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**TOWN OF NEWBURGH
PLANNING BOARD
TECHNICAL REVIEW COMMENTS**

PROJECT: U.S. CRANE & RIGGING
PROJECT NO.: 2016-14
PROJECT LOCATION: SECTION 97, BLOCK 1, LOT 21.2
REVIEW DATE: 10 MARCH 2017
MEETING DATE: 16 MARCH 2017
PROJECT REPRESENTATIVE: MASER CONSULTING

1. The site has been redesigned to provide an emergency access only from Stewart Ave.. this access will be gated. Comments from Jurisdictional Fire Department regarding the access road should be received.
2. City of Newburgh Flow Acceptance letter for the new building must be received. Planning Board can take no action on the project until City of Newburgh Flow Acceptance letter is received. Proposed hydraulic loading from the structure should be identified in a narrative report submitted to the Town Engineer for processing with the City of Newburgh.
3. Crane and truck parking areas have been delineated on the site. The balance of the site will be utilized for storage of passenger vehicles by the current site operator. Expansion of the contractor yard/ LHI use beyond those depicted on the site plan would require review and approval by the Planning Board for future expansions.
4. Future submissions should incorporate architectural renderings for architectural review by the Planning Board.
5. A review of the proposed sanitary sewer lateral serving the new structure identifies that only the proposed office portion of the building will be served by the sanitary sewer line. It is unclear if restroom facilities and or other water uses are proposed in the larger portion of the structure. It is noted that the finished floor elevations between the office and the proposed building are approximately 11 ft elevation difference.
6. The Water Main Wet Tap detail identifies thrust blocks to be utilized. Town of Newburgh requires all piping be restrain joint pipe without the use of thrust blocks.

7. Sanitary sewer connection to an existing Town manhole must be by core drilling only, any other method is not acceptable to the Town. This must be noted on detail.
8. Gerald Canfield's comments regarding location of any proposed hydrants on the site should be received.
9. Stormwater Pollution Prevention Plan is under review by this office. The majority of the site is existing impervious cover. A slight reduction in impervious cover is proposed through implementation of landscaping and green areas. Water quality controls are proposed utilizing proprietary treatment products. A stormwater facilities control maintenance agreement is required to be executed.
10. The proposed structure is located at the side yard setback. Standard note requiring submission of surveyed plot plan prior to issuance of Building Permit should be added to the plan.
11. Previous comments regarding parking in front yard set back with regard to compliance with design guidelines still exist. The Applicants representatives have stated that landscaping is proposed to request a waiver for the design guideline requirement of no parking in front of structures. Planning Board should evaluate proposed landscape with regard to the need to grant the design guideline waiver.
12. A more detailed survey has been submitted with the plans. This office withdraws the comment requiring delineation of the NYSDEC Wetlands as topography on the site clearly indicates no work is proposed within wetland areas. Flood plain boundary has been depicted based on 2009 mapping and site topography.
13. Project requires Town Board approval for an overlay district.
14. Based on additional information submitted along with the detailed plans and Stormwater Pollution Prevention Plan identifying water quality control methodology this office would take no exception to the Planning Board issuing a Negative Declaration for the proposed project with the current scope of the project as indicated in the submitted narratives and plans.
15. It is requested the Applicants discuss with the Planning Board the storage of the cranes on the site. Visual impacts may result if numerous cranes are stored on the site in the operational mode for the cranes. Currently this office envisions the cranes being stored in a stowed condition and not with booms extended.

Respectfully submitted,

McGoey, Hauser and Edsall
Consulting Engineers, D.P.C.

Patrick J. Hines
Principal

PJH/kbw



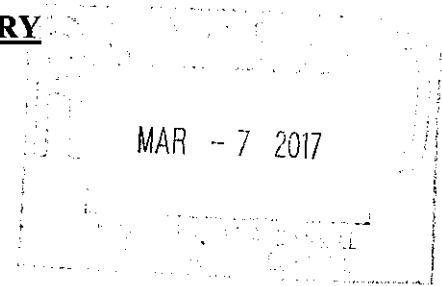
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March 3, 2017

VIA HAND DELIVERY

John Ewasutyn, Chairman
Town of Newburgh Planning Board
308 Gardnertown Road
Newburgh, New York 12550



Re: U.S. Crane, Newburgh
Town of Newburgh, Orange County, New York
MC Project No. 16001017A

Dear Chairman Ewasutyn:

Below please find our responses to comment letters received from Creighton Manning dated August 29, 2016 and January 27, 2017; and McGoey, Hauser & Edsall (MHE), dated August 24, 2016 and January 27, 2017. The comments have been repeated here for clarity.

Creighton Manning August 29, 2016

Comment 1: The project proposes a new entrance to Stewart Avenue, about 200 feet north of Route 17K. The entrance is 30 feet wide. The applicant should confirm the intended use of this driveway as it serves a 20-vehicle parking lot and appears too small for truck/crane access.

Response 1: The entrance on Stewart Avenue, removed for a previous meeting with the Planning Board, has been replaced and changed to act as an access for emergency responders only. This was done to address the comments from the fire department officials during our public meeting on February 22nd 2017

Comment 2: How many employees and truck deliveries expected on a typical day?

Response 2: The initial estimates for employees are 30 people working a single shift. As explained at our initial meeting with the Planning Board and subsequently at our public meeting on 2/22/2017, cranes, or tractor-trailers with crane parts, normally leave the site early in the morning to miss rush-hour. A typical crane setup could take 10 or more tractor-trailers to erect. A typical operation would include four tractor-trailers leaving the site in one day to provide the necessary materials to begin building a crane. Similarly, when these tower cranes return to the site, they are disassembled and loaded on trucks which would return to the site in staggered increments. Truck cranes stored at the site could also be called upon periodically



and normally would require a flatbed vehicle to follow this type crane with counterweights and other equipment.

At most, three or four deliveries of steel would be anticipated any given day. Parts, Federal Express, UPS, garbage trucks would also make deliveries and pickups at the site.

It is not believed by the applicant or his Engineer that any of these vehicle motions would tax the four-lane highways serving the site (Route 17K and Route 300).

Comment 3: The Route 17K entrance is about 70 feet wide, while the exit is about 40 feet. This access has historically accommodated delivery trucks, RV's, and trailers. Is this >100-foot wide access point necessary for the future use of the property? Can the width be reduced?

Response 3: We agree with the Traffic Engineer's assessment of the driveway and as such, have reduced the width of the pavement. This reduction of pavement and increase in pervious surfaces has also benefitted the site in stormwater run-off reduction.

Comment 4: Will oversized loads (weight, length, height, or width) be generated from the site?

Response 4: Yes, oversized loads are a possibility considering the vehicles that are anticipated to use this site. The applicant, considering his business, is well-versed in the permitting requirements for operating these large vehicles on state highways.

Comment 5: Where will steel deliveries (pick up or drop off) occur? Where/how will these vehicles circulate? Other than vehicles, will there be any outside storage of materials?

Response 5: Steel deliveries are proposed to occur inside of the fabrication building. Architectural plans being prepared show interior cranes that will be utilized for off-loading the tractor-trailers that can drive into and out of the garage doors of this building. Circulation of these vehicles is shown on a separate drawing. Vehicles into the site from Route 17K can drive through the fabrication building, leave the site or park in one of over 50 spots designated for tractor-trailers or truck cranes.

Should there be a large delivery of steel it is possible that some of this material could be stored outside.

Comment 6: We would suggest that the sidewalk from Dollar General (BJS Holdings, Newburgh Project 2010-24) continue across the US Crane entrance to their westerly property line.



Response 6: We propose to extend the sidewalk from Dollar General across the entrance of the site as requested.

