

Full Size Site Plans

are available for viewing at the

Zoning Board of Appeals

Office located at

308 Gardnertown Road

Newburgh, NY

845-566-4901

5°22'21"W
32.68'

SECTION 76
BLOCK 1
LOT 1.2
N/F REPUTED OWNER
BHAJ ENTERPRISES, LLC
LIBER 12594, PAGE 1840

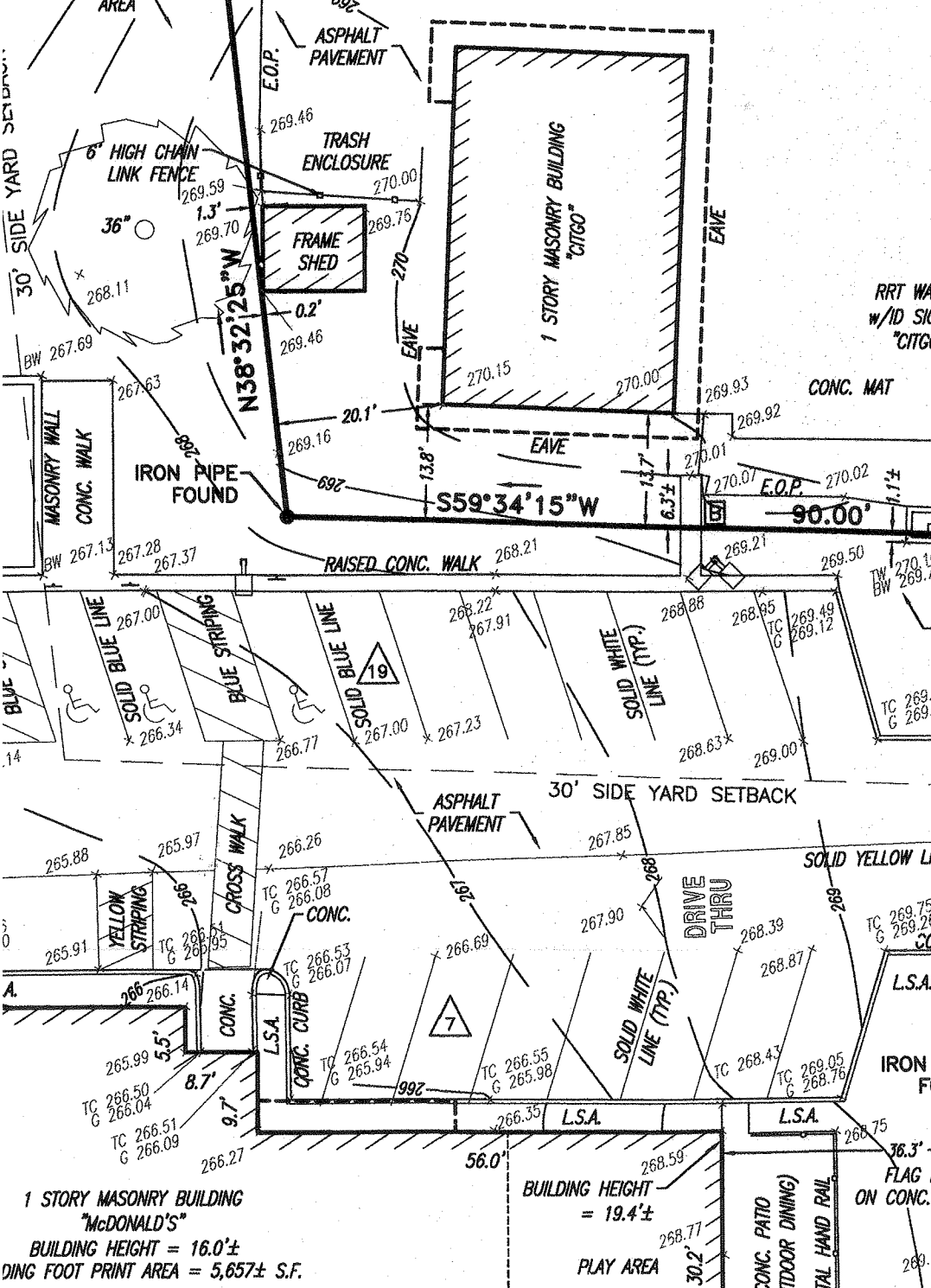
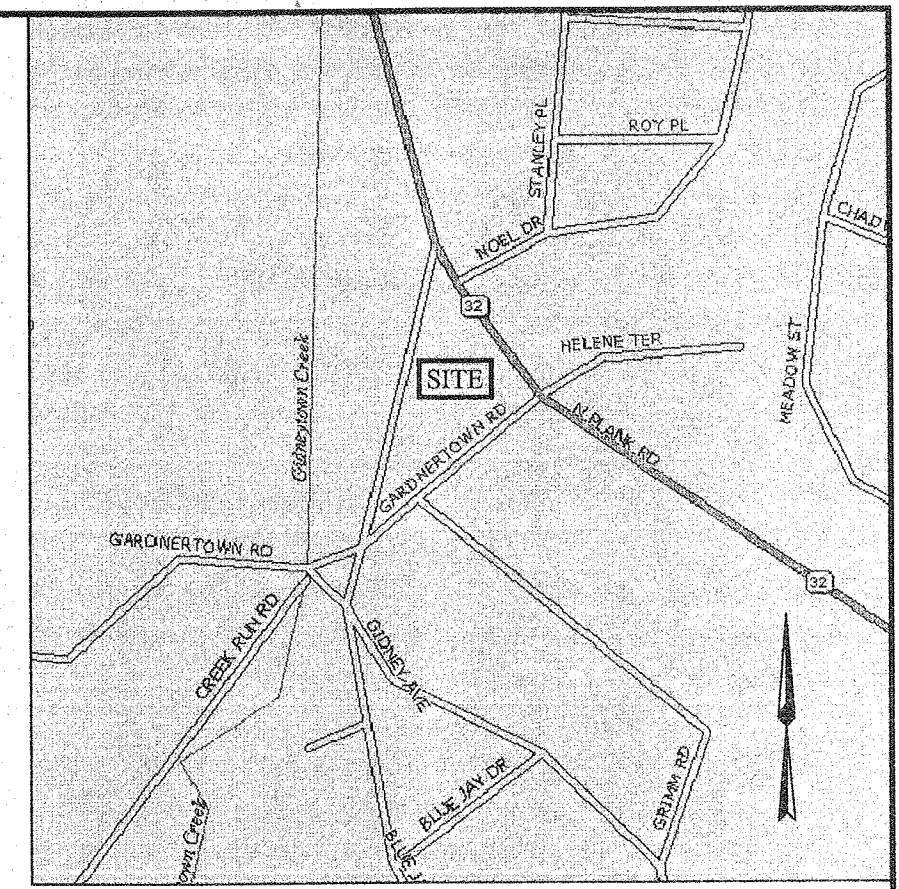


TABLE OF POSSIBLE ENCROACHMENTS:

- A RRT WALL CROSSING NORTHEASTERLY PROPERTY LINE
- B CONC. WALK CROSSING NORTHEASTERLY PROPERTY LINE
- C GARDNERTOWN ROAD AND GIDNEY AVENUE ROADWAY IMPROVEMENTS WITHIN DEED LIMITS
- D UTILITY POLE AND GUY WIRES CROSSING RIGHT OF WAY LINE ALONG ROUTE 32
- E OVERHEAD WIRES CROSSING OVER PROPERTY LINE
- F STONE AREA CROSSING RIGHT OF WAY LINE ALONG ROUTE 32
- G GUY WIRE CROSSING RIGHT OF WAY LINE ALONG ROUTE 32



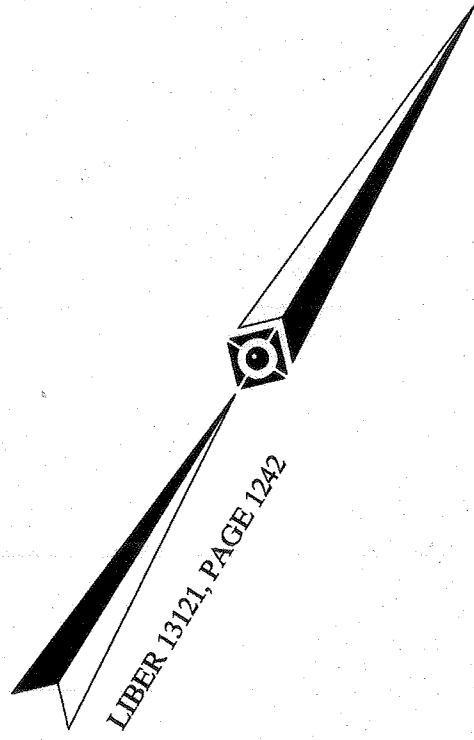
VICINITY MAP
©2008 DeLorme, Street Atlas USA

BY THE DIG SAFELY NEW YORK
MARK OUT UNDERGROUND FACILITIES
UNDERGROUND UTILITY INFORMATION
UTILITY COMPANIES RESPONSE TO THIS
-048-00

PHONE NUMBER
845-206-6046
845-564-7813
315-448-2506

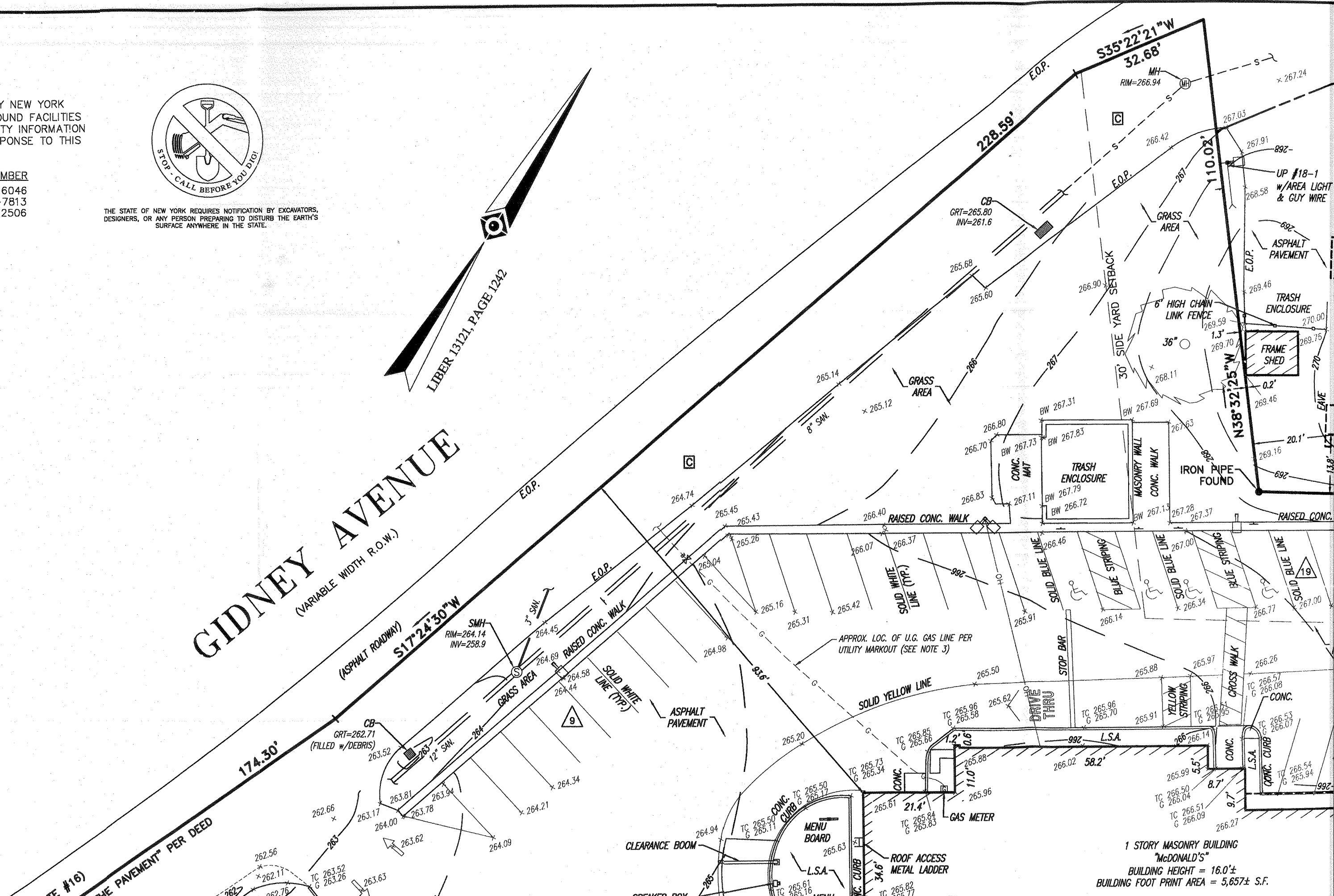


THE STATE OF NEW YORK REQUIRES NOTIFICATION BY EXCAVATORS,
DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S
SURFACE ANYWHERE IN THE STATE.



GIDNEY AVENUE

(VARIABLE WIDTH R.O.W.)



