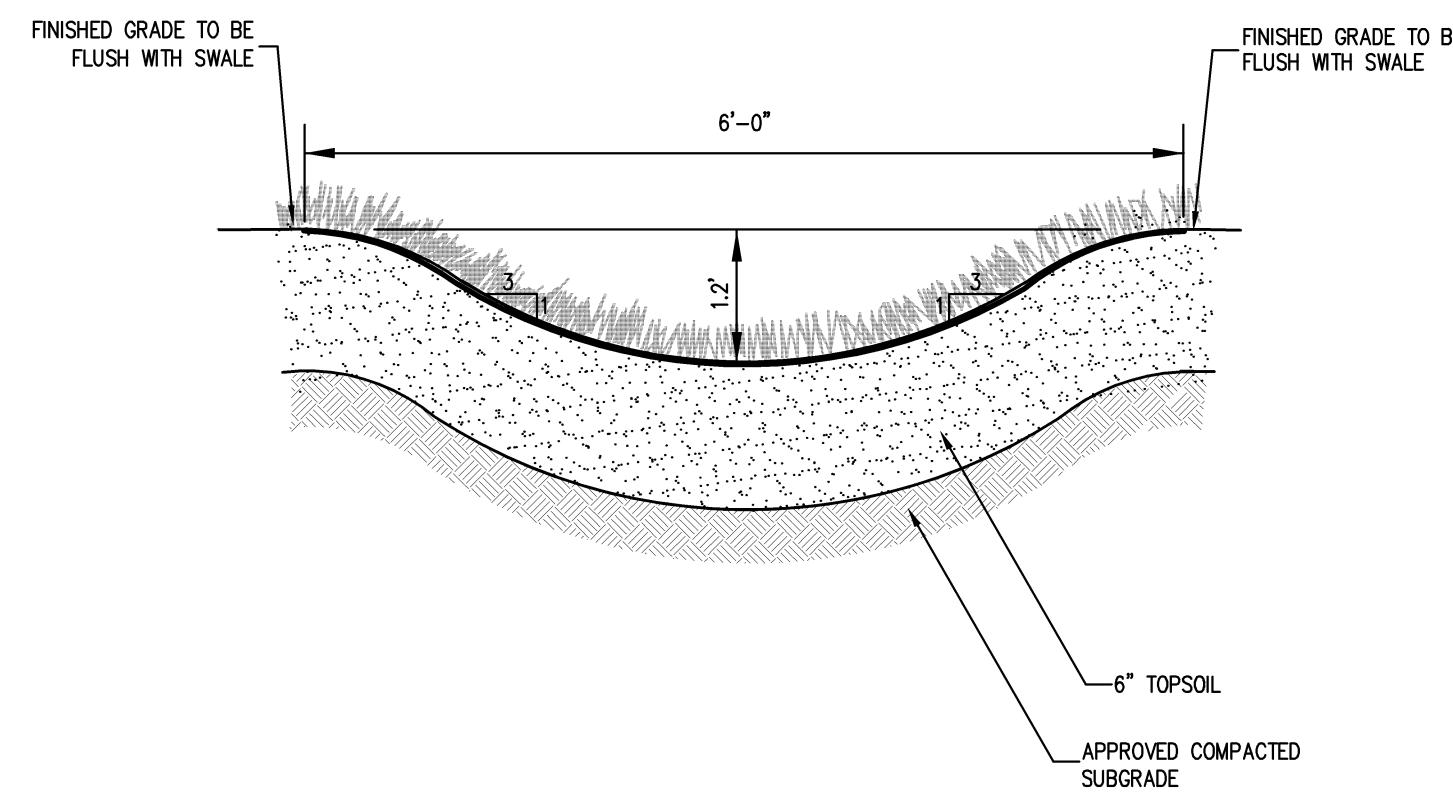






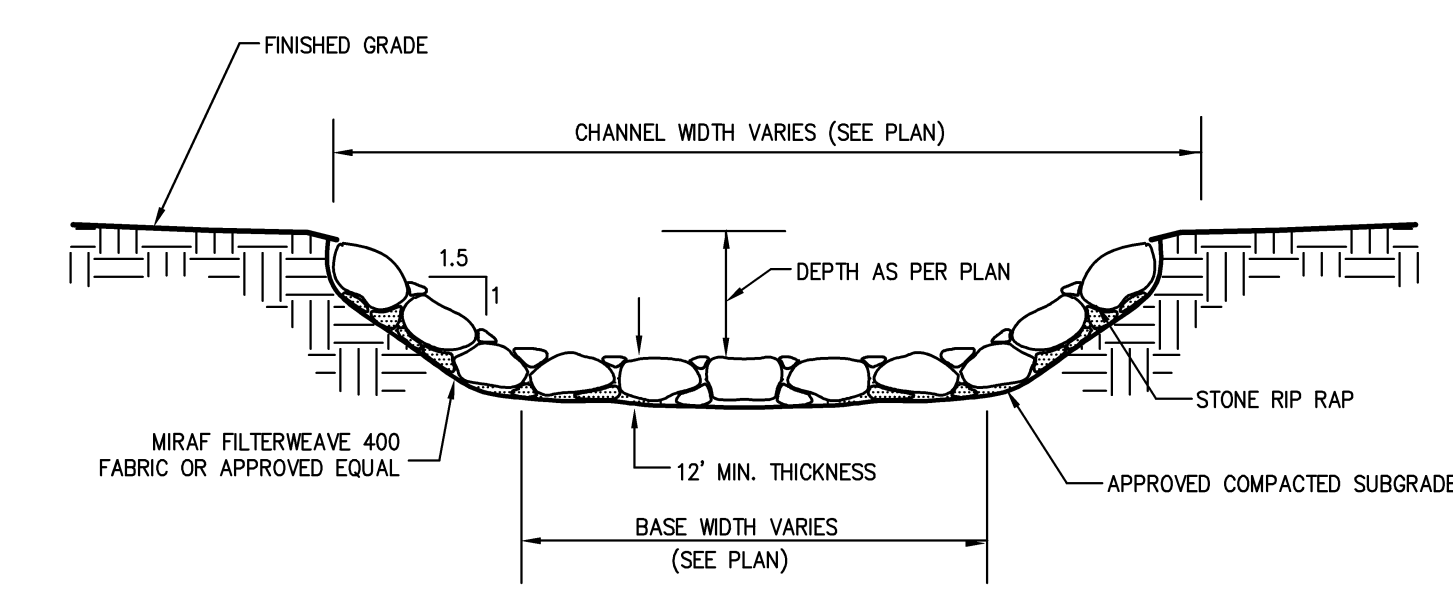
**NOTES**

1. IMMEDIATELY AFTER GRADING OPERATIONS, THE GRASS SWALE SHALL BE STABILIZED WITH SOD CONTAINING A MIXTURE OF 50% KENTUCKY BLUEGRASS, 25% CREEPING RED FESCUE AND 25% PERENNIAL RYEGRASS. SOD SHALL BE CUT IN UNIFORM WIDTH STRIPS OF LIVE GRASS.
2. FOR SWALE SLOPES EXCEEDING 8% JUTE MESH SHALL BE UTILIZED TO STABILIZE THE SWALE BASE.
3. AREA ADJACENT TO SWALE TO BE BROUGHT TO FINISHED GRADE IMMEDIATELY AS REQUIRED, TOPSOILED, SEEDED AND MAINTAINED FOR EROSION CONTROL.