Comment 7: Additional comments will follow with more detailed submissions.

Response 7: Comment noted.

Creighton Manning January 27, 2017

Comment 1: How many steel deliveries are expected in a typical day? Given the dispersion of the company's mobile cranes, how many, typically, would be at the Newburgh site? Are they used daily? Does their use fluctuate with the construction season? For tower cranes, we assume their use is long term (they are assembled on site and remain until the crane work is complete). How many tower crane truck deliveries (or trips) are generated each day? A narrative describing the number of US Crane employee, truck trips, and auto auction employee/delivery trips is suggested.

Response 1: We hope that our detailed response to Comment 2 above provides a sufficient response as to the typical operations of the site and the number of vehicles entering and leaving the site on a given day. The applicant will request data on the motions of vehicles into and out of the Auto Auction facility as requested.

Comment 2: It's difficult to distinguish between what is existing and staying, versus what is existing and being removed/reconstructed. For example, is the crane trailer storage proposed within the existing fenced in lot? Is there a gate? How will they enter/exit? On a future submission, we suggest including a sheet that only shows proposed work and existing features that will remain after the site is developed.

Response 2: The initial sketch plan was not intending to show a high level of detail such as fencing, gates etc. This more detailed submittal should address the Engineer's request for more detailed site plan information.

Comment 3: Truck and trailer turning movements should be depicted where access and site travel is proposed.

Response 3: See above Comment 5 regarding circulation. Separate truck circulation plans have been provided with this application.

Comment 4: The Route 17K entrance is about 70 feet wide, while the exit is about 40 feet.



This access has historically accommodated delivery trucks, RV's, and trailers. Is this >100-foot wide access point necessary for the future use of the property? Can the width be reduced?

Response 4: See our Response 3 above.

Comment 5: Will oversize loads (weight, length, height, or width) be generated from the site?

Response 5: See our Response 4 above.

Comment 6: We would suggest that the sidewalk from Dollar General (BJS Holdings, Newburgh Project 2010-24) continue across the US Crane entrance to their westerly property line.

Response 6: See our response above regarding the sidewalk extension.

Comment 7: Additional comments will follow with more detailed submissions.

Response 7: Comment noted.

McGoev, Hauser & Edsall (MHE) August 24, 2016

Comment 1: NYSDEC wetland boundaries should be updated on the plans. NYSDEC personnel should mark out the wetland boundary which should be surveyed by the Applicant's representative depicting current wetland boundary.

Response 1: We disagree with the Engineer's comment. There's absolutely no disturbance going on anywhere near the wetland on the site. There are many hundreds of feet of asphalt pavement in place between the proposed improvements and the wetland. We are not proposing any disturbance to the wetland or the buffer on this site and therefore we do not see a need to reestablish a wetland boundary where we are not near encroaching.

Comment 2: Approximate flood plain elevation depicted on the plans should be labeled in accordance with the 2009 flood plain maps. Actual flood plain boundaries should be depicted.

Response 2: Floodplain boundary has been shown on the plan and taken from the 2009 mapping.

Comment 3: Topography on the site should be depicted in support of the flood plain boundary and site development.



- Response 3: Land Surveyors from Maser Consulting attained the existing topography. Proposed site grading has been shown on the site.
- Comment 4: Finish floor elevations for existing and proposed structures should be labeled.
- Response 4: Floor elevations have been provided (see grading plan)
- Comment 5: City of Newburgh Flow Acceptance letter for increased flow from car/truck wash and proposed building should be provided.
- Response 5: The car/truck wash is an existing feature proposed to be continued to be used. Flows from the new building shall be provided and we shall provide estimates for the use of the car/truck wash.
- Comment 6: The narrative report identifies that stormwater quantity mitigation is not required as a re-development site. Treatment for water quality volumes will be required. Implementation of water quality control prior to discharge to the wetland area is required for re-developed sites.
- Response 6: Water quality improvements have been incorporated in the site plan as required for redevelopment sites. We have also sought wherever possible to increase the amount of pervious cover and the removal of areas previously paved.
- Comment 7: Access to Stewart Avenue should be submitted to the Highway Department for review. It is noted a 30-foot wide access drive is proposed. Are tractor-trailers proposed to access site from Stewart Avenue? It is noted that the drive lanes along the western portion of the proposed structure are only 20-foot width.
- Response 7: After meeting with the Fire Department, a gated emergency access is now proposed to access Stewart Avenue.
- Comment 8: Height of the building should be identified.
- Response 8: Exterior height of the building shall not exceed 35 feet. See the drawings by the applicant's Architect.
- Comment 9: Compliance with design guidelines regarding parking in front of the new proposed structure should be addressed. Site lighting should be depicted.
- Response 9: As the access to Stewart Avenue is now a gated emergency access only, we do not believe that this site violates the design guidelines regarding parking in front of a new proposed structure. We have proposed supplemental screening in this area. Site lighting plans have been provided with this application.



Comment 10: Survey should depict adjoining land use and structures.

Response 10: Land Surveyors for Maser consulting have provided a topographic and boundary survey including adjoining land uses and structures as requested.

Comment 11: Gerry Canfield's input regarding the use of the site in the M1 zone should be received.

Response 11: Based on Jerry Canfield's input at an earlier Planning Board Meeting, we are seeking the LH I overlay zone to be placed on this site. We have met with the Town Board and the Town Board has requested that we hold a meeting with the public which we did on 2/22/2017. Based on the turn-out at this public meeting and the lack of negative comments regarding this application, we sincerely hope that the Town Board acts swiftly in providing the LH I overlay zone for this site.

Comment 12: Compliance with Section 109-18 Non Residential Structures in the flood plain should be addressed.

Response 12: No new structures are proposed to be built in the floodplain. No fill is proposed to be placed in the floodplain. The only structures in the floodplain are existing structures which will require flood insurance.

Comment 13: The number of cranes and trailers to be stored on the site should be identified.

Response 13: The existing site has over 25 acres of pavement for vehicle storage. Fifteen (15) of these acres are proposed to be leased back to the Auto Auction for a period of five or six years. We have provided spaces for over 50 large vehicles on the site. Given the amount of pavement on the site, if any restrictions on the number of vehicles are to be imposed by the Planning Board, this will need to be reviewed by the applicant.

Comment 14: Ken Wersted's comments regarding traffic flow on the site as well as onto Stewart Avenue and Route 17K should be addressed.

Response 14: Responses to the Traffic Engineer's comments have been provided above. Traffic flow on the site as shown on the attached diagram's flow onto Stewart Avenue will only be for emergency vehicles and traffic to and from the site from Route 17K. This should not be an issue given the intensity of the existing and proposed uses on the site. The auto wash is a low volume user and the same applies for the proposed US Crane facility.

Comment 15: The following agencies will be involved agencies for SEQRA review:

- 1) NYSDOT



2) NYSDEC

The following will be interested agencies:

- 1) City of Newburgh
- 2) Orange County Planning

Response 15: New York State DOT Permit shall be required. No permits are required from the New York State TEC and they need not be an involved agency as no encroachment or disturbance is proposed to the wetland buffer or wetland.

Comment 16: The Applicant's representative is requested to advise the Board as to how the site will function with the multiple uses.

Response 16: The applicant can discuss further with the Board his intention for use at the site and how the two businesses of a car/truck wash and US crane will utilize the site.

McGoey, Hauser & Edsall (MHE) January 27, 2017

Comment 1: The Applicant is currently before the Town Board for a request to allow the heavy equipment overlay zone to be placed on the lot. Based on discussions with the Town Board, the access drive to Stewart Avenue has been eliminated. Project sponsors are to hold a Public Informational Meeting with surrounding residents.

Response 1: Comment noted and other clarifications to this earlier comment have been provided in this document.