1 STORY MASONRY BUILDING
"McDONALD'S"
BUILDING HEIGHT = 16.0'±
BUILDING FOOT PRINT AREA = 5,657± S.F.

UTILITIES:

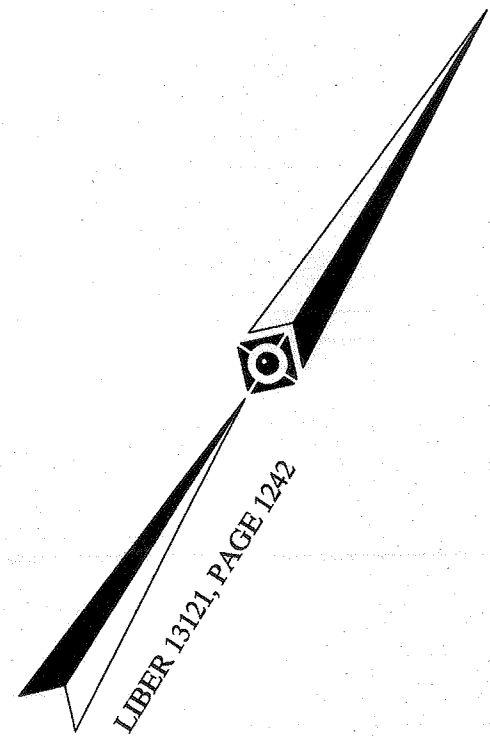
THE FOLLOWING COMPANIES WERE NOTIFIED BY THE DIG SAFELY NEW YORK (1-800-962-7962) AND REQUESTED TO MARK OUT UNDERGROUND FACILITIES AFFECTING AND SERVICING THIS SITE. THE UNDERGROUND UTILITY INFORMATION SHOWN HEREON IS BASED UPON THE UTILITY COMPANIES RESPONSE TO THIS REQUEST. SERIAL NUMBER(S): 10071-074-048-00

UTILITY COMPANY	PHONE NUMBER
CENTRAL HUDSON GAS AND ELECTRIC SOUTH	845-206-6046
TOWN OF NEWBURGH HIGHWAY	845-564-7813
VERIZON / HUDSON VALLEY	315-448-2506

PHONE NUMBER
845-206-6046
845-564-7813
315-448-2506

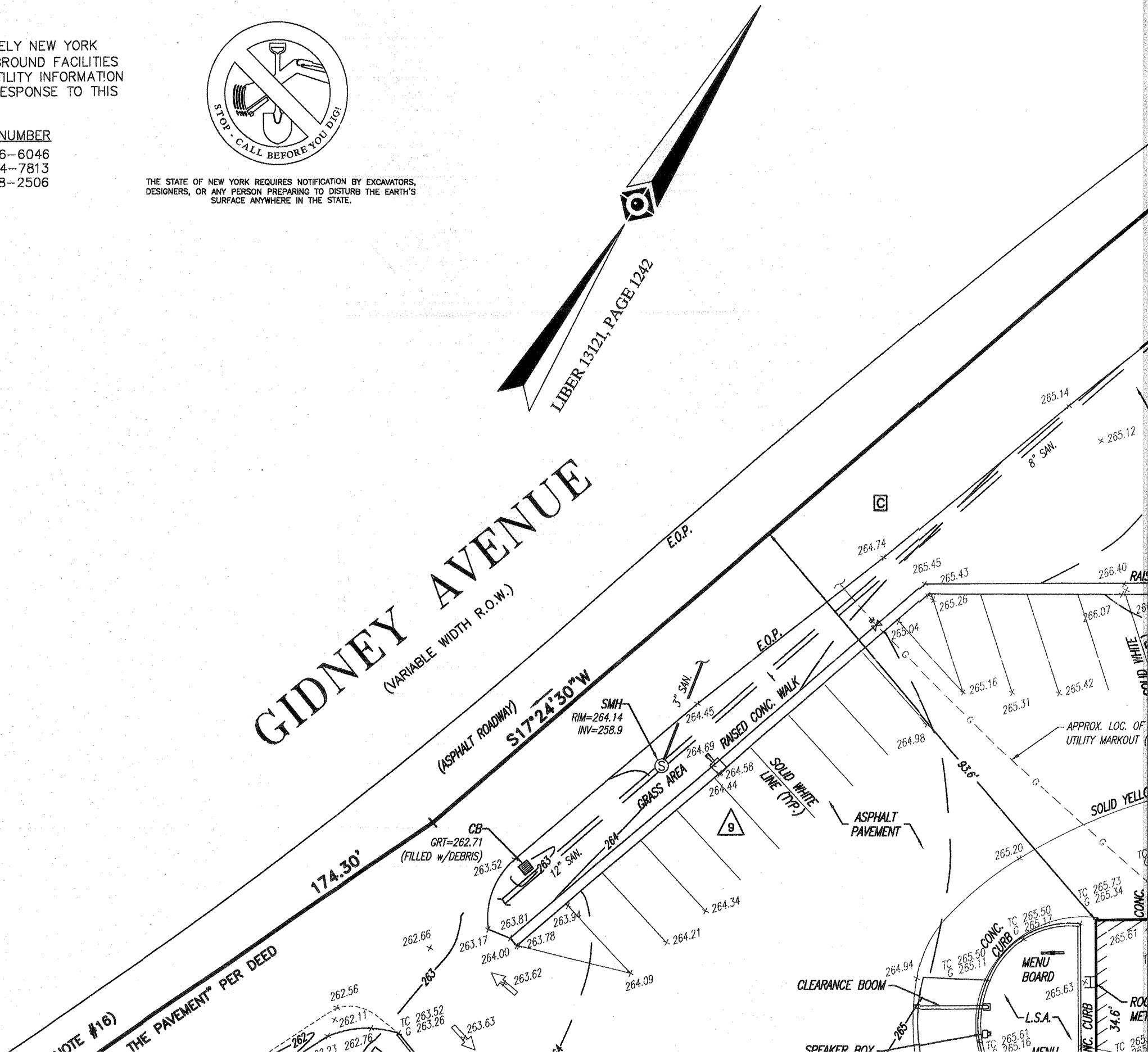


THE STATE OF NEW YORK REQUIRES NOTIFICATION BY EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S SURFACE ANYWHERE IN THE STATE.



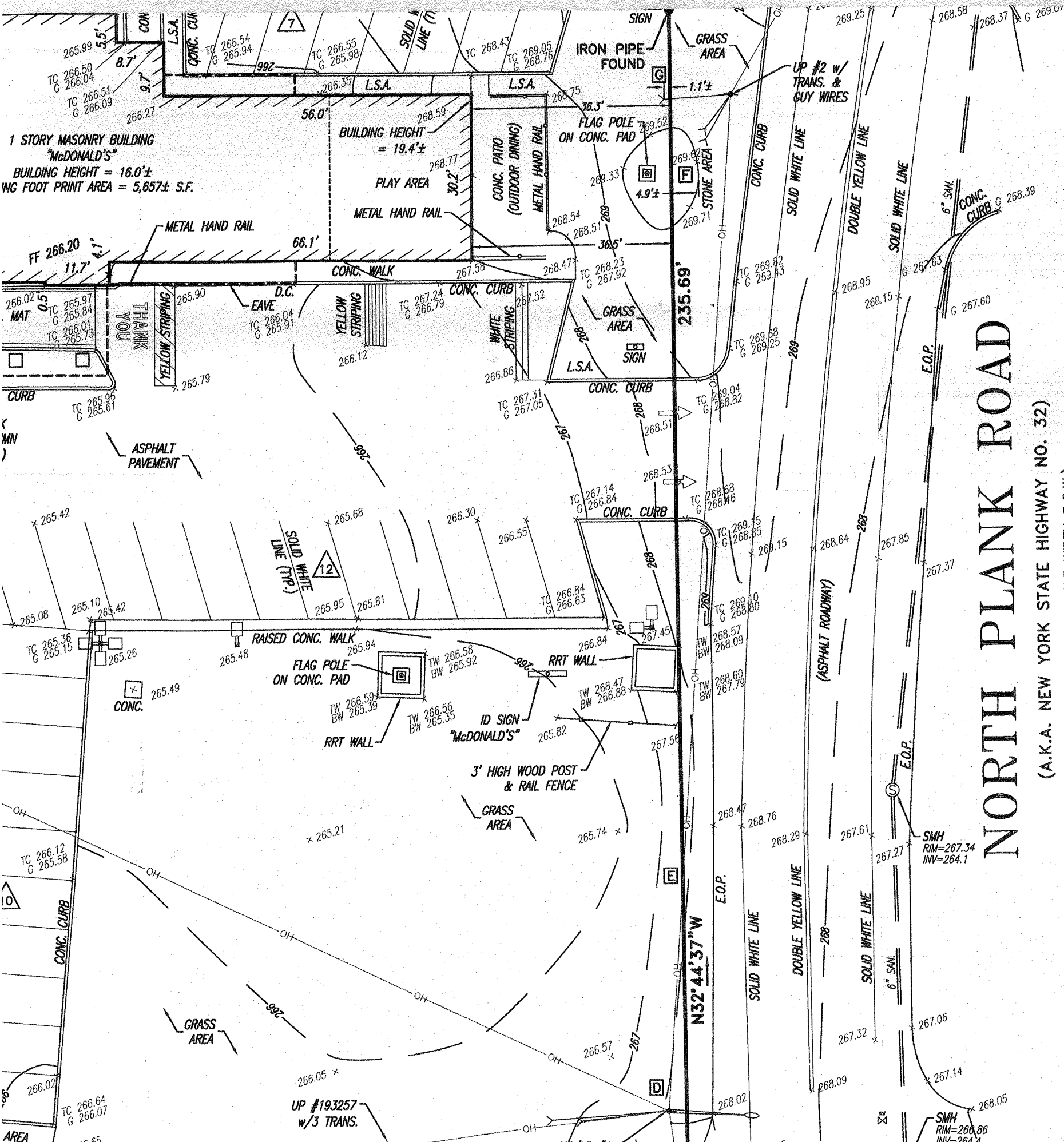
GIDNEY AVENUE

(VARIABLE WIDTH R.O.W.)



NOTE #16)
THE PAVEMENT PER DEED

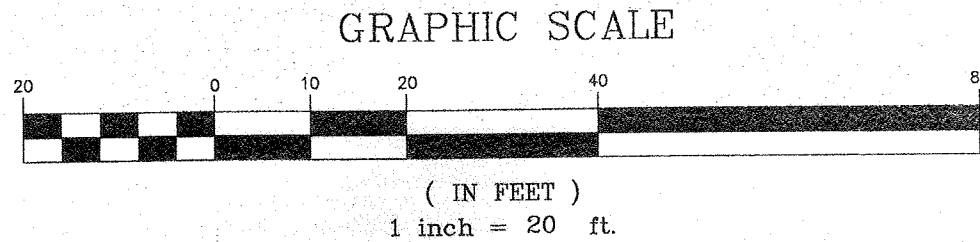
CLEARANCE BOOM
MENU BOARD
L.S.A.
CONC. CURB
ROCK MET
SOLID WHITE
SOLID YELLOW
CONC.



NORTH PLANK ROAD

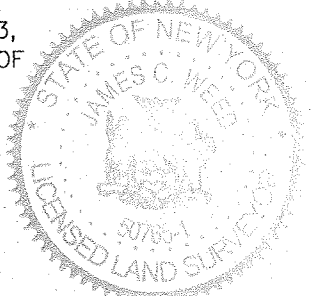
(A.K.A. NEW YORK STATE HIGHWAY NO. 32)
(PUBLIC - VARIABLE WIDTH R.O.W.)