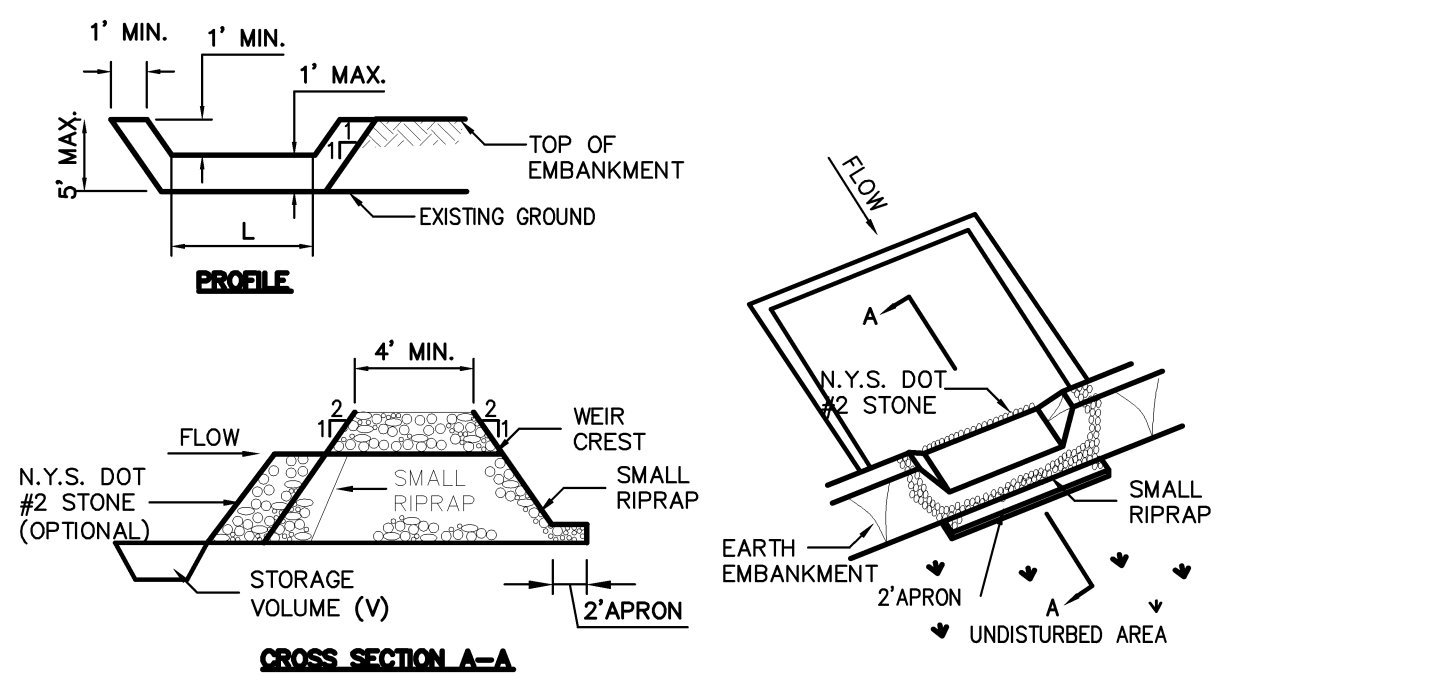


**NOTES**

1. IMMEDIATELY AFTER GRADING OPERATIONS, THE VEGETATED SWALE SHALL BE STABILIZED WITH A MIXTURE AS NOTED IN PLANT LIST
2. FOR SWALE SLOPES EXCEEDING 8% JUTE MESH SHALL BE UTILIZED TO STABILIZE THE SWALE BASE.
3. AREA ADJACENT TO SWALE TO BE BROUGHT TO FINISHED GRADE IMMEDIATELY AS REQUIRED, TOPSOILED, SEEDED AND MAINTAINED FOR EROSION CONTROL.



**NOTE:**  
AREA ADJACENT TO CHANNEL TO BE BROUGHT TO FINISHED GRADE IMMEDIATELY AS REQUIRED, TOPSOILED, SEEDED AND MAINTAINED FOR EROSION CONTROL.



TEMPORARY SEDIMENT TRAP	DRAINAGE AREA (AC)	SPILLWAY LENGTH L = 4 X DA (FT)	STORAGE VOLUME REQUIRED V = 3600 X DA (CF)	STORAGE VOLUME PROVIDED (CF)	CLEANOUT VOLUME (CF)
1	1.09	4.50	3,924	4,000	2,000
4	0.59	2.50	2,154	2,200	1,100

OPTION: A ONE FOOT LAYER OF N.Y.S. DOT #2 STONE MAY BE PLACED ON THE UPSTREAM SIDE OF THE RIPRAP IN PLACE OF THE EMBEDDED FILTER CLOTH.