Comment 2: The crane parking area has been moved to a more central portion of the site. Office area has been moved to the rear of the proposed structure where previously it was accessed from Stewart Avenue. Revised concept plan is being presented to the Board for a review, a single access point is proposed off NYS Route 17K at the existing access point of the project.

Response 2: Again, noted and other clarifications to this earlier have been provided in this document. The office has been relocated to the south side of the building with no access propose to Stewart but for emergency access.

Comment 3: Detailed review will be undertaken upon submission of complete design plans.

Response 3: Comment noted



Chairman John Ewasutyn
MC Project No. 16001017A
March 3, 2017
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Very truly yours,

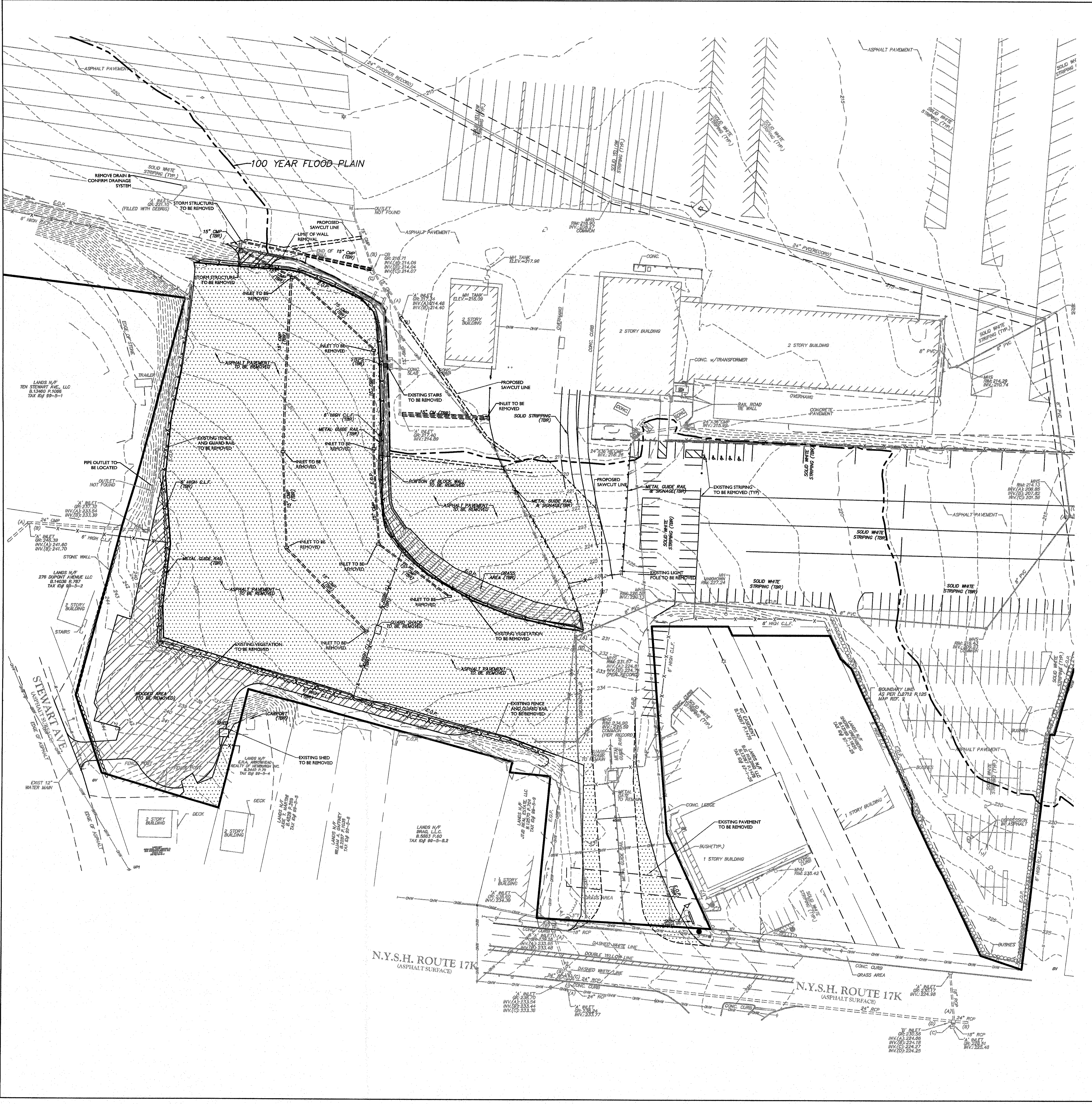
MASER CONSULTING P.A.

A handwritten signature in black ink, appearing to read 'A. Fetherston', written in a cursive style.

Andrew B. Fetherston, P.E.
Principal Associate

ABF/jm
Enclosure

cc: Tom Auringer, w/enclosures
Tim McColgan, w/enclosures
Verticon, LTD, w/enclosures

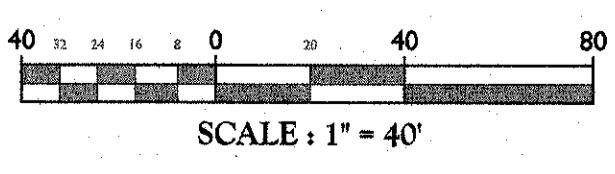


DEMOLITION NOTES:

- IN ACCORDANCE WITH STATE LAW, THE CONTRACTOR SHALL BE REQUIRED TO CALL THE BOARD OF PUBLIC UTILITIES ONE CALL DAMAGE PROTECTION SYSTEM FOR UTILITY MARK OUT IN ADVANCE OF ANY EXCAVATION OR DIGGING. NEW YORK (800-NY-9228 OR 811). A PRE-DEMOLITION CONFERENCE WILL NEED TO BE ARRANGED WITH ONE CALL, NEW YORK AND MUST BE HELD A MINIMUM OF 7 DAYS BEFORE THE START OF ANY DEMOLITION.
- CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL EXISTING SITE IMPROVEMENTS AND UTILITIES. ALL DISCREPANCIES SHALL BE IDENTIFIED TO THE ENGINEER IN WRITING.
- ALL DEMOLITION DEBRIS TO BE REMOVED BY CONTRACTOR IN ACCORDANCE WITH ALL APPLICABLE REGULATIONS.
- MASER CONSULTING, P.A. IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION.
- ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN ACCORDANCE WITH THESE PLANS, AS WELL AS ALL FEDERAL, STATE AND LOCAL REGULATIONS. ANY DISCREPANCIES OR DEVIATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR TO MASER CONSULTING, P.A. IN WRITING FOR RESOLUTION PRIOR TO INITIATION OF SITE ACTIVITY.
- PRIOR TO STARTING ANY DEMOLITION CONTRACTOR IS RESPONSIBLE FOR:
 - ENSURING COPIES OF ALL PERMITS AND APPROVALS MUST BE MAINTAINED ON SITE AND AVAILABLE FOR REVIEW (SEE TYPICAL AND SEPARATE CONTROL PLAN DRAWINGS SET).
 - INSTALLING THE REQUIRED SOIL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO SITE DISTURBANCE.
 - ALL UTILITIES AND SERVICES, INCLUDING BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE LIMITS OF DISTURBANCE, THE CONTRACTOR SHALL USE AND COMPLY WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY NOTIFICATION SYSTEM TO LOCATE ALL THE UNDERGROUND UTILITIES.
 - PROTECTING AND MAINTAINING IN OPERATION ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED DURING ALL DEMOLITION ACTIVITIES.
 - COORDINATING WITH THE APPLICABLE UTILITY SERVICE PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY DEPROTECTION REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY REQUIREMENTS.
 - CLEAN THE EXISTING UTILITY STRUCTURES ON-SITE PRIOR TO CONSTRUCTION AND VERIFY THE INVERTS FOR CONNECTION.
- COORDINATION WITH UTILITY COMPANIES AND THE TOWN OF NEWBURGH REGARDING WORKING "OFF-ROAD" OR ON WEEDS AS MAY BE REQUIRED TO MINIMIZE THE IMPACT ON THE AFFECTED PARTIES.
- A COMPLETE INSPECTION FOR CONTAMINANTS BY A LICENSED ENVIRONMENTAL TESTING AGENCY, SHALL BE PERFORMED ON ALL BUILDINGS AND/OR STRUCTURES PRIOR TO REMOVAL. SAME SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL ENVIRONMENTAL REGULATIONS. ANYALL CONTAMINANTS SHALL BE REMOVED AND DISPOSED OF BY A FEDERALLY LICENSED CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REGULATIONS. ALL ENVIRONMENTAL WORK INCLUDING HAZARDOUS MATERIAL, SOILS, ASBESTOS, OR OTHER REFERENCED OR WRITING HEREIN IS THE SOLE RESPONSIBILITY OF THE OWNER'S ENVIRONMENTAL CONSULTANT.
- MASER CONSULTING, P.A. IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION. CONTRACTOR IS TO PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL THE O.S.H.A. REQUIREMENTS TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
- THE CONTRACTOR SHALL PROVIDE ALL THE "MEANS AND METHODS" NECESSARY TO PREVENT MOVEMENT, SETTLEMENT, OR COLLAPSE OF EXISTING STRUCTURES, AND ANY OTHER IMPROVEMENTS THAT ARE REMAINING ON OR OFF SITE. THE DEMOLITION CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS OF DAMAGE TO ALL ITEMS THAT ARE TO REMAIN AS A RESULT OF HIS ACTIVITIES. ALL REPAIRS SHALL USE NEW MATERIAL. THE REPAIRS SHALL RESTORE THE ITEM TO THE PREDEMOLITION CONDITION.
- TREE CLEARING ACTIVITIES CAN ONLY OCCUR BETWEEN THE DATES OF OCTOBER 1ST AND MARCH 31ST.
- THE CONTRACTOR SHALL PERFORM EARTH MOVEMENT ACTIVITIES, DEMOLITION AND REMOVAL OF ALL FOUNDATION WALLS, FOOTINGS, AND OTHER MATERIALS WITHIN THE LIMITS OF DISTURBANCE IN ACCORDANCE WITH DIRECTION BY OWNER'S STRUCTURAL OR GEOTECHNICAL ENGINEER.
- ROCK EXCAVATIONS WILL BE PERFORMED BY MECHANICAL MEANS ONLY. USE OF EXPLOSIVES IS PROHIBITED. ALL THE REQUIRED PERMITS AND CONTROL MEASURES THAT ARE REQUIRED BY THE FEDERAL, STATE AND LOCAL GOVERNMENTS SHALL BE IN PLACE PRIOR TO STARTING. THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL INSPECTION AND SEISMIC VIBRATION TESTING THAT IS REQUIRED TO MONITOR THE EFFECTS ON ALL LOCAL STRUCTURES AS APPLICABLE.
- CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND GENERALLY ACCEPTED SAFE PRACTICES IN CONFORMANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL, AS WELL AS FEDERAL, STATE AND LOCAL REGULATIONS WHEN DEMOLITION RELATED ACTIVITIES IMPACT ROADWAYS OR ROADWAY RIGHTS-OF-WAY.
- CONDUCT DEMOLITION ACTIVITIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, SIDEWALKS, WALKWAYS, AND OTHER ADJACENT FACILITIES. STREET CLOSURE PERMITS MUST BE RECEIVED FROM THE APPROPRIATE GOVERNMENTAL AUTHORITY.
- DEMOLITION ACTIVITIES AND EQUIPMENT SHALL NOT USE AREAS OUTSIDE THE DERIVED PROPERTY LINE WITHOUT WRITTEN PERMISSION OF THE OWNER AND/OR APPROPRIATE GOVERNMENT AGENCY.
- USE DUST CONTROL MEASURES TO LIMIT AIRBORNE DUST AND DIRT BINS AND SCATTERING IN THE AIR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL STANDARDS. AFTER THE DEMOLITION IS COMPLETE, ADJACENT STRUCTURES AND IMPROVEMENTS SHALL BE CLEANED OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL ADJACENT AREAS TO THEIR "PRE-DEMOLITION" CONDITION.
- THIS DEMOLITION PLAN IS INTENDED TO IDENTIFY THOSE EXISTING ITEMS/CONDITIONS WHICH ARE TO BE REMOVED. IT IS NOT INTENDED TO PROVIDE DIRECTION OTHER THAN THAT ALL METHODS AND MEANS ARE TO BE IN ACCORDANCE WITH STATE, FEDERAL, LOCAL, AND JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL O.S.H.A. AND OTHER SAFETY PRECAUTIONS NECESSARY TO PROVIDE A SAFE WORK SITE.
- DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL DEMOLITION WASTES AND DEBRIS (SOLID WASTE) SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL TOWN, COUNTY, STATE AND FEDERAL LAWS AND APPLICABLE CODES.
- DEMOLITION SHALL NOT PROCEED UNTIL THE APPROXIMATE LOCATION OF THE EXISTING UTILITIES ARE MARKED IN THE FIELD AND ALL UTILITY CONNECTIONS ARE SUITABLY SHUT OFF AND DISCONNECTED AND PROPER DEMOLITION PERMITS ARE IN PLACE WITH THE TOWN.
- CONTRACTOR IS RESPONSIBLE TO RESTORE ALL DISTURBED SITE AREAS TO ORIGINAL CONDITION AS DIRECTED BY THE OWNER.
- PROTECT ALL EXISTING UTILITIES TO REMAIN (INCLUDING DRAINAGE STRUCTURES, HYDRANTS, VALVES, SEWER MANHOLES ETC.) DURING DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGES CAUSED BY HIMSELF OR SUB-CONTRACTORS.

DRAWING LEGEND

PROPERTY BOUNDARY	---
EDGE OF PAVEMENT (E.O.P.)	----
EXISTING CURB	=====
DEPRESSED CURB	=====
PAVEMENT STRIPING	-----
OVERHEAD WIRES	---O---O---O---
WATERLINE MARKOUT	W-----W-----W-----
EASEMENTS	-----
MAJOR CONTOUR	-----
MINOR CONTOUR	-----
SANITARY PIPES	-----
STORMWATER PIPES	-----
TREELINE	-----
SIGN	+
POST / BOLLARD	o
UTILITY POLE	o
GUY WIRE	o
LIGHT POLE	o
SANITARY MANHOLE	o
SANITARY CLEANOUT	o
SANITARY MANHOLE	o
DRAINAGE MANHOLE	o
WATER VALVE	o
CATCH BASIN	o
TR = TO BE REMOVED	o
UTP = UTILITY POLE	o
DI = DRAIN INLET	o
CB = CATCH BASIN	o
GR = GRATE	o
CO = CLEANOUT	o
SAWCUT LINE	o
PAVEMENT TO BE REMOVED	-----
VEGETATION TO BE REMOVED	-----