SEE SHEET 2 FOR NOTES, REFERENCES, DESCRIPTIONS AND ZONING TABLE



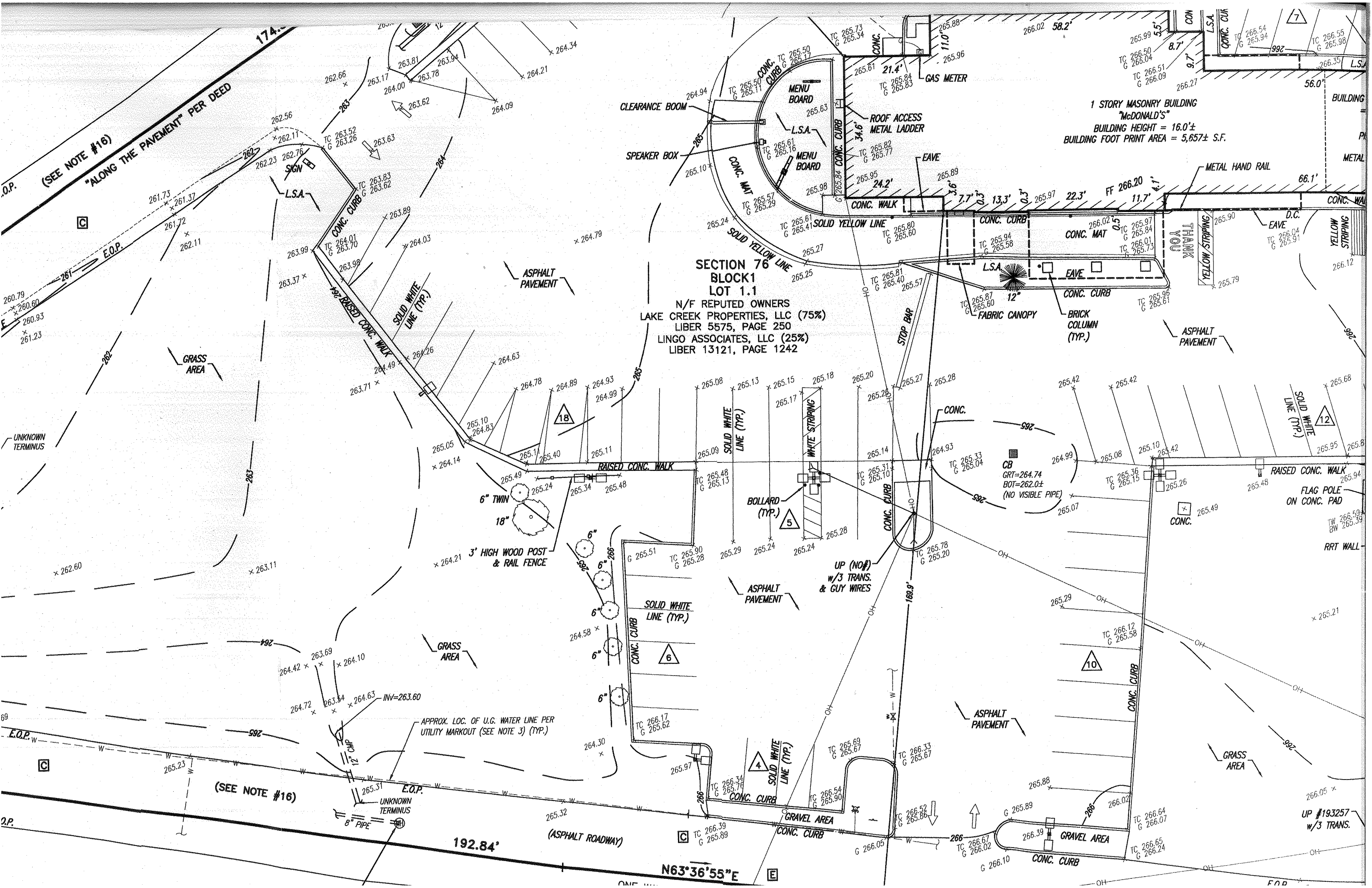
TO: McDONALD'S CORPORATION, A DELAWARE CORPORATION; McDONALD'S REAL ESTATE COMPANY, A DELAWARE CORPORATION; McDONALD'S USA, LLC, A DELAWARE LIMITED LIABILITY COMPANY; AND CHICAGO TITLE INSURANCE COMPANY

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT WAS BASED WERE MADE IN ACCORDANCE WITH THE "2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS", JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(b), 7(a), 7(b)(1), 7(c), 8, 9, 11(b), 13, 14 & 17 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON FEBRUARY 1, 2012.



NOT A VALID ORIGINAL DOCUMENT UNLESS EMBOSSED WITH A BLUE INK SEAL

James C. Weed



(SEE NOTE #16)
"ALONG THE PAVEMENT" PER DEED

1 STORY MASONRY BUILDING
"McDONALD'S"
BUILDING HEIGHT = 16.0'±
BUILDING FOOT PRINT AREA = 5,657± S.F.

SECTION 76
BLOCK 1
LOT 1.1
N/F REPUTED OWNERS
LAKE CREEK PROPERTIES, LLC (75%)
LIBER 5575, PAGE 250
LINGO ASSOCIATES, LLC (25%)
LIBER 13121, PAGE 1242

(SEE NOTE #16)

192.84'

N63°36'55"E

UP #193257
W/3 TRANS.

APPROX. LOC. OF U.G. WATER LINE PER
UTILITY MARKOUT (SEE NOTE 3) (TYP.)

UNKNOWN
TERMINUS

E.O.P.
E.O.P.
E.O.P.

BUILDING
METAL

BUILDING

CONC. WALK

YELLOW STRIPING

CONC. WALK

ASPHALT PAVEMENT

ASPHALT PAVEMENT

SOLID WHITE LINE (TYP.)

SOLID WHITE LINE (TYP.)

RAISED CONC. WALK

FLAG POLE ON CONC. PAD

RRT WALL

ASPHALT PAVEMENT

ASPHALT PAVEMENT

ASPHALT PAVEMENT

ASPHALT PAVEMENT

ASPHALT PAVEMENT

ASPHALT PAVEMENT

ASPHALT PAVEMENT

E.O.P.

C

UNKNOWN TERMINUS

C

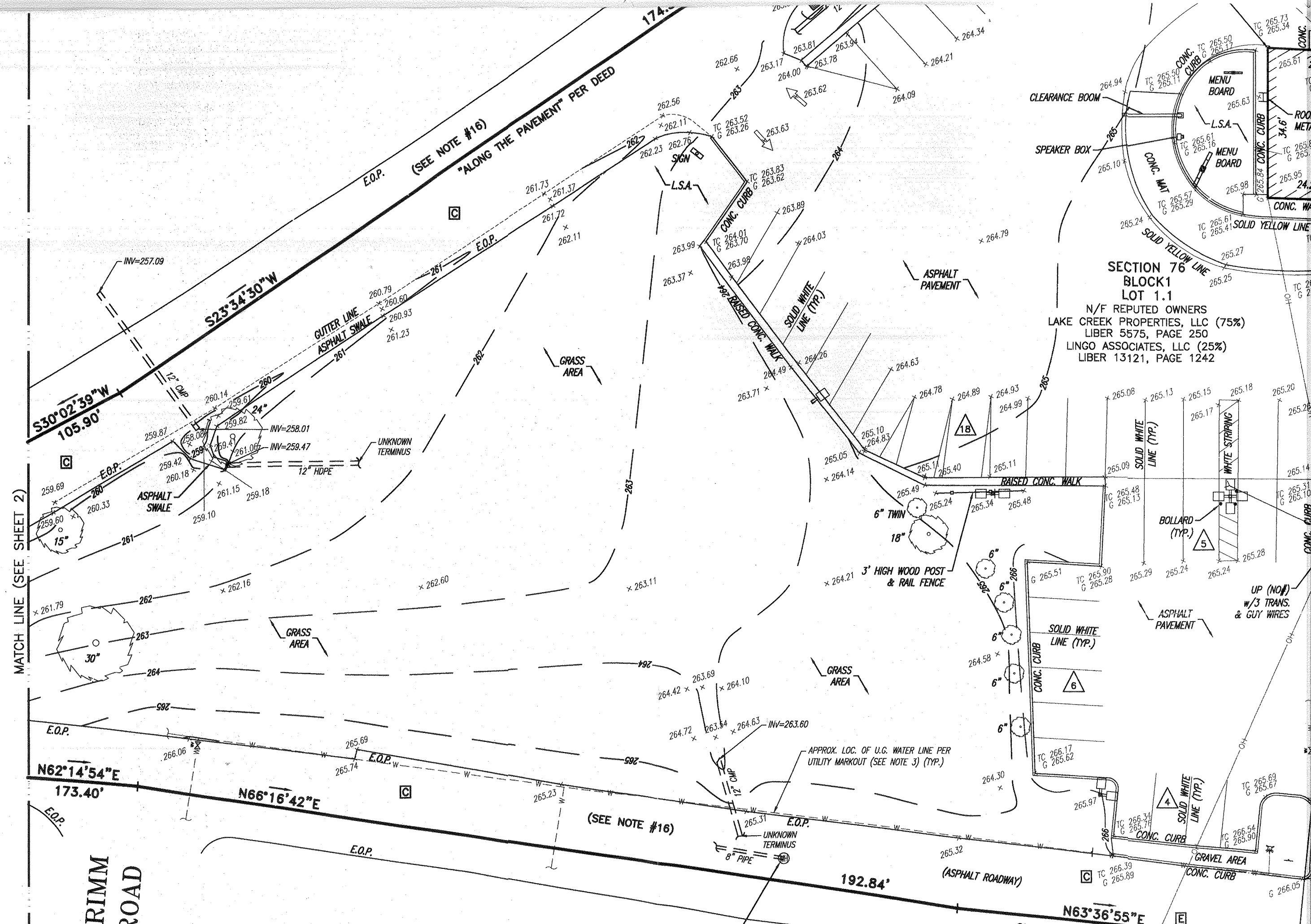
Q.P.

C

E

E.O.P.

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SECTION 76
BLOCK 1
LOT 1.1
N/F REPUTED OWNERS
LAKE CREEK PROPERTIES, LLC (75%)
LIBER 5575, PAGE 250
LINGO ASSOCIATES, LLC (25%)
LIBER 13121, PAGE 1242

MATCH LINE (SEE SHEET 2)

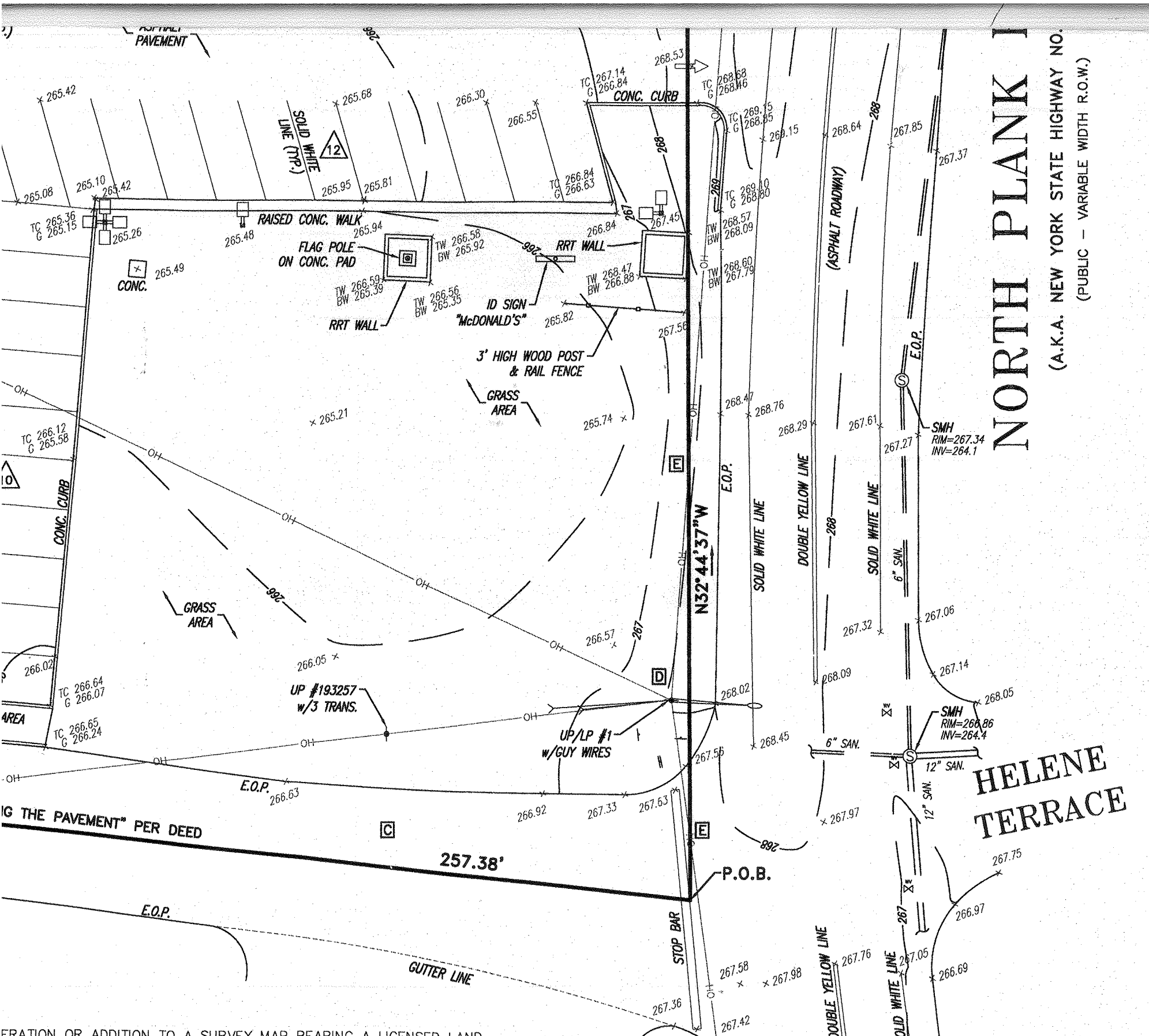
(SEE NOTE #16)

APPROX. LOC. OF U.G. WATER LINE PER UTILITY MARKOUT (SEE NOTE 3) (TYP.)