- CONSTRUCTION SPECIFICATIONS**
1. AREA UNDER EMBANKMENT SHALL BE CLEARED, GRUBBED AND STRIPPED OF ANY VEGETATION AND ROOT MAT. THE POOL AREA SHALL BE CLEARED.
  2. THE FILL MATERIAL FOR THE EMBANKMENT SHALL BE FREE OF ROOTS AND OTHER WOODY VEGETATION AS WELL AS OVER-SIZED STONES, ROCKS, ORGANIC MATERIAL OR OTHER OBJECTIONABLE MATERIAL. THE EMBANKMENT SHALL BE COMPACTED BY TRAVERSING WITH EQUIPMENT WHILE IT IS BEING CONSTRUCTED.
  3. ALL CUT AND FILL SLOPES SHALL BE 2:1 OR FLATTER.
  4. THE STONE USED IN THE OUTLET SHALL BE SMALL RIPRAP 4"-8" ALONG WITH A 1" THICKNESS OF 2" AGGREGATE PLACED ON THE UP-GRADE SIDE ON THE SMALL RIPRAP OR EMBEDDED FILTER CLOTH IN THE RIPRAP.
  5. SEDIMENT SHALL BE REMOVED AND TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO 1/2 THE DESIGN DEPTH OF THE TRAP.
  6. THE STRUCTURE SHALL BE INSPECTED AFTER EACH RAIN AND REPAIRS MADE AS NEEDED.
  7. CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION IS MINIMIZED.
  8. THE STRUCTURE SHALL BE REMOVED AND THE AREA STABILIZED WHEN THE DRAINAGE AREA HAS BEEN PROPERLY STABILIZED.
- MAXIMUM DRAINAGE AREA 5 ACRES