MASER CONSULTING, P.A.
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DATE	DRAWN BY	DESCRIPTION

DATE	DRAWN BY	DESCRIPTION

ANDREW B. FETHERSTON
 NEW YORK LICENSED PROFESSIONAL ENGINEER - LICENSE NUMBER: 07355-1

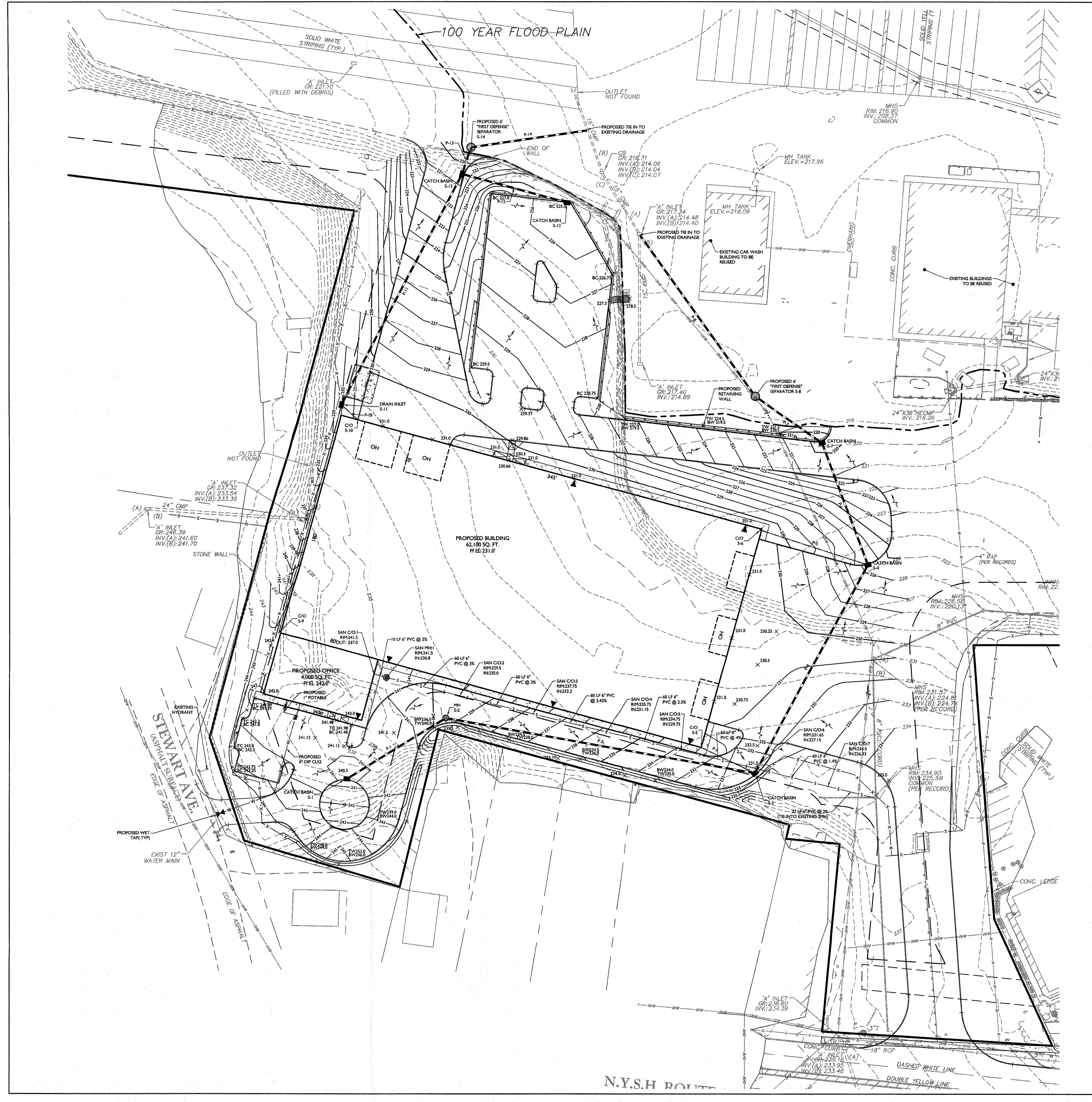
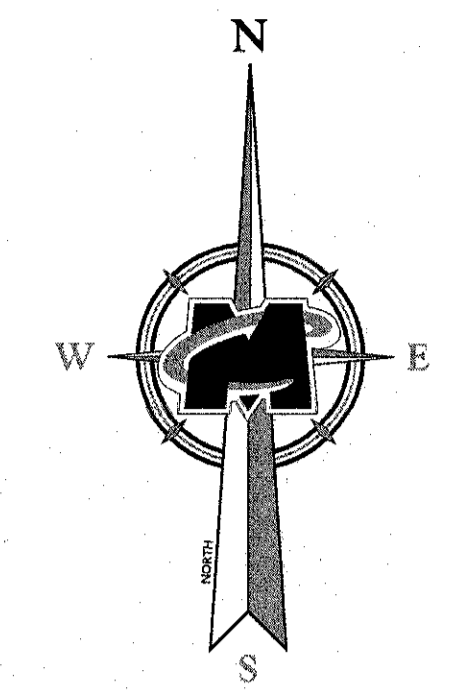
PRELIMINARY SITE PLAN
 FOR
18 ROUTE 17K LLC
 SECTION 97
 BLOCK 1
 LOT 21.2
 TOWN OF NEWBURGH
 COUNTY OF ORANGE
 STATE OF NEW YORK

NEW WINDSOR OFFICE
 355 Hudson Valley Avenue
 Suite 101
 New Windsor, NY 12553
 Phone: 845.347.1033
 Fax: 845.347.1033

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	03/11/17	CPM	ABF
PROJECT NUMBER	DRAWING NAME		
1801077A	C-DEMPO		

EXISTING CONDITIONS & DEMOLITION PLAN

SHEET NUMBER:
02 of 11



EAST Drainage Structure Table

Structure I.D.	Description	Rim/Grate	Pipes (In)	Inverts (In)	Pipes (Out)	Inverts (Out)
S-1	30 in. x 48 in. 'A' INLET	240.50	15" HDPE	237.00	15" HDPE	237.00
S-2	48 in. dia. CYLINDER MH	239.50	15" HDPE	235.00	15" HDPE	235.00
S-3	30 in. x 48 in. 'A' INLET	233.23	4" HDPE	224.00	15" HDPE	224.00
S-4	30 in. x 48 in. 'A' INLET	226.00	15" HDPE	219.00	15" HDPE	218.75
S-5	6 in. Clean Out	224.00	4" HDPE	222.75	4" HDPE	228.00
S-6	6 in. Clean Out	221.00	4" HDPE	219.75	4" HDPE	228.50
S-7	30 in. x 48 in. 'A' INLET	219.50	15" HDPE	216.25	15" HDPE	216.25
S-8	72 in. dia. CYLINDER MH	219.36	15" HDPE	215.25	15" HDPE	215.25