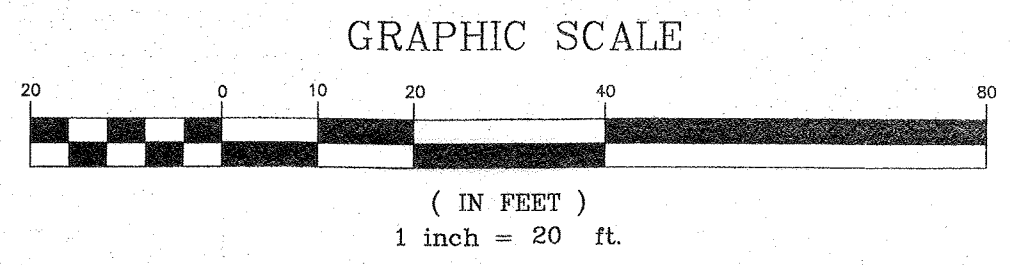
RIMM ROAD

192.84'

N63°36'55\"E

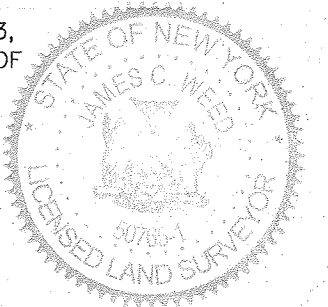


SEE SHEET 2 FOR NOTES, REFERENCES, DESCRIPTIONS AND ZONING TABLE



TO: McDONALD'S CORPORATION, A DELAWARE CORPORATION; McDONALD'S REAL ESTATE COMPANY, A DELAWARE CORPORATION; McDONALD'S USA, LLC, A DELAWARE LIMITED LIABILITY COMPANY; AND CHICAGO TITLE INSURANCE COMPANY

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT WAS BASED WERE MADE IN ACCORDANCE WITH THE "2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS", JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(b), 7(a), 7(b)(1), 7(c), 8, 9, 11(b), 13, 14 & 17 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON FEBRUARY 1, 2012.



NOT A VALID ORIGINAL DOCUMENT UNLESS EMBOSSED WITH A BLUE INK SEAL

James C. Weed
JAMES C. WEED
 NEW YORK PROFESSIONAL LAND SURVEYOR #50765
 DATE 10-02-2013

FIELD DATE
10-20-11
 FIELD BOOK NO.
11-21
 FIELD BOOK PG.
78

ALTA/ACSM LAND TITLE SURVEY
McDONALD'S USA, LLC
 187 NORTH PLANK ROAD
 LOT 1.1, BLOCK 1, SECTION 76
 TOWN OF NEWBURGH
 ORANGE COUNTY, NEW YORK
 L/C #031-0145

FIELD CREW
T.M./B.A.
 DRAWN:
J.P.

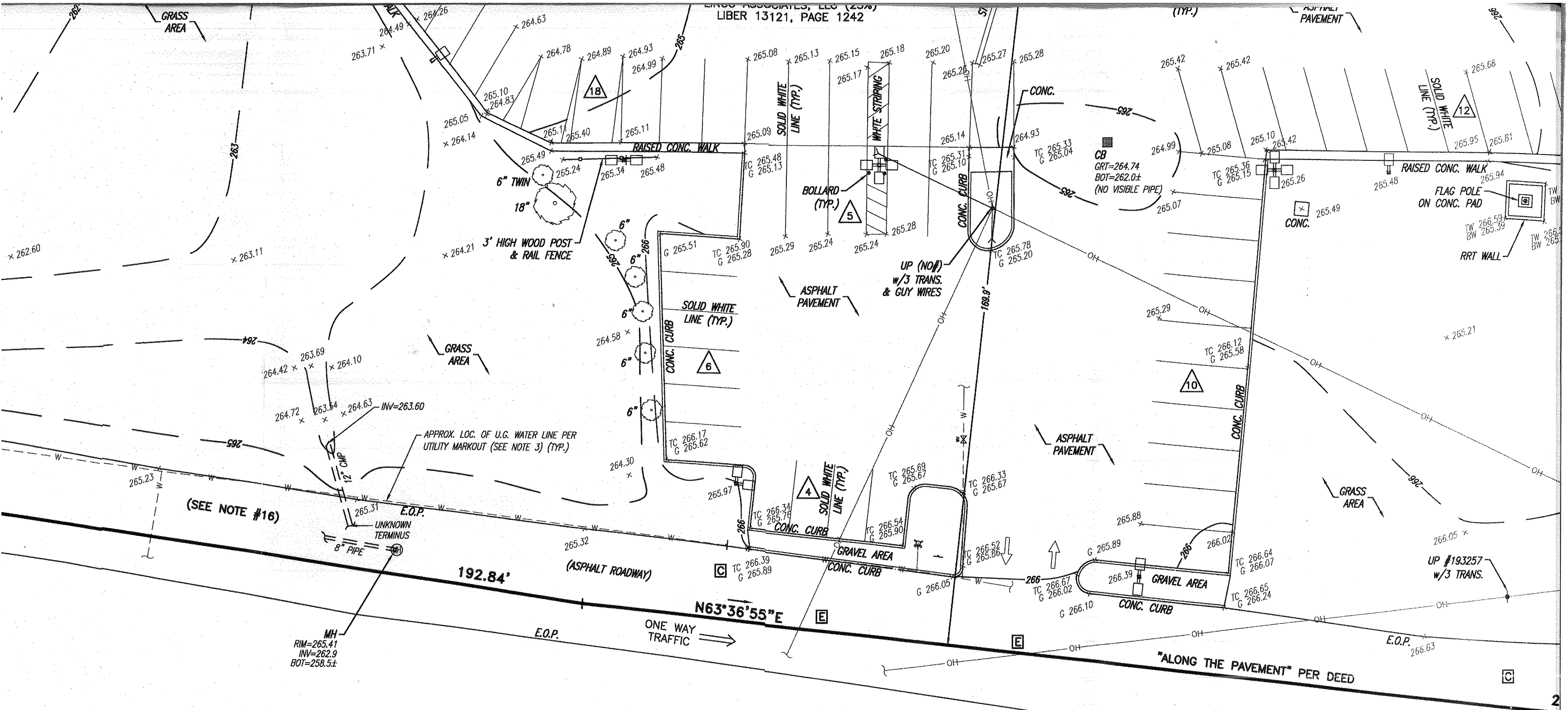
CONTROL POINT ASSOCIATES, INC.
 35 TECHNOLOGY DRIVE
 WARREN, NJ 07059
 908.668.0099 - 908.668.9595 FAX
 CHALFONT, PA 215.712.9800
 SOUTHBOROUGH, MA 508.948.3000

OPERATION OR ADDITION TO A SURVEY MAP BEARING A LICENSED LAND SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7209, SUB-DIVISION 2, OF THE NEW YORK STATE SURVEYS AND MAPPING LAW.

THE ORIGINAL OF THIS SURVEY MARKED WITH AN ORIGINAL OF THE SURVEYOR'S EMBOSSED SEAL SHALL BE CONSIDERED TO BE VALID TRUE COPIES.

No.	DESCRIPTION OF REVISION	DRAWN:	APPROVED:	DATE
3	REVISE SIGNATURE	W.P.H.	J.C.W.	10-2-13
2	REVISE TO SHOW TREES	J.P.	J.D.B.	3-8-12
1	REVISE WITH RECEIPT OF TITLE	W.P.H.	J.D.B.	2-28-12

REVIEWED:	APPROVED:	DATE	SCALE	FILE NO.	DWG. NO.
W.P.H.	J.D.B.	10-26-11	1"=20'	C11443	1 OF 2



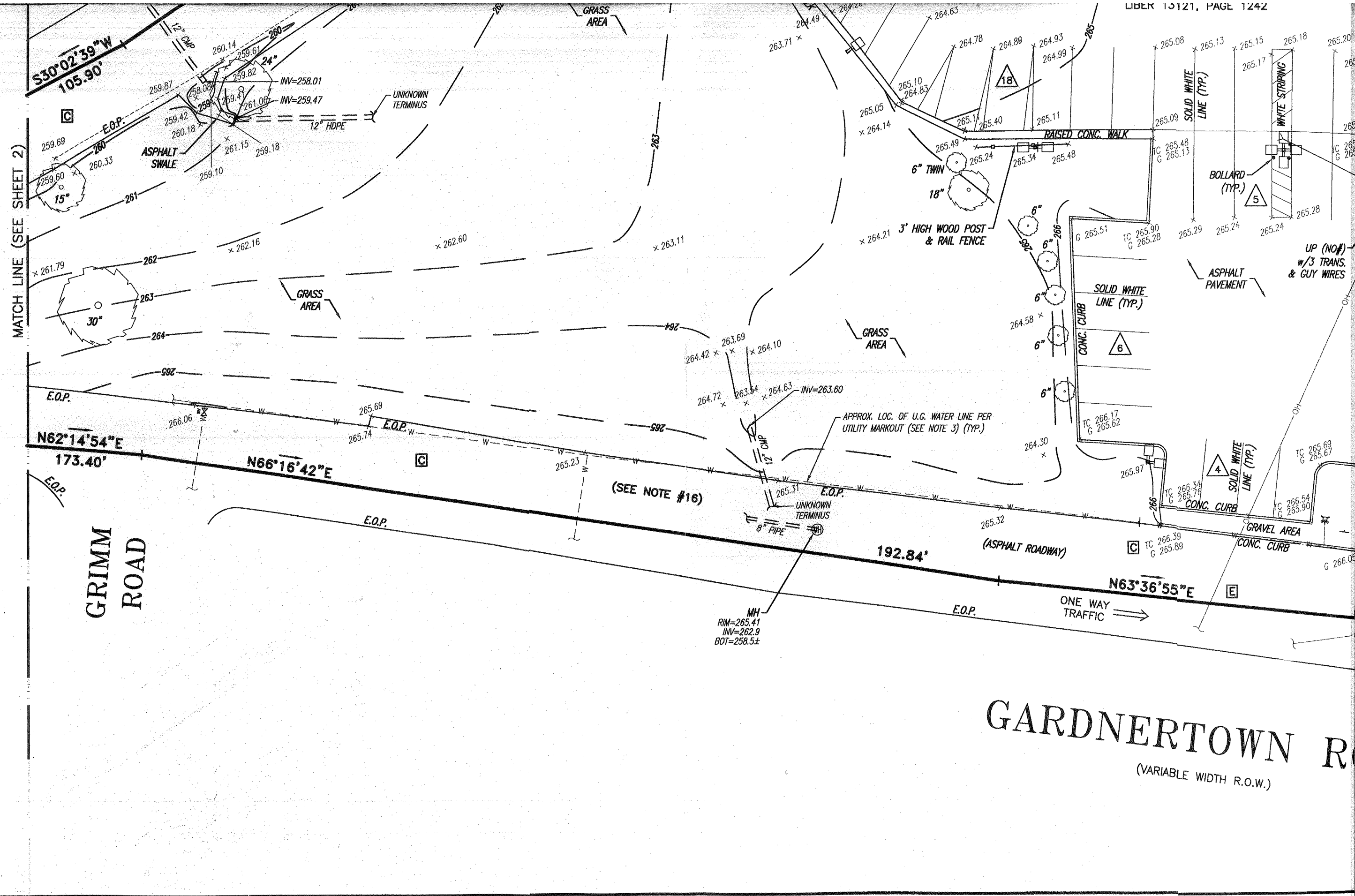
GARDNERTOWN ROAD

(VARIABLE WIDTH R.O.W.)

UNAUTHORIZED ALTERATION OR ADDITION TO A SURVEY MAP BEARING A LICENSED SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7209, SUB-DIVISION 2, OF THE NEW STATE EDUCATION LAW.

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GARDNERTOWN ROAD
(VARIABLE WIDTH R.O.W.)

MATCH LINE (SEE SHEET 2)

MATCH LINE (SEE SHEET 3)

$S30^{\circ}02'39''W$
105.90'

$N62^{\circ}14'54''E$
173.40'

$N66^{\circ}16'42''E$

192.84'

$N63^{\circ}36'55''E$

ONE WAY TRAFFIC

MH
RIM=265.41
INV=262.9
BOT=258.5±

(SEE NOTE #16)

APPROX. LOC. OF U.G. WATER LINE PER
UTILITY MARKOUT (SEE NOTE 3) (TYP.)

UNKNOWN
TERMINUS

UNKNOWN
TERMINUS

BOLLARD
(TYP.)

UP (NO#)
w/3 TRANS.
& GUY WIRES

ASPHALT
SWALE

GRASS
AREA

GRASS
AREA

ASPHALT
PAVEMENT

RAISED CONC. WALK

6" TWIN
18"
3' HIGH WOOD POST
& RAIL FENCE

SOLID WHITE
LINE (TYP.)

CONC. CURB

SOLID WHITE
LINE (TYP.)