GRASS SWALE

53

VEGETATED SWALE

54

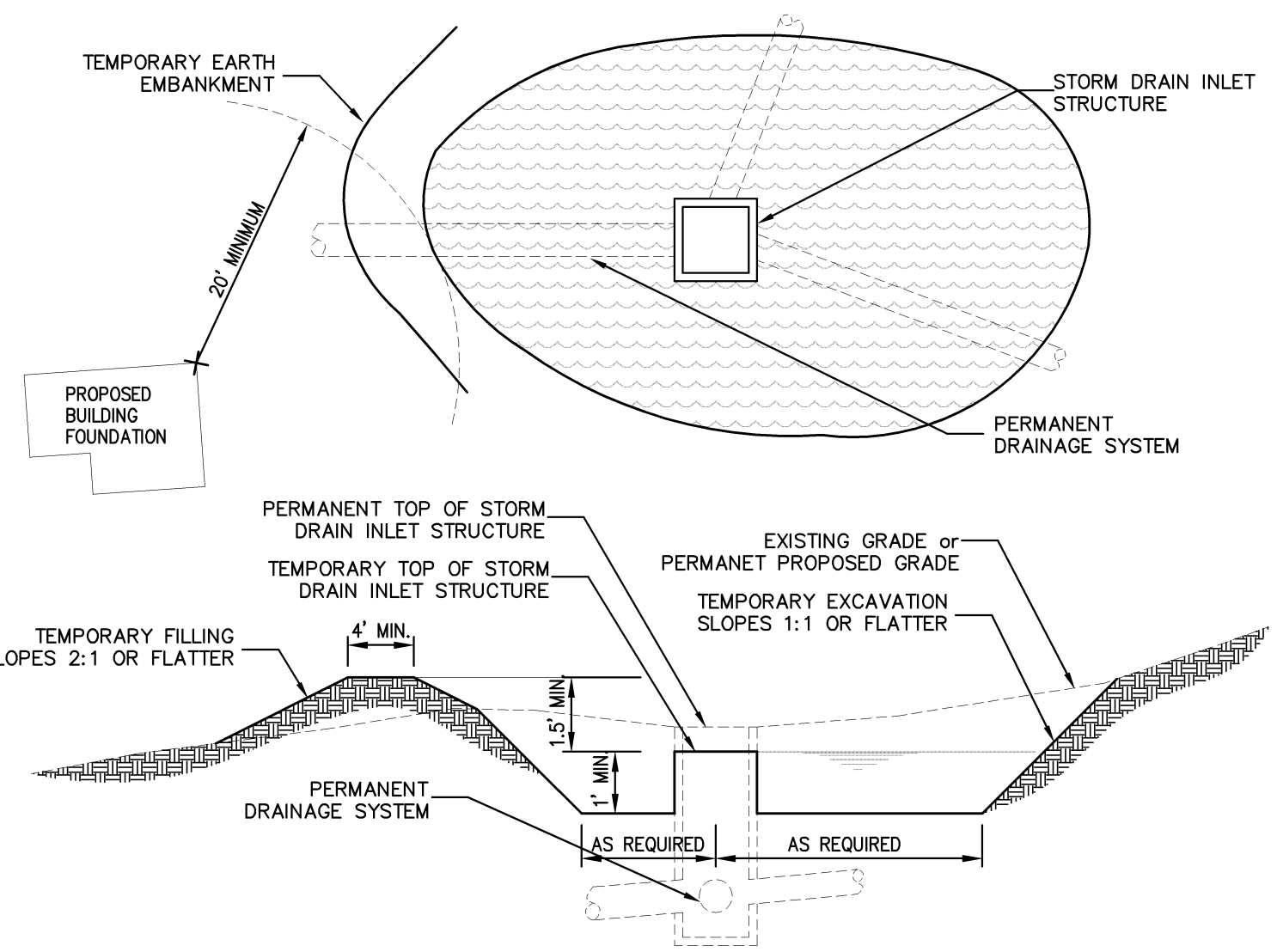
STONE RIP RAP CHANNEL

55

STONE OUTLET SEDIMENT TRAP

56

TEMPORARY SEDIMENT TRAP	TYPE OF TRAP	DRAINAGE AREA (AC)	STORAGE REQUIRED (CF)	STORAGE PROVIDED (CF)	ELEVATIONS (L.F.)			
					OUTLET INVERT	BOTTOM OF TRAP	1/2 TOP OF CLEANOUT	TOP OF DRAINAGE STRUCTURE
3	ST-III	0.63	2,268	2,300	413.70	413.70	415.00	417.20
5	ST-III	0.47	1,678	1,700	409.50	409.50	411.00	417.20



EMBANKMENT SECTION THRU RISER

**CONSTRUCTION SPECIFICATIONS**

1. SEDIMENT SHALL BE REMOVED AND THE TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO 1/2 THE DESIGN DEPTH OF THE TRAP. REMOVED SEDIMENT SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
2. THE VOLUME OF SEDIMENT STORAGE SHALL BE 3600 CUBIC FEET PER ACRE OF CONTRIBUTORY DRAINAGE.
3. THE STRUCTURE SHALL BE INSPECTED AFTER EACH RAIN AND REPAIRS MADE AS NEEDED.
4. CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION SHALL BE MINIMIZED.
5. THE SEDIMENT TRAP SHALL BE REMOVED AND THE AREA STABILIZED WHEN THE CONSTRUCTED DRAINAGE AREA HAS BEEN PROPERLY STABILIZED.

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CATCH BASIN SEDIMENT TRAP ST-III