WEST Drainage Structure Table

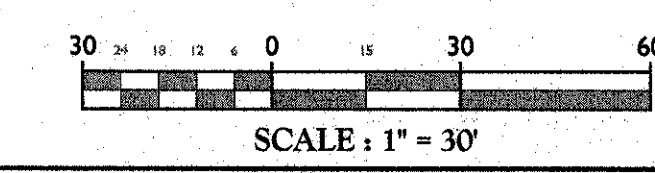
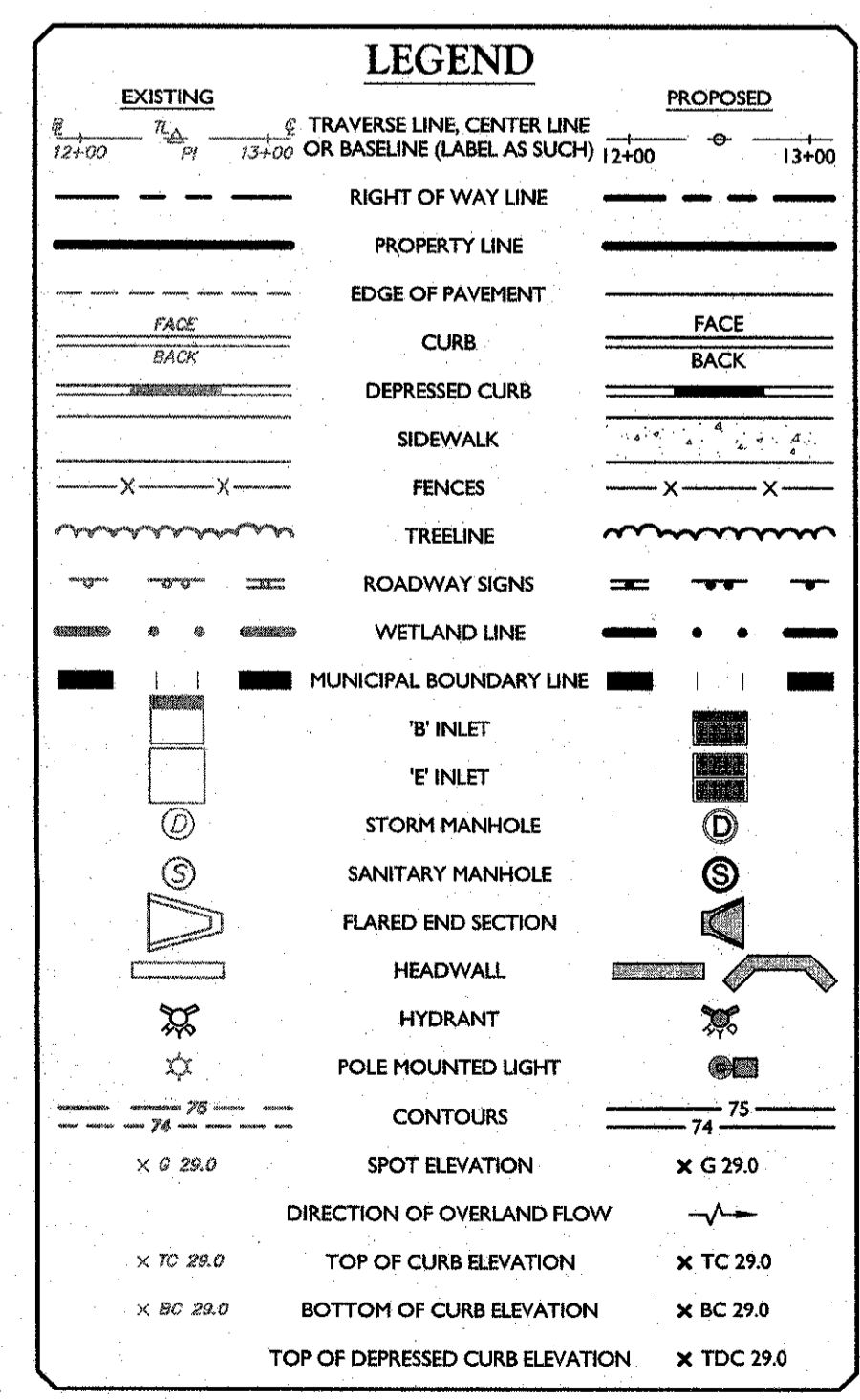
Structure I.D.	Description	Rim/Grate	Pipes (In)	Inverts (In)	Pipes (Out)	Inverts (Out)
S-9	6 in. Clean Out	240.86	4" HDPE	237.00	4" HDPE	239.50
S-10	6 in. Clean Out	1.72	4" HDPE	227.75	4" HDPE	227.75
S-11	30 in. x 48 in. 'A' INLET	232.00	4" HDPE	227.00	15" HDPE	227.00
S-12	30 in. x 48 in. 'A' INLET	0.00	4" HDPE	227.75	15" HDPE	221.50
S-13	30 in. x 48 in. 'A' INLET	227.00	15" HDPE	217.20	15" HDPE	217.00
S-14	72 in. dia. CYLINDER MH	219.00	15" HDPE	215.75	15" HDPE	215.75

EAST Drainage Pipe Table

Pipe I.D.	Description	Length	Invert Up	Invert Dn	Slope
P-1	15" HDPE	9'	237.00	235.00	2.16%
P-2	15" HDPE	24'	235.00	226.00	3.63%
P-3	4" HDPE	47'	228.00	224.00	8.52%
P-4	15" HDPE	18'	224.00	219.00	2.65%
P-5	4" HDPE	90'	228.50	222.75	4.41%
P-6	15" HDPE	104'	218.75	216.25	2.41%
P-7	15" HDPE	65'	216.25	215.25	1.58%
P-8	15" HDPE	157'	215.25	214.40	0.54%

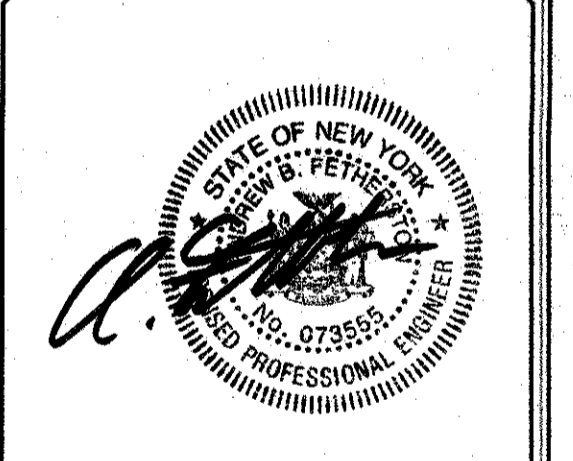
WEST Drainage Pipe Table

Pipe I.D.	Description	Length	Invert Up	Invert Dn	Slope
P-9	4" HDPE	18'	239.50	227.75	6.25%
P-10	4" HDPE	18'	227.75	227.00	4.26%
P-11	15" HDPE	207'	227.00	217.20	4.73%
P-12	15" HDPE	80'	221.50	217.25	4.93%
P-13	15" HDPE	22'	217.00	215.75	5.60%
P-14	15" HDPE	93'	215.75	-0.62	233.73%



REV.	DATE	DRAWN BY	DESCRIPTION

REV.	DATE	DRAWN BY	DESCRIPTION



ANDREW B. FETHERSTON
 NEW YORK LICENSED PROFESSIONAL ENGINEER - LICENSE NUMBER: 07355-1

PRELIMINARY SITE PLAN
 FOR
18 ROUTE 17K LLC

SECTION 97
BLOCK 1
LOT 21.2

TOWN OF NEWBURGH
COUNTY OF ORANGE
STATE OF NEW YORK

NEW WINDSOR OFFICE
 355 Hudson Valley Avenue
 Suite 101
 New Windsor, NY 12553
 Phone: 845.564.4495
 Fax: 845.567.1025

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	03/15/17	CPM	ABF
PROJECT NUMBER	DRAWING NAME	DATE	SCALE
1600107A	C-GRAD		