CONC. CURB

GRAVEL AREA
CONC. CURB

15"

30"

6"

6"

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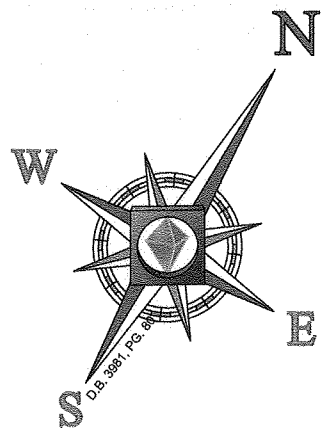
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6"

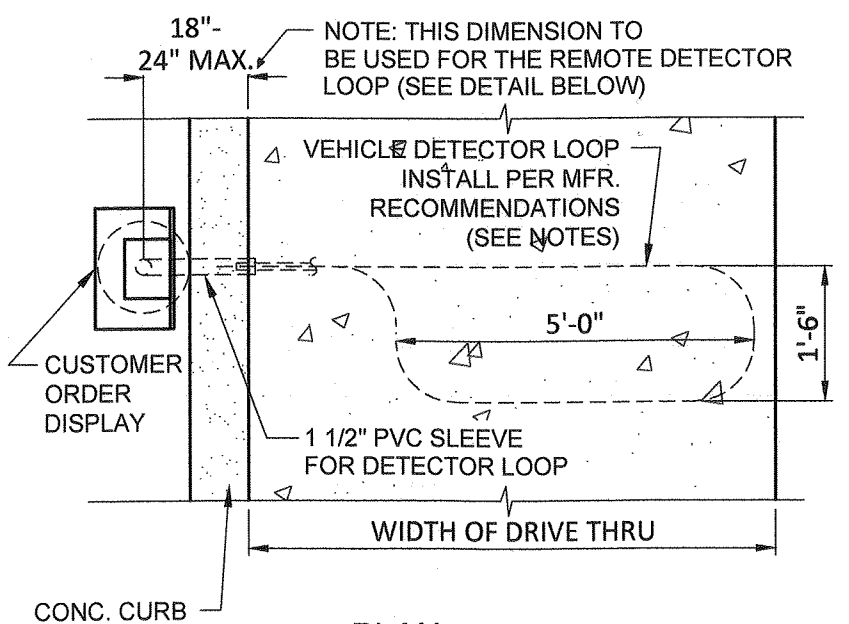
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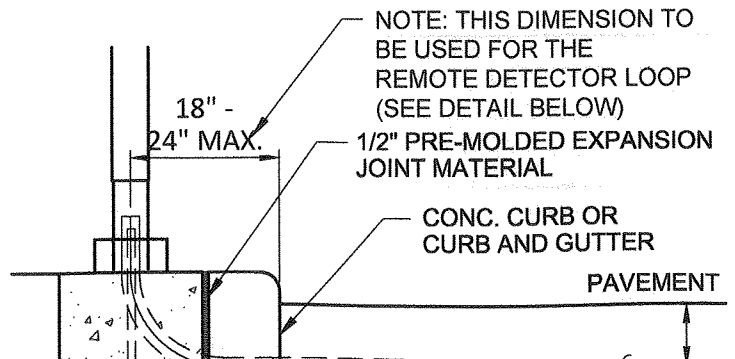
6"



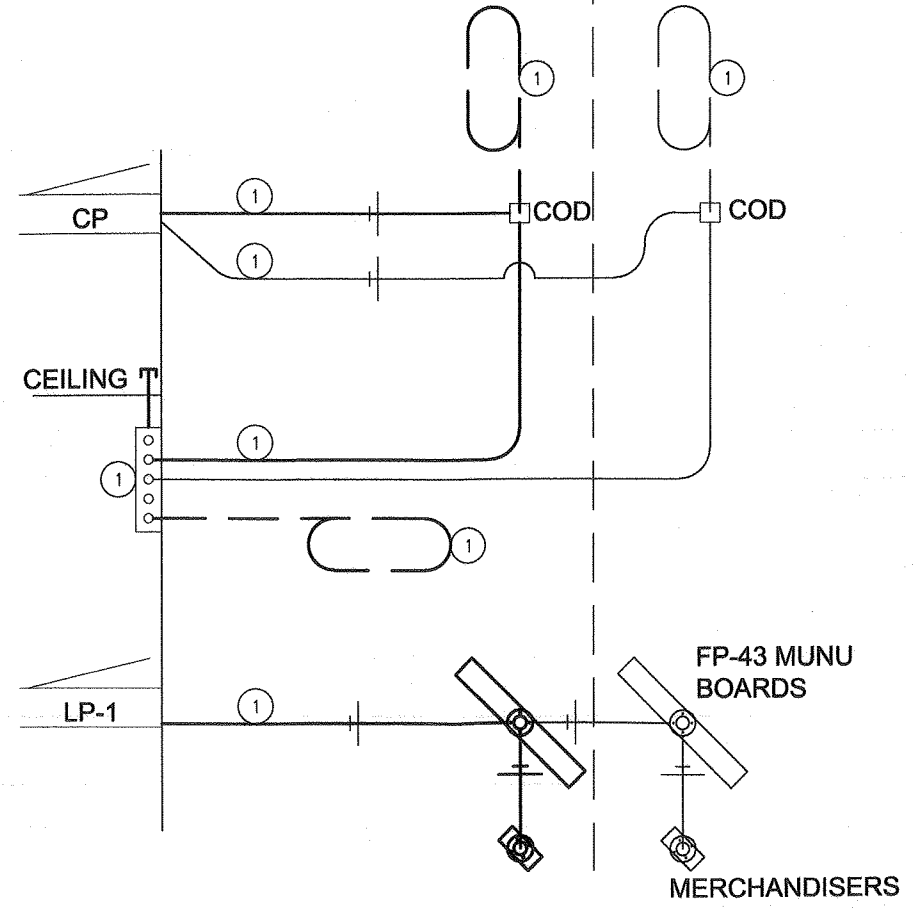
SEE DRIVE THRU CONCRETE PAD DETAIL FOR DIMENSIONS OF DRIVE-THRU LANE CONCRETE PAD FOR DETECTOR LOOP.
NOTE: NO STEEL IS TO BE USED IN CONCRETE.



PLAN



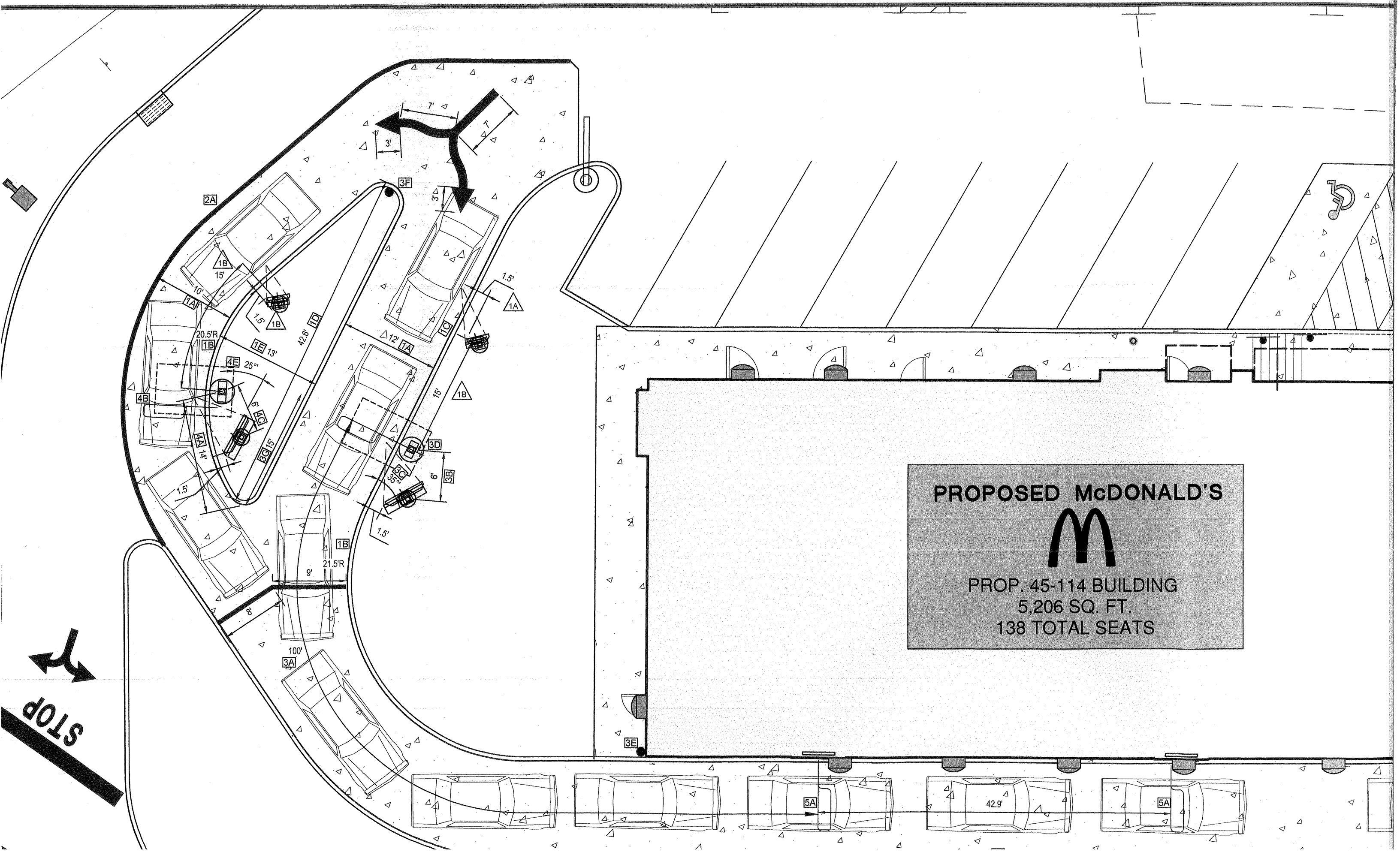
OPTIONAL SECONDARY DOUBLE DRIVE THRU



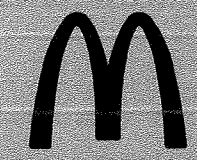
PROVIDE PER POS & COD ISOLATED GROUND/DEDICATED CIRCUIT DETAIL. 1/2"C-2#12 & 1#12 GROUND & 1 #12 ISOLATED GROUND

1 1/2"C FOR REMOTE DETECTOR & COD AUDIO & DATA CABLES. SEE REMOTE VEHICLE DETECTOR WIRING DIAGRAM (3/DT1.0). EC TO PROVIDE 3 PULL STRINGS INSIDE CONDUIT.





PROPOSED McDONALD'S

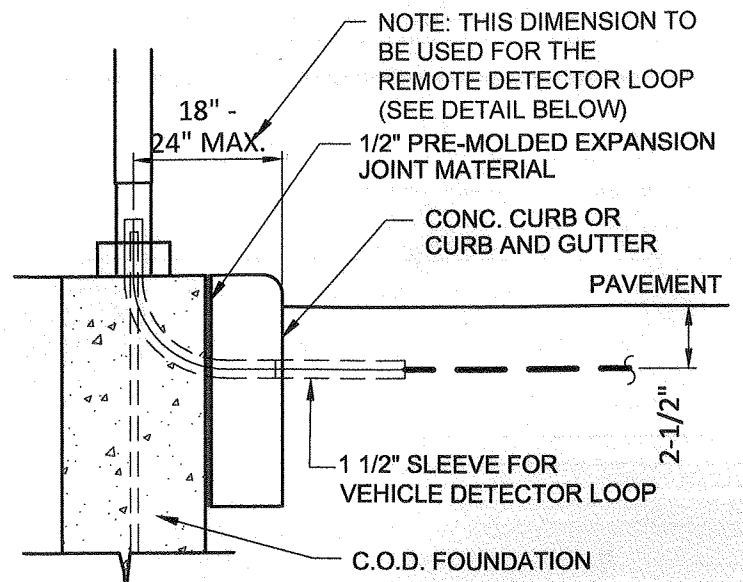


PROP. 45-114 BUILDING
5,206 SQ. FT.
138 TOTAL SEATS

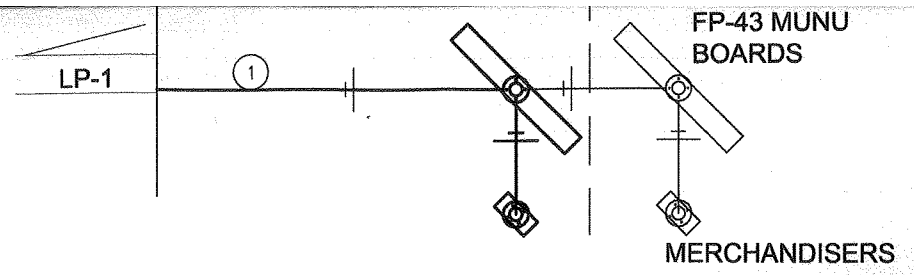
STOP

CONC. CURB

PLAN



NOTE: FOR REMOTE DETECTOR LOOP ADD CONCRETE PAD, J-BOX AND 24" MAX. DIST. FROM C CONDUIT TO EDGE OF CURB



PROVIDE PER POS & COD ISOLATED GROUND/DEDICATED CIRCUIT DETAIL. 1/2"C-2#12 & 1#12 GROUND & 1 #12 ISOLATED GROUND

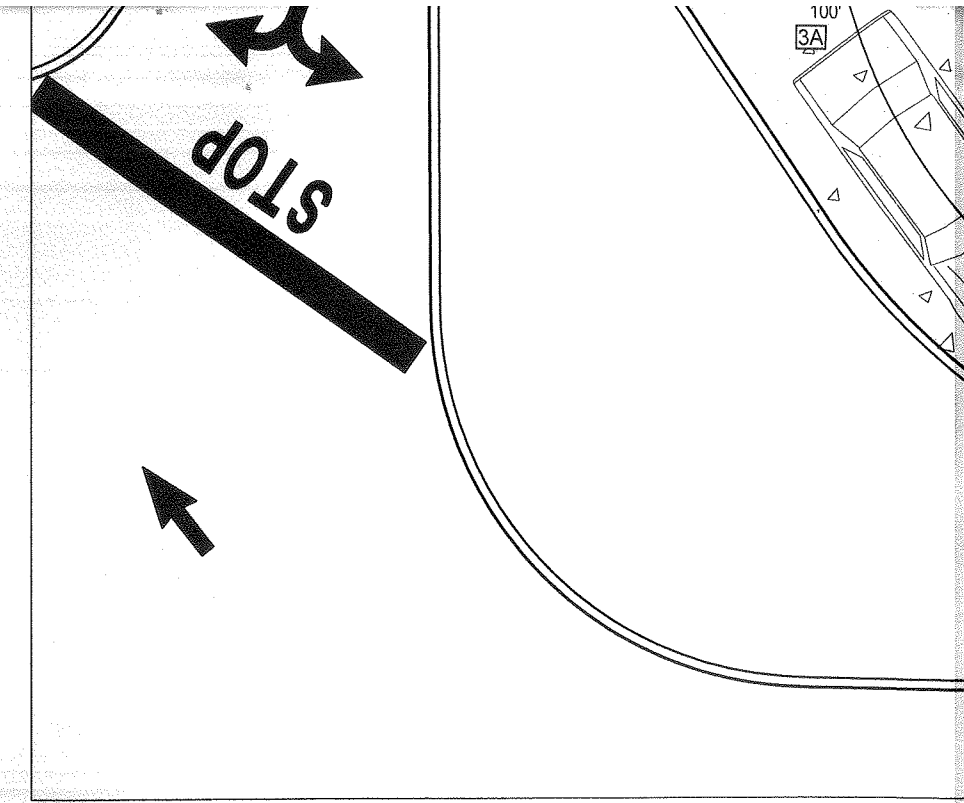
1 1/2"C FOR REMOTE DETECTOR & COD AUDIO & DATA CABLES. SEE REMOTE VEHICLE DETECTOR WIRING DIAGRAM (3/DT1.0). EC TO PROVIDE 3 PULL STRINGS INSIDE CONDUIT.