57

X

X

X

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1.	REVISED	03/21/2014	JE

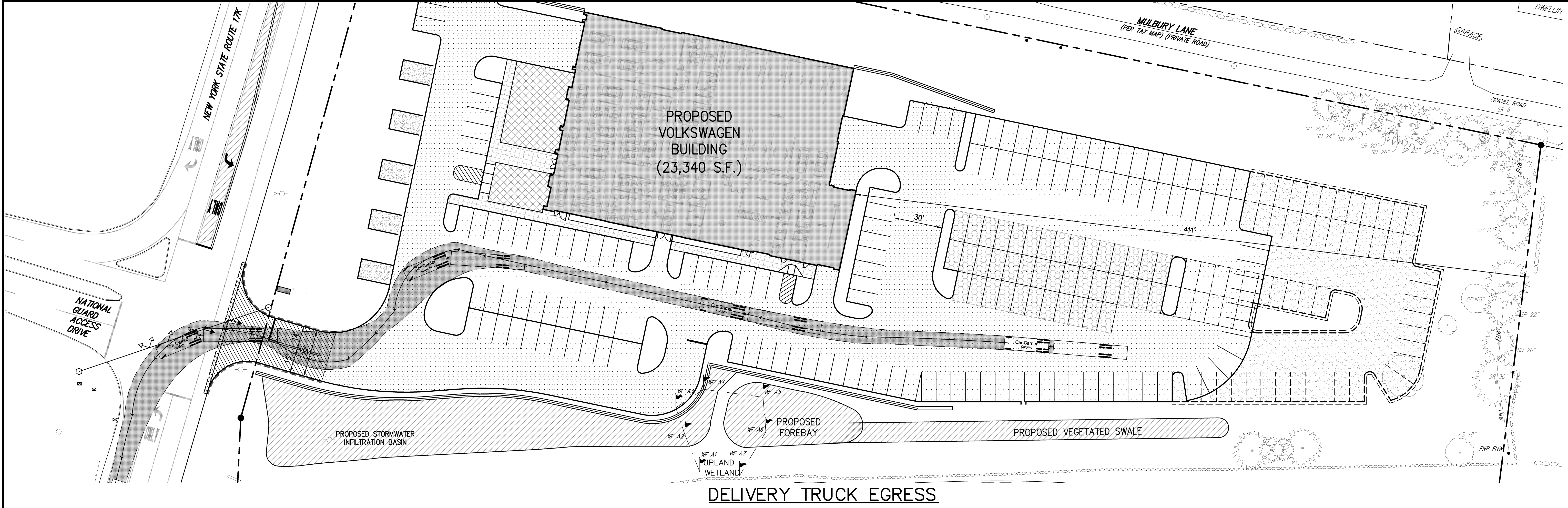
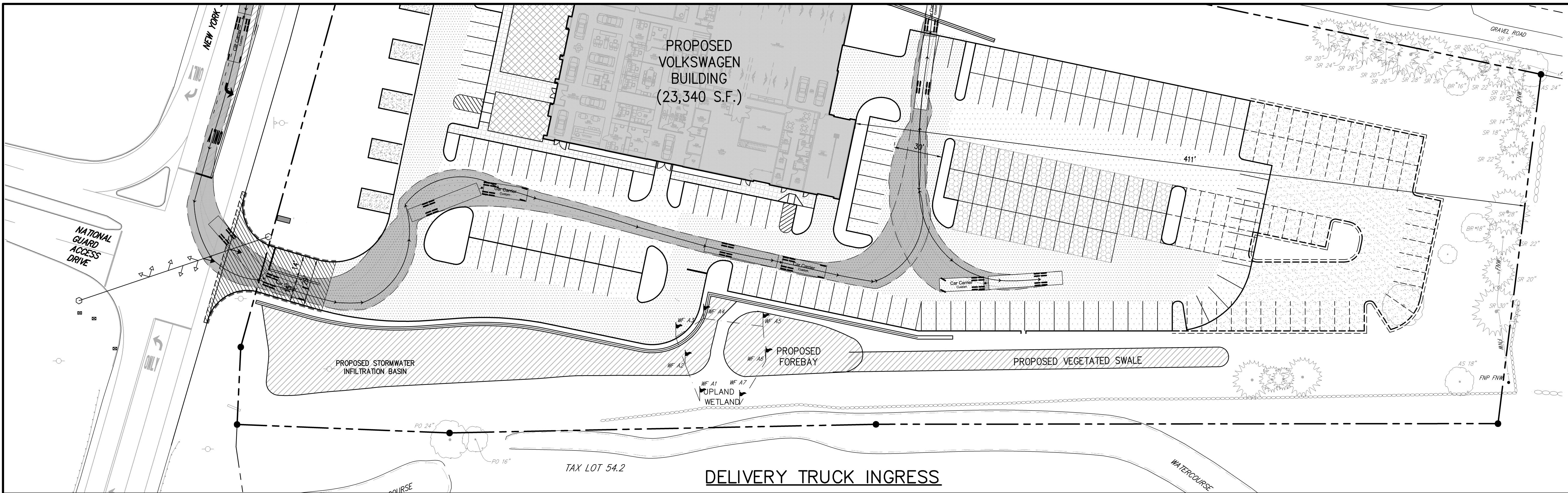
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voice 914.273.5225 • fax 914.273.2102  
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**CONSTRUCTION DETAILS**