SHEET TITLE:
GRADING, DRAINAGE AND UTILITY PLAN

SHEET NUMBER:
5 of 11

- ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCE, OR IN THEIR PROPER SEQUENCE, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
- ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN FOURTEEN (14) DAYS FOR DISTURBANCES LESS THAN FIVE (5) ACRES AND SEVEN (7) DAYS FOR DISTURBANCES GREATER THAN FIVE (5) ACRES, AND NOT SUBJECT TO CONSTRUCTION TRAFFIC, WILL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PREVENTS THE ESTABLISHMENT OF TEMPORARY COVER, THE DISTURBED AREAS WILL BE MULCHED WITH STRAW, OR EQUIVALENT MATERIAL, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
- PERMANENT VEGETATION TO BE SEEDED OR SOODED ON ALL EXPOSED AREAS WITHIN FIVE (5) DAYS AFTER FINAL GRADING. MULCHING IS REQUIRED ON ALL SEEDING. WHEN HYDROSEEDING, MULCH SHALL NOT BE INCLUDED IN THE TANK WITH THE SEED.
- ALL WORK TO BE DONE IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.
- A SUBBASE COURSE WILL BE APPLIED IMMEDIATELY FOLLOWING ROUGH GRADING AND INSTALLATION OF IMPROVEMENTS TO STABILIZE STREETS, ROADS, DRIVEWAYS, AND PARKING AREAS. IN AREAS WHERE NO UTILITIES ARE PRESENT, THE SUBBASE SHALL BE INSTALLED WITHIN FIVE (5) DAYS OF THE PRELIMINARY GRADING.
- IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING, ALL CRITICAL AREAS SUBJECT TO EROSION (I.E. STEEP SLOPES AND ROADWAY EMBANKMENTS) WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR A SUITABLE EQUIVALENT, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
- ANY STEEP SLOPES RECEIVING PIPELINE INSTALLATION WILL BE BACKFILLED AND STABILIZED DAILY, AS THE INSTALLATION CONTINUES (I.E. SLOPES GREATER THAN 3:1).
- THE STANDARD FOR STABILIZED CONSTRUCTION ACCESS REQUIRES THE INSTALLATION OF A STONE PAD, AT ALL CONSTRUCTION DRIVEWAYS, BEFORE INITIAL SITE DISTURBANCE.
- IN ACCORDANCE WITH THE STANDARD FOR MANAGEMENT OF HIGH ACID PRODUCING SOILS, ANY SOIL HAVING A PH OF 4 OR LESS OR CONTAINING IRON SULFIDES SHALL BE COVERED WITH A MINIMUM OF TWELVE (12) INCHES OF SOIL HAVING A PH OF 5 OR MORE PRIOR TO SEEDBED PREPARATION. AREAS WHERE TREES OR SHRUBS ARE TO BE PLANTED SHALL BE COVERED WITH A MINIMUM OF TWENTY-FOUR (24) INCHES OF SOIL HAVING A PH OF 5 OR MORE.
- AT THE TIME THE SITE PREPARATION FOR PERMANENT VEGETATIVE STABILIZATION IS GOING TO BE ACCOMPLISHED, ANY SOIL THAT WILL NOT PROVIDE A SUITABLE ENVIRONMENT TO SUPPORT ADEQUATE VEGETATIVE GROUND COVER, SHALL BE REMOVED OR TREATED IN SUCH A WAY THAT IT WILL PERMANENTLY ADJUST THE SOIL CONDITIONS AND RENDER IT SUITABLE FOR VEGETATIVE GROUND COVER. IF THE REMOVAL OR TREATMENT OF THE SOIL WILL NOT PROVIDE SUITABLE CONDITIONS, NON-VEGETATIVE MEANS OF PERMANENT GROUND STABILIZATION WILL HAVE TO BE EMPLOYED.
- CONDUIT OUTLET PROTECTION MUST BE REVIEWED & SUPPLEMENTED AT ALL OUTFALLS PRIOR TO THE DRAINAGE SYSTEM BECOMING OPERATIONAL.
- UNFILTERED DEWATERING IS NOT PERMITTED. TAKE ALL NECESSARY PRECAUTIONS DURING ALL DEWATERING OPERATIONS TO MINIMIZE SEDIMENT TRANSFER. ANY DEWATERING METHODS USED MUST BE IN ACCORDANCE WITH STATE STANDARDS.
- SHOULD THE CONTROL OF DUST AT THE SITE BE NECESSARY, THE SITE WILL BE SPRINKLED UNTIL THE SURFACE IS WET, TEMPORARY VEGETATIVE COVER SHALL BE ESTABLISHED OR MULCH SHALL BE APPLIED IN ACCORDANCE WITH STATE STANDARDS FOR EROSION CONTROL.
- ALL SOIL WASHED, DROPPED, SPILLED, OR TRACKED OUTSIDE THE LIMIT OF DISTURBANCE OR ONTO PUBLIC RIGHT-OF-WAY WILL BE REMOVED IMMEDIATELY.
- THE PROPERTY OWNER SHALL BE RESPONSIBLE FOR ANY EROSION AND SEDIMENTATION THAT MAY OCCUR BELOW STORMWATER OUTFALLS OR OFFSITE AS A RESULT OF CONSTRUCTION OF THE PROJECT.
- STOCKPILE AND STAGING LOCATIONS DETERMINED IN THE FIELD, SHALL BE PLACED WITHIN THE LIMITS OF DISTURBANCE ACCORDING TO THE CERTIFIED PLAN.
- CONCRETE WASHOUT, DUMPSTER, & STAGING AREA LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED AT THE PRE-CONSTRUCTION MEETING. THEY SHALL BE PLACED IN THE PROXIMITY OF THE CONSTRUCTION ENTRANCE AND STAGING AREAS AND SHALL BE USED PRIOR TO EXITING THE PROJECT SITE. THE LOCATION SHALL BE IN A PRACTICAL, CLEARLY DELINEATED, AREA AND BE MAINTAINED THROUGHOUT CONSTRUCTION.
- ALL SOIL STOCKPILES ARE TO BE TEMPORARILY STABILIZED IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL.
- IF SOIL ACTIVITIES HAVE BEEN TEMPORARILY SUSPENDED, I.E. WINTER SHUTDOWN, ALL DISTURBED AREAS MUST BE TEMPORARILY STABILIZED AND ONCE A MONTH INSPECTIONS CAN OCCUR. THE GENERAL PERMIT CAN BE REFERENCED FOR FURTHER INFORMATION.
- ALL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED BY THE PROPERTY OWNER, AND SHALL BECOME THEIR RESPONSIBILITY.
- PAVEMENT AREAS ARE TO BE KEPT CLEAN AT ALL TIMES.
- DURING CONSTRUCTION, ANY ADDITIONAL CONTROL MEASURES DEEMED NECESSARY TO PREVENT EROSION OR CONTROL SEDIMENT BEYOND THESE MEASURES SHOWN ON THE APPROVED PLAN SHALL BE INSTALLED OR EMPLOYED AT THE DIRECTION OF THE PROJECT ENGINEER.
- ALL TEMPORARY, STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES CAN BE REMOVED, WHEN ALL CONSTRUCTION ACTIVITY IDENTIFIED IN THE SWPPP HAS BEEN COMPLETED, ALL AREAS OF DISTURBANCE HAVE ACHIEVED FINAL STABILIZATION AND ALL POST-CONSTRUCTION STORMWATER MANAGEMENT PRACTICES HAVE BEEN CONSTRUCTED IN CONFORMANCE WITH THE SWPPP AND ARE OPERATIONAL.