2 1/2"C FOR COD LOOP DETECTOR, ELECTRICAL EQUIPMENT ITEM #217.11E1

2 1/2"C FOR ORDER/CASH BOOTH LOOP DETECTOR, ELECTRICAL EQUIPMENT ITEM #217.11E1

215.02E4 IN ORDER/CASH BOOTH. PROVIDE 2" HOLE & GROMMET IN JUNCTION BOX COVER PLATE FOR POS DATA CABLES. PROVIDE 2 1/2"C ABOVE CEILING.

1/2"C-2#12, POWER FOR ILLUMINATION OF MENUBOARD



1

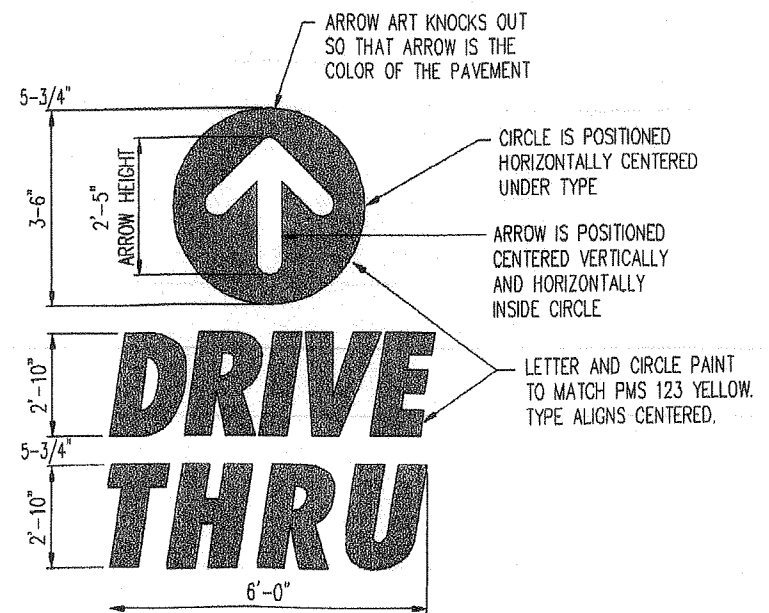
AUTO DETECTOR LOOP DETAIL

NOT TO SCALE (SOURCE: McDONALD'S)

2

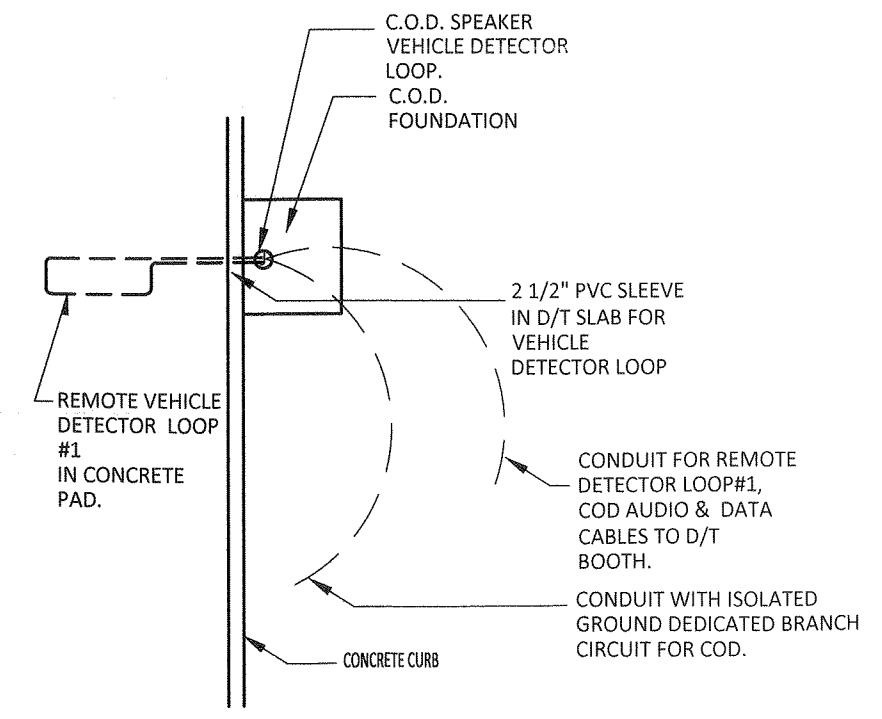
DRIVE-THRU SITE WIRING DIAGRAM

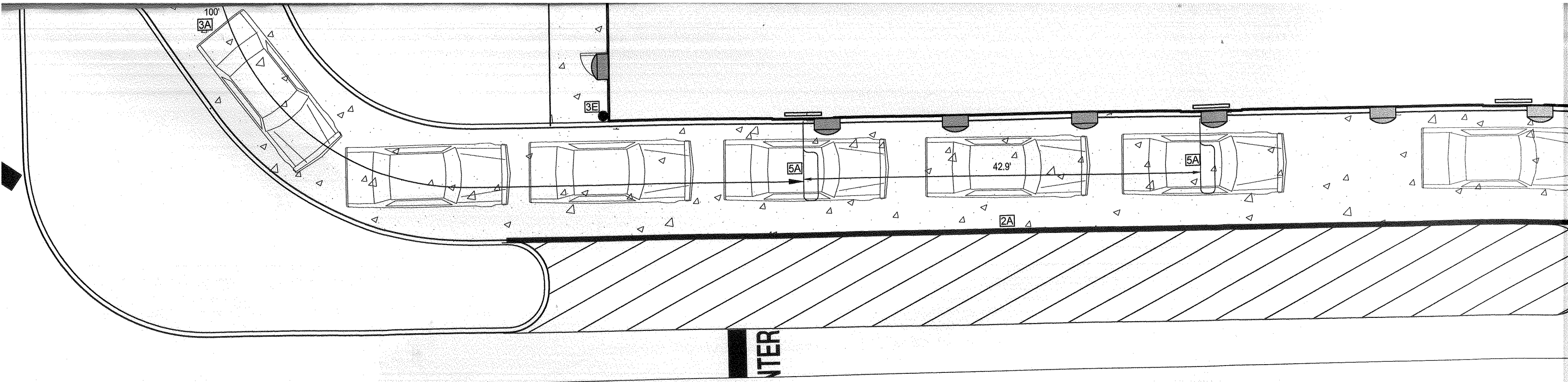
NOT TO SCALE (SOURCE: McDONALD'S)



PAINTED TRAFFIC ARROWS & LETTERS DETAIL

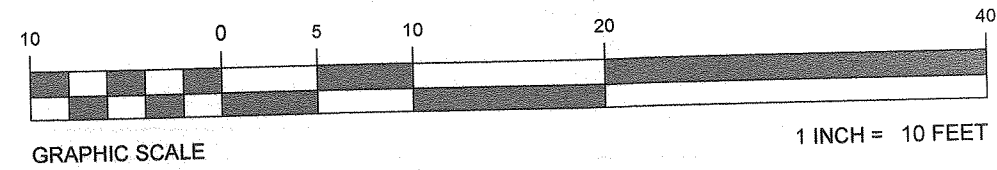
NO SCALE





DRIVE-THRU ENLARGEMENT

SCALE : 1" = 10'



SIDE BY SIDE DRIVE-THRU STANDARD A

1. SIDE BY SIDE DRIVE-THRU STANDARD A CURBING DETAILS:

- 1A DRIVE-THRU LANES BOUND BY CURB ON BOTH SIDES ARE TO BE 12'-0". LANES BOUND BY CURB ON ONE SIDE AND PAINTED STRIPING ON THE OTHER SIDE ARE TO BE A MIN. OF 10'-0". 10' & 12' SHOWN, COMPLIES
- 1B THE MIN. RADIUS FOR ALL INSIDE/DRIVER'S SIDE DRIVE-THRU CURBING IS 20'-0" TO INSIDE OF CURB. 21.5' SHOWN, COMPLIES
- 1C PRIMARY LANE CURBING SHOULD BE AS STRAIGHT AS POSSIBLE. (LESS CURVING, THE BETTER). COMPLIES
- 1D THE OVERALL LENGTH OF THE CURBED ISLAND SHOULD BE 35'-45'. THE LENGTH OF THE ISLAND FROM THE COD ALLOWS FOR THREE CARS IN THE SECONDARY LANE, TWO IN THE PRIMARY LANE AND ONE AT THE COMMITMENT POINT. 42.8' SHOWN, COMPLIES
- 1E THE ISLAND WIDTH SHOULD BE 13' AT THE WIDEST POINT (FACE OF CURB TO FACE OF CURB). 13' SHOWN, COMPLIES

2. SIDE BY SIDE DRIVE-THRU STANDARD A PAVEMENT MARKINGS:

- 2A 6" WIDE YELLOW PAINT STRIPE TO SPAN OUTER EDGE OF THE ENTIRE DRIVE-THRU LANE. LANE STARTS AT DRIVE-THRU ENTRANCE WHERE "MCDONALD'S GATEWAY" IS LOCATED. COMPLIES
- 2B DOUBLE-HEADED ARROW PAVEMENT MARKING. STANDARD STRIPING MARKINGS ARE 7' SHAFT, 7' ARROW STEM AND 3' FOR THE ARROW HEAD. TIP OF ARROW HEAD TO BE LOCATED AT CENTER OF EACH LANE. COMPLIES
- 2C MERGE POINT IS LOCATED WHERE TWO VEHICLES LEAVING EACH COD SIMULTANEOUSLY WOULD MEET. THE MERGE POINT STRIPING IS TO BE LOCATED BY OFFSETTING THE INNER PRIMARY LANE BACK OF CURB 9.0' AND OFFSETTING THE OUTER LANE BACK OF CURB 11.0'. AT THE INTERSECTION OF THESE OFFSETS, A 6" YELLOW STRIPE IS TO BE MARKED PERPENDICULAR TO THE OUTER LANE AS WELL AS THE INNER PRIMARY LANE. COMPLIES

3. SIDE BY SIDE DRIVE-THRU STANDARD A EQUIPMENT POSITIONING FOR PRIMARY LANE:

- 3E A SINGLE BOLLARD SHOULD BE POSITIONED AT THE CORNER OF THE BUILDING ON THE DRIVE-THRU SIDE. IT SHOULD BE FLUSH AGAINST THE BUILDING AND FACE OF THE BOLLARD SHOULD BE TIGHT AGAINST THE BACK OF THE CURB. COMPLIES
- 3F AUGER "MCDONALD'S GATEWAY" FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES
- 3G THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY COD MUST BE 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE. 15' SHOWN, COMPLIES
- 3H THE PRIMARY LANE DETECTOR LOOP SHOULD BE PERPENDICULAR TO THE CENTER OF THE PRIMARY COD.