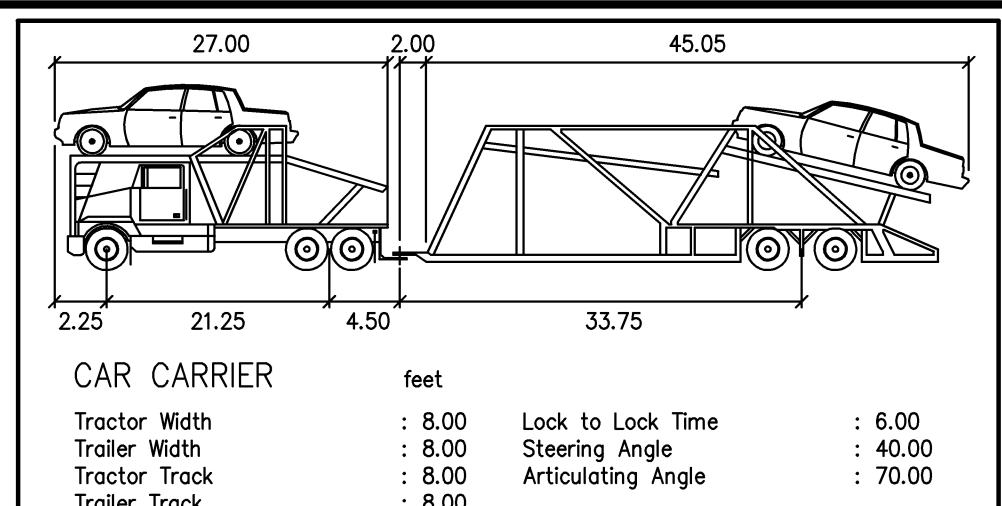
VOLKSWAGEN OF NEWBURGH  
ROUTE 17K VW DEALERSHIP  
TOWN OF NEWBURGH, NEW YORK

DATE:	11/07/2013
PROJECT No:	13021
DWG. NO.:	SP-16
SCALE:	N.T.S.
DRAWN BY:	JE
APPROVED BY:	RR
<b>SP-16</b>	



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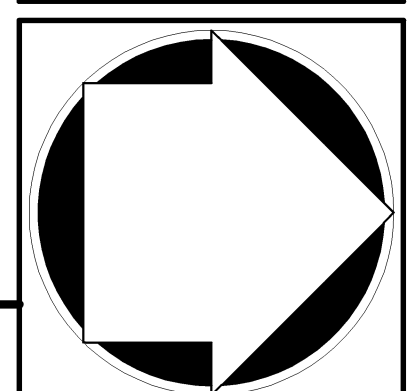
ROUTE 17 CARS, LLC  
 1143 DUTCHESS TURNPIKE  
 POUGHKEEPSIE, NEW YORK 12603

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 Armonk, NY 10504  
 voice 914.273.5225 • fax 914.273.1102  
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TRUCK TURNING  
 ANALYSIS PLAN

VOLKSWAGEN OF NEWBURGH  
 ROUTE 17K VW DEALERSHIP  
 TOWN OF NEWBURGH, NEW YORK



DRWG: MTP APPROVED: RR  
 SCALE: 1" = 20'  
 DATE: 11/07/2013  
 PROJECT NO: 13021  
 DWG: TMR LS  
 SITE TRUCK ACCESS/TURNING SITE: TRUCK  
 DRAWING NO: SP-17