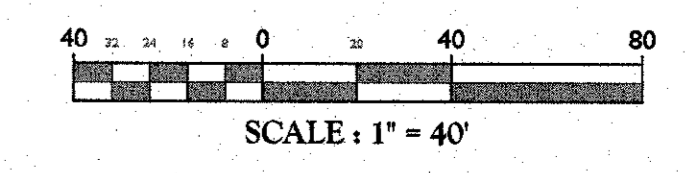
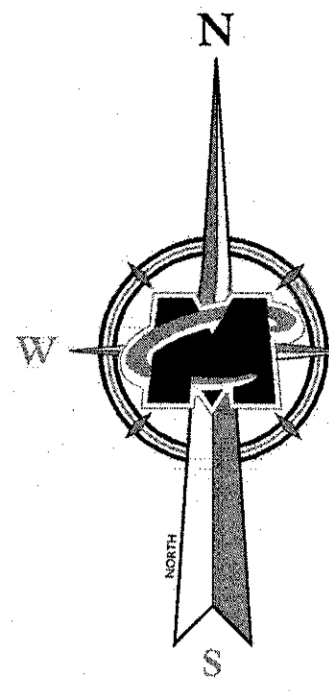
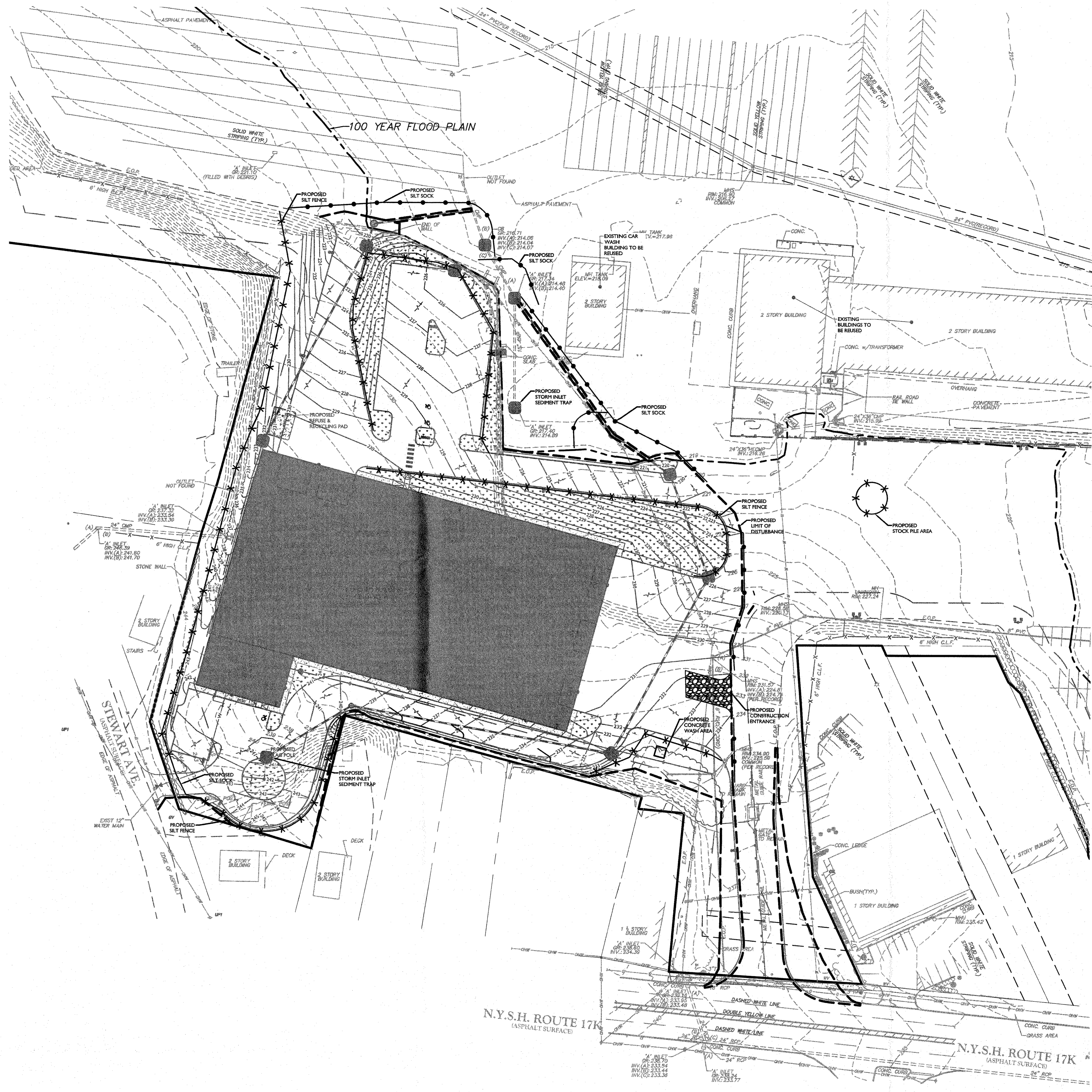
FINAL STABILIZATION - MEANS THAT ALL SOIL DISTURBANCE ACTIVITIES HAVE CEASED AND A UNIFORM, PERENNIAL VEGETATIVE COVER WITH A DENSITY OF EIGHTY (80) PERCENT OVER THE ENTIRE PERVIOUS SURFACE HAS BEEN ESTABLISHED, OR OTHER EQUIVALENT STABILIZATION MEASURES, SUCH AS PERMANENT LANDSCAPE MULCHES, ROCK RIP-RAP OR WASHED/CRUSHED STONE HAVE BEEN APPLIED ON ALL DISTURBED AREAS THAT ARE NOT COVERED BY PERMANENT STRUCTURES, CONCRETE OR PAVEMENT.

MAINTENANCE PLAN DURING CONSTRUCTION:
INSPECTION AND MAINTENANCE SHALL BE PERFORMED IN CONFORMANCE WITH GP-0-15-002. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF-PRODUCING RAINFALL BUT IN NO CASE LESS THAN ONCE EVERY WEEK. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED AND INSTALLED FOR THE PROJECT. THE SEDIMENT TRAPS WILL BE CLEANED OUT WHEN THE LEVEL OF SEDIMENT REACHES 25% OF ITS CAPACITY. SEDIMENT WILL BE REMOVED FROM BEHIND THE SILT FENCE WHEN IT BECOMES APPROXIMATELY 6" DEEP AT THE FENCE. THE SEDIMENT FENCE WILL BE REPAIRED AS NECESSARY TO MAINTAIN A BARRIER. ALL SEEDED AREAS WILL BE FERTILIZED, RE-SEEDING AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE LANDSCAPE PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.

NOTE: DURING THE CONSTRUCTION OF A PHASE, EACH SUBSEQUENT PHASE WILL HAVE BEEN CAPPED AND STABILIZED WITH DENSE GRASS COVER.

LEGEND FOR EROSION CONTROL DURING CONSTRUCTION

- x — SILT FENCE
- STORM INLET SEDIMENT TRAP
- STABILIZED CONSTRUCTION ENTRANCE
- STONE OUTLET SEDIMENT TRAP
- TEMPORARY SWALE
- CONSTRUCTION LIMIT LINE
- SILT SOCK
- x — 6' FOOT HIGH CHAIN LINK FENCE
- CONCRETE WASH AREA

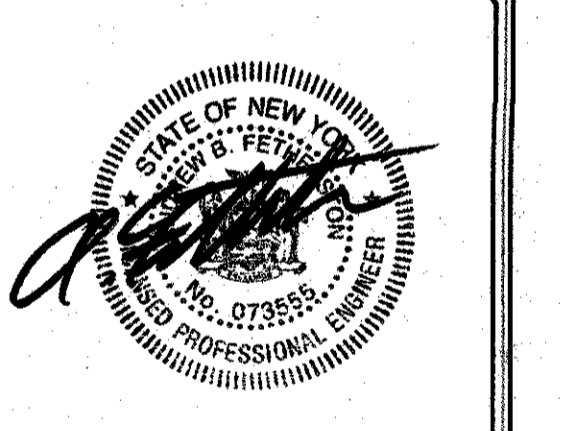


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 NEW YORK LICENSED PROFESSIONAL
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 FOR
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**SECTION 97
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 LOT 21.2**
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SCALE	DATE	DRAWN BY	CHECKED BY
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PROJECT NUMBER:	1601017A	DRAWING NAME:	C-SECC

SOIL EROSION & SEDIMENT CONTROL PLAN
 SHEET NUMBER:
6 of 11