4. SIDE BY SIDE DRIVE-THRU STANDARD A EQUIPMENT POSITIONING FOR SECONDARY LANE:

- 4A TO POSITION THE SECONDARY COD, DRAW AN ARC WITH A 14' RADIUS THAT IS CENTERED FROM THE MIDPOINT OF THE ISLAND TIP. THEN OFFSET THE FACE OF THE CURB BY 24" TO DETERMINE THE LOCATION OF CENTER OF FOUNDATION OF THE SECONDARY COD. 14' SHOWN, COMPLIES
- 4B THE LOOP DETECTOR IS TO BE 2' FORWARD OF THE COD CENTER LINE WITH THE LOOP FACING FORWARD AND THE DETECTOR LOOP PERPENDICULAR TO THE SECONDARY COD WHEN POSSIBLE. COMPLIES
- 4C THE CENTER OF THE SECONDARY MENU BOARD FOUNDATION SHALL BE 5.3' (5.0' MIN. AND 6.0' MAX.) FROM THE CENTER OF THE COD FOUNDATION WITH THE END CAP OF THE SECONDARY ,EMU BOARD NOT LESS THAN 12" FROM THE FACE PF THE CURB. 6.0' SHOWN, COMPLIES
- 4D AUGER "MCDONALD'S ORDER HERE CANOPY" COD/CANOPY FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES
- 4E THE SECONDARY MENU SHALL BE AT AN ANGLE OF APPROXIMATELY 25° FROM A VEHICLE POSITIONED AT THE COD WITH 100% VISIBILITY. 25° SHOWN, COMPLIES

5. SIDE BY SIDE DRIVE-THRU STANDARD D DETECTOR LOOP:

- 5A DETECTOR LOOPS SHALL BE LOCATED AT THE CENTER OF THE OPENING WINDOW AT THE

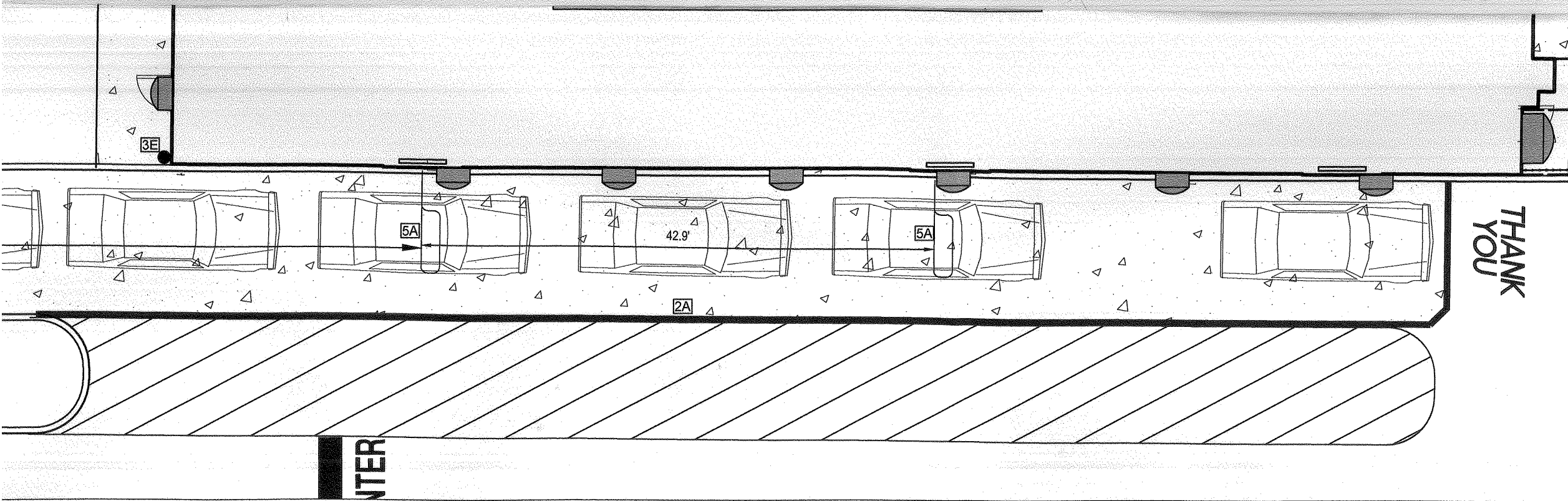
C.O.D. SPEAKER
VEHICLE DETECTOR
LOOP.
C.O.D.
FOUNDATION

2 1/2" PVC SLEEVE
IN D/T SLAB FOR
VEHICLE
DETECTOR LOOP

CONDUIT FOR REMOTE
DETECTOR LOOP#1,
COD AUDIO & DATA
CABLES TO D/T
BOOTH.

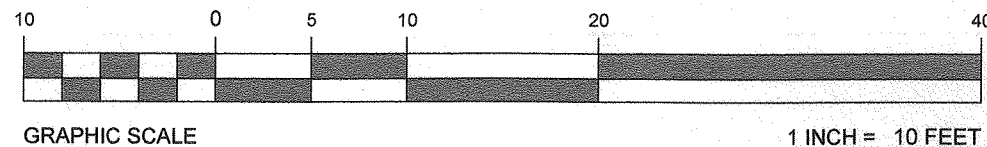
CONDUIT WITH ISOLATED
GROUND DEDICATED BRANCH
CIRCUIT FOR COD.

CONCRETE CURB



DRIVE-THRU ENLARGEMENT

SCALE : 1" = 10'



DRIVE-THRU STANDARD A

STANDARD A CURBING DETAILS:
 CURB ON BOTH SIDES ARE TO BE 12'-0". LANES
 SIDE AND PAINTED STRIPING ON THE OTHER SIDE ARE
 & 12' SHOWN, COMPLIES

INSIDE/DRIVER'S SIDE DRIVE-THRU CURBING
 3. 21.5' SHOWN, COMPLIES

SHOULD BE AS STRAIGHT AS POSSIBLE.
 (ER). COMPLIES

THE CURBED ISLAND SHOULD BE 35'-45'. THE
 FROM THE COD ALLOWS FOR THREE CARS IN THE
 THE PRIMARY LANE AND ONE AT THE
 SHOWN, COMPLIES

BE 13' AT THE WIDEST POINT (FACE OF CURB TO
 IN, COMPLIES

STANDARD A PAVEMENT MARKINGS:
 RIPE TO SPAN OUTER EDGE OF THE ENTIRE DRIVE-THRU LANE. LANE
 ENTRANCE WHERE "MCDONALD'S GATEWAY" IS LOCATED. COMPLIES

PAVEMENT MARKING. STANDARD STRIPING MARKINGS ARE 7" SHAFT,
 R THE ARROW HEAD. TIP OF ARROW HEAD TO BE LOCATED AT
 COMPLIES

WHERE TWO VEHICLES LEAVING EACH COD SIMULTANEOUSLY
 POINT STRIPING IS TO BE LOCATED BY OFFSETTING THE INNER
 CURB 9.0' AND OFFSETTING THE OUTER LANE BACK OF CURB 11.0'. AT
 THESE OFFSETS, A 6" YELLOW STRIPE IS TO BE MARKED PERPENDICULAR
 AS THE INNER PRIMARY LANE. COMPLIES

- 3E A SINGLE BOLLARD SHOULD BE POSITIONED AT THE CORNER OF THE BUILDING ON THE DRIVE-THRU SIDE. IT SHOULD BE FLUSH AGAINST THE BUILDING AND FACE OF THE BOLLARD SHOULD BE TIGHT AGAINST THE BACK OF THE CURB. COMPLIES
 - 3F AUGER "MCDONALD'S GATEWAY" FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES
 - 3G THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY COD MUST BE 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE. 15' SHOWN, COMPLIES
 - 3H THE PRIMARY LANE DETECTOR LOOP SHOULD BE PERPENDICULAR TO THE CENTER OF THE PRIMARY COD.
4. SIDE BY SIDE DRIVE-THRU STANDARD A EQUIPMENT POSITIONING FOR SECONDARY LANE:
- 4A TO POSITION THE SECONDARY COD, DRAW AN ARC WITH A 14' RADIUS THAT IS CENTERED FROM THE MIDPOINT OF THE ISLAND TIP. THEN OFFSET THE FACE OF THE CURB BY 24" TO DETERMINE THE LOCATION OF CENTER OF FOUNDATION OF THE SECONDARY COD. 14' SHOWN, COMPLIES
 - 4B THE LOOP DETECTOR IS TO BE 2' FORWARD OF THE COD CENTER LINE WITH THE LOOP FACING FORWARD AND THE DETECTOR LOOP PERPENDICULAR TO THE SECONDARY COD WHEN POSSIBLE. COMPLIES
 - 4C THE CENTER OF THE SECONDARY MENU BOARD FOUNDATION SHALL BE 5.3' (5.0' MIN. AND 6.0' MAX.) FROM THE CENTER OF THE COD FOUNDATION WITH THE END CAP OF THE SECONDARY MENU BOARD NOT LESS THAN 12" FROM THE FACE OF THE CURB. 6.0' SHOWN, COMPLIES
 - 4D AUGER "MCDONALD'S ORDER HERE CANOPY" COD/CANOPY FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES
 - 4E THE SECONDARY MENU SHALL BE AT AN ANGLE OF APPROXIMATELY 25° FROM A VEHICLE POSITIONED AT THE COD WITH 100% VISIBILITY. 25° SHOWN, COMPLIES

FINAL PLAN SIGN

P.M. _____

G.C. _____

O/O _____

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APPROVALS	DATE

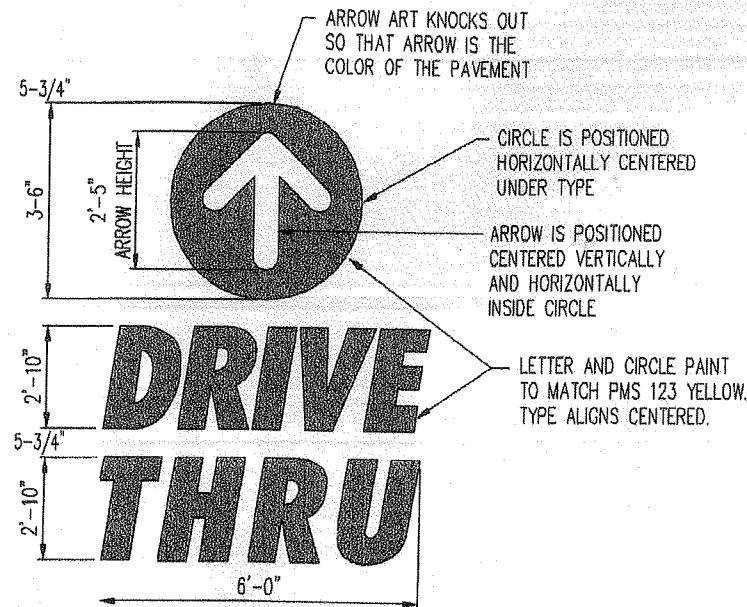
RE (2 REQUIRED)

SIGNATURES

1

AUTO DETECTOR LOOP DETAIL

NOT TO SCALE (SOURCE: McDONALD'S)



PAINTED TRAFFIC ARROWS & LETTERS DETAIL
NO SCALE

NOTE: ALL FONTS LETTERS TO BE "FUTURA CONDENSED EXTRA BOLD OBLIQUE"

2

DRIVE-THRU SITE WIRING DIAGRAM

NOT TO SCALE (SOURCE: McDONALD'S)



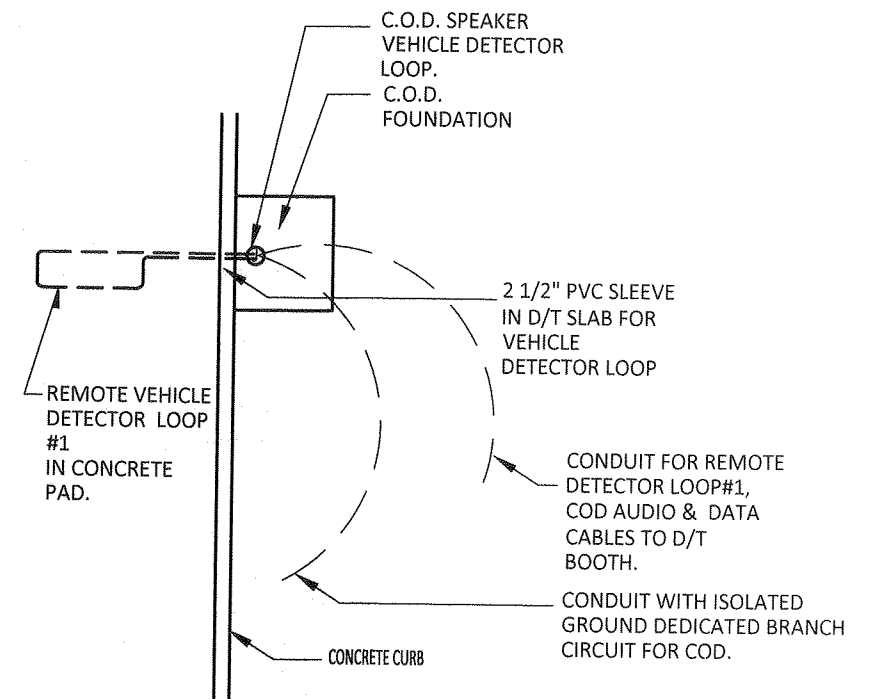
'THANK YOU' PAVEMENT MARKING

NOT TO SCALE (SOURCE: McDONALD'S)

5

AUTO DETECTOR WIRING DIAGRAM

NOT TO SCALE (SOURCE: McDONALD'S)



G:\2011\110699\DRAWINGS\CURRENT DRAWINGS\SITE PLAN PACKAGE\110699-SPP-08->LAYOUT: C-11 DT

KF

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THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTH OF UTILITIES, PIPING, DRYWELLS, ETC. PRIOR TO THE START OF WORK. THE CONTRACTOR MUST CONTACT THE APPROPRIATE LOCAL "ONE CALL" SYSTEM TO ENSURE THAT ALL UTILITIES ARE PROPERLY AND COMPLETELY MARKED OUT IN THE FIELD PRIOR TO ANY WORK AT THE SITE. ANY DISCREPANCIES BETWEEN THE FIELD LOCATIONS AND THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY. THE CONTRACTOR IS ADVISED THAT UTILITY INFORMATION SHOWN ON THE PLAN IS A COMPILATION OF FIELD LOCATIONS BY VARIOUS UTILITY AGENCIES AND RECORD DRAWINGS AND MUST BE CONFIRMED ACCORDINGLY. THE CONTRACTOR IS ALSO ADVISED THAT ALL SUCH FACILITIES DISTURBED DURING CONSTRUCTION MUST BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.

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DRIVE-THRU ENLARGEMENT

SCALE : 1" = 10'



GRAPHIC SCALE

1 INCH = 10 FEET

O.D. SPEAKER
VEHICLE DETECTOR
LOOP.
O.D.
FOUNDATION

2 1/2" PVC SLEEVE
IN D/T SLAB FOR
VEHICLE
DETECTOR LOOP

CONDUIT FOR REMOTE
DETECTOR LOOP#1,
COD AUDIO & DATA
CABLES TO D/T
BOOTH.

CONDUIT WITH ISOLATED
GROUND DEDICATED BRANCH
CIRCUIT FOR COD.

CURB

DETECTOR COD DIAGRAM

(SOURCE: McDONALD'S)

SIDE BY SIDE DRIVE-THRU STANDARD A

1. SIDE BY SIDE DRIVE-THRU STANDARD A CURBING DETAILS:

- 1A DRIVE-THRU LANES BOUND BY CURB ON BOTH SIDES ARE TO BE 12'-0". LANES BOUND BY CURB ON ONE SIDE AND PAINTED STRIPING ON THE OTHER SIDE ARE TO BE A MIN. OF 10'-0". 10' & 12' SHOWN, COMPLIES
- 1B THE MIN. RADIUS FOR ALL INSIDE/DRIVER'S SIDE DRIVE-THRU CURBING IS 20'-0" TO INSIDE OF CURB. 21.5' SHOWN, COMPLIES
- 1C PRIMARY LANE CURBING SHOULD BE AS STRAIGHT AS POSSIBLE. (LESS CURVING, THE BETTER). COMPLIES
- 1D THE OVERALL LENGTH OF THE CURBED ISLAND SHOULD BE 35'-45'. THE LENGTH OF THE ISLAND FROM THE COD ALLOWS FOR THREE CARS IN THE SECONDARY LANE, TWO IN THE PRIMARY LANE AND ONE AT THE COMMITMENT POINT. 42.6' SHOWN, COMPLIES
- 1E THE ISLAND WIDTH SHOULD BE 13' AT THE WIDEST POINT (FACE OF CURB TO FACE OF CURB). 13' SHOWN, COMPLIES

2. SIDE BY SIDE DRIVE-THRU STANDARD A PAVEMENT MARKINGS:

- 2A 6" WIDE YELLOW PAINT STRIPE TO SPAN OUTER EDGE OF THE ENTIRE DRIVE-THRU LANE. LANE STARTS AT DRIVE-THRU ENTRANCE WHERE "MCDONALD'S GATEWAY" IS LOCATED. COMPLIES
- 2B DOUBLE-HEADED ARROW PAVEMENT MARKING. STANDARD STRIPING MARKINGS ARE 7' SHAFT, 7' ARROW STEM AND 3' FOR THE ARROW HEAD. TIP OF ARROW HEAD TO BE LOCATED AT CENTER OF EACH LANE. COMPLIES
- 2C MERGE POINT IS LOCATED WHERE TWO VEHICLES LEAVING EACH COD SIMULTANEOUSLY WOULD MEET. THE MERGE POINT STRIPING IS TO BE LOCATED BY OFFSETTING THE INNER PRIMARY LANE BACK OF CURB 9.0' AND OFFSETTING THE OUTER LANE BACK OF CURB 11.0'. AT THE INTERSECTION OF THESE OFFSETS, A 6" YELLOW STRIPE IS TO BE MARKED PERPENDICULAR TO THE OUTER LANE AS WELL AS THE INNER PRIMARY LANE. COMPLIES

3. SIDE BY SIDE DRIVE-THRU STANDARD A EQUIPMENT POSITIONING FOR PRIMARY LANE:

- 3A MIN. 60' (+5'; 60'-65') LINEAR DISTANCE BETWEEN THE CENTER LINE OF THE COD FACE AND THE CENTER LINE OF THE OPEN CASH BOOTH WINDOW AS MEASURED ALONG THE CENTER LINE OF THE LANE. THIS MAY ONLY BE INCREASED IN 20' INCREMENTS (+/- 5' FOR 80', 100' & 120') TO A MAX. OF 120'. 100' IS OPTIMAL. 100' SHOWN, COMPLIES
- 3B THE CENTER OF THE PRIMARY MENU BOARD FOUNDATION IS TO BE 5.3' (5.0' MIN. AND 6.0' MAX.) FROM THE CENTER OF THE COD FOUNDATION. 6.0' SHOWN, COMPLIES
- 3C THE PRIMARY MENU BOARD SHOULD BE AT AN ANGLE APPROXIMATELY 25° TO 35° ANGLE FROM A CAR POSITIONED AT THE COD TO MAXIMIZE SECOND CAR VIEWING. 35° SHOWN, COMPLIES
- 3D AUGER "MCDONALD'S ORDER HERE CANOPY" COD/CANOPY FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES

- 3E A SINGLE BOLLARD SHOULD BE POSITIONED AT THE CORNER OF THE BUILDING ON THE DRIVE-THRU SIDE. IT SHOULD BE FLUSH AGAINST THE BUILDING AND FACE OF THE BOLLARD SHOULD BE TIGHT AGAINST THE BACK OF THE CURB. COMPLIES
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- 3G THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY COD MUST BE 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE. 15' SHOWN, COMPLIES
- 3H THE PRIMARY LANE DETECTOR LOOP SHOULD BE PERPENDICULAR TO THE CENTER OF THE PRIMARY COD.

4. SIDE BY SIDE DRIVE-THRU STANDARD A EQUIPMENT POSITIONING FOR SECONDARY LANE:

- 4A TO POSITION THE SECONDARY COD, DRAW AN ARC WITH A 14' RADIUS THAT IS CENTERED FROM THE MIDPOINT OF THE ISLAND TIP. THEN OFFSET THE FACE OF THE CURB BY 24" TO DETERMINE THE LOCATION OF CENTER OF FOUNDATION OF THE SECONDARY COD. 14' SHOWN, COMPLIES
- 4B THE LOOP DETECTOR IS TO BE 2' FORWARD OF THE COD CENTER LINE WITH THE LOOP FACING FORWARD AND THE DETECTOR LOOP PERPENDICULAR TO THE SECONDARY COD WHEN POSSIBLE. COMPLIES
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- 4E THE SECONDARY MENU SHALL BE AT AN ANGLE OF APPROXIMATELY 25° FROM A VEHICLE POSITIONED AT THE COD WITH 100% VISIBILITY. 25° SHOWN, COMPLIES

5. SIDE BY SIDE DRIVE-THRU STANDARD D DETECTOR LOOP:

- 5A DETECTOR LOOPS SHALL BE LOCATED AT THE CENTER OF THE OPENING WINDOW AT THE CASH AND PRESENT BOOTHS.

SIDE BY SIDE DRIVE-THRU STANDARD A OPTIONAL FEATURES:

1. SIDE BY SIDE DRIVE-THRU STANDARD G OPTIONAL EQUIPMENT:

- 1A PRE-SELL BOARD MUST BE 18"-24" FROM FACE OF CURB. THE DISTANCE BETWEEN THE PRIMARY COD AND PRE-SELL BOARD IS TO BE 15' AS MEASURED ALONG THE CENTER LINE OF THE LANE. THIS IS MEASURED FROM THE CENTER OF THE PRE-SELL BOARD FOUNDATION TO THE CENTER OF THE COD FOUNDATION. THE ANGLE OF THE PRE-SELL BOARD SHOULD MAXIMIZE VISIBILITY TO THE THIRD CAR FROM COD. COMPLIES
- 1B PRE-SELL BOARD MUST BE MIN. 12" FROM FACE OF CURB. THE DISTANCE BETWEEN THE SECONDARY COD AND PRE-SELL BOARD IS TO BE 15' AS MEASURED ALONG FACE OF CURB. THIS IS MEASURED FROM THE POINT PERPENDICULAR TO THE CENTER OF THE COD FOUNDATION TO THE POINT PERPENDICULAR TO THE CENTER OF THE COD FOUNDATION. THE ANGLE OF THE PRE-SELL BOARD SHOULD MAXIMIZE VISIBILITY TO THE SECOND CAR FROM COD (PREFERRED 35°). COMPLIES

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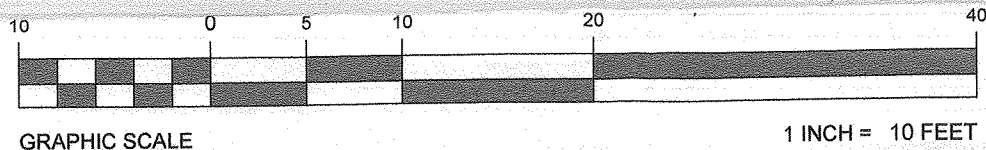
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PROPOSED SITE IMPROVEMENTS	
SECTION 76, BLOCK 1	
STREET ADDRESS: 65 NORTH PLANK	
CITY: TOWN OF NEWBURGH	
COUNTY: ORANGE	
REGIONAL DWG. NO	DRAWING NO
031-0145	031-0145
CAD FILE: J110599-SPP-0B	

DRIVE-THRU ENLARGEMENT

SCALE : 1" = 10'



DRIVE-THRU STANDARD A

3E A SINGLE BOLLARD SHOULD BE POSITIONED AT THE CORNER OF THE BUILDING ON THE DRIVE-THRU SIDE. IT SHOULD BE FLUSH AGAINST THE BUILDING AND FACE OF THE BOLLARD SHOULD BE TIGHT AGAINST THE BACK OF THE CURB. COMPLIES

3F AUGER "McDONALD'S GATEWAY" FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS. COMPLIES

3G THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY COD MUST BE 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE. 15' SHOWN, COMPLIES

3H THE PRIMARY LANE DETECTOR LOOP SHOULD BE PERPENDICULAR TO THE CENTER OF THE PRIMARY COD.

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4B THE LOOP DETECTOR IS TO BE 2' FORWARD OF THE COD CENTER LINE WITH THE LOOP FACING FORWARD AND THE DETECTOR LOOP PERPENDICULAR TO THE SECONDARY COD WHEN POSSIBLE. COMPLIES

4C THE CENTER OF THE SECONDARY MENU BOARD FOUNDATION SHALL BE 5.3' (5.0' MIN. AND 6.0' MAX.) FROM THE CENTER OF THE COD FOUNDATION WITH THE END CAP OF THE SECONDARY MENU BOARD NOT LESS THAN 12" FROM THE FACE OF THE CURB. 6.0' SHOWN, COMPLIES

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SIDE BY SIDE DRIVE-THRU STANDARD A OPTIONAL FEATURES:

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2A DISTANCE BETWEEN THE CENTER LINE OF THE COD FACE AND THE CENTER OF THE CASH BOOTH WINDOW AS MEASURED ALONG THE CENTER LINE OF THE COD SHOULD BE INCREASED IN 20' INCREMENTS (+/- 5' FOR 80', 100' & 120') TO A MAX. 100' SHOWN, COMPLIES

3 THE SECONDARY MENU BOARD FOUNDATION IS TO BE 5.3' (5.0' MIN. AND 6.0' MAX.) FROM THE CENTER OF THE COD FOUNDATION. 6.0' SHOWN, COMPLIES

4 THE SECONDARY MENU BOARD SHOULD BE AT AN ANGLE APPROXIMATELY 25° TO 35° ANGLE FROM THE CENTER OF THE COD TO MAXIMIZE SECOND CAR VIEWING. 35° SHOWN, COMPLIES

5 AUGER "McDONALD'S ORDER HERE CANOPY" COD/CANOPY FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS.

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CO-SIGN SIGNATURES		
CONTRACTOR		
OWNER		

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PROPOSED SITE IMPROVEMENTS		
SECTION 76, BLOCK 1, LOT 1.1		
STREET ADDRESS		
65 NORTH PLANK ROAD		
CITY	STATE	
TOWN OF NEWBURGH	NY	
COUNTY		
ORANGE		
REGIONAL DWG. NO	PLAN DESCRIPTION	
031-0145	DRIVE-THRU PLAN & DETAILS	
CAD FILE: J110599-SPP-0B		

STATUS	DATE	BY
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PLAN CHECKED	08/15/17	LM
AS-BUILT	N/A	N/A

C-11
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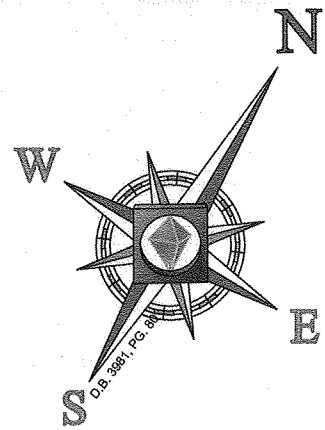
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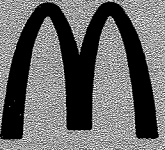
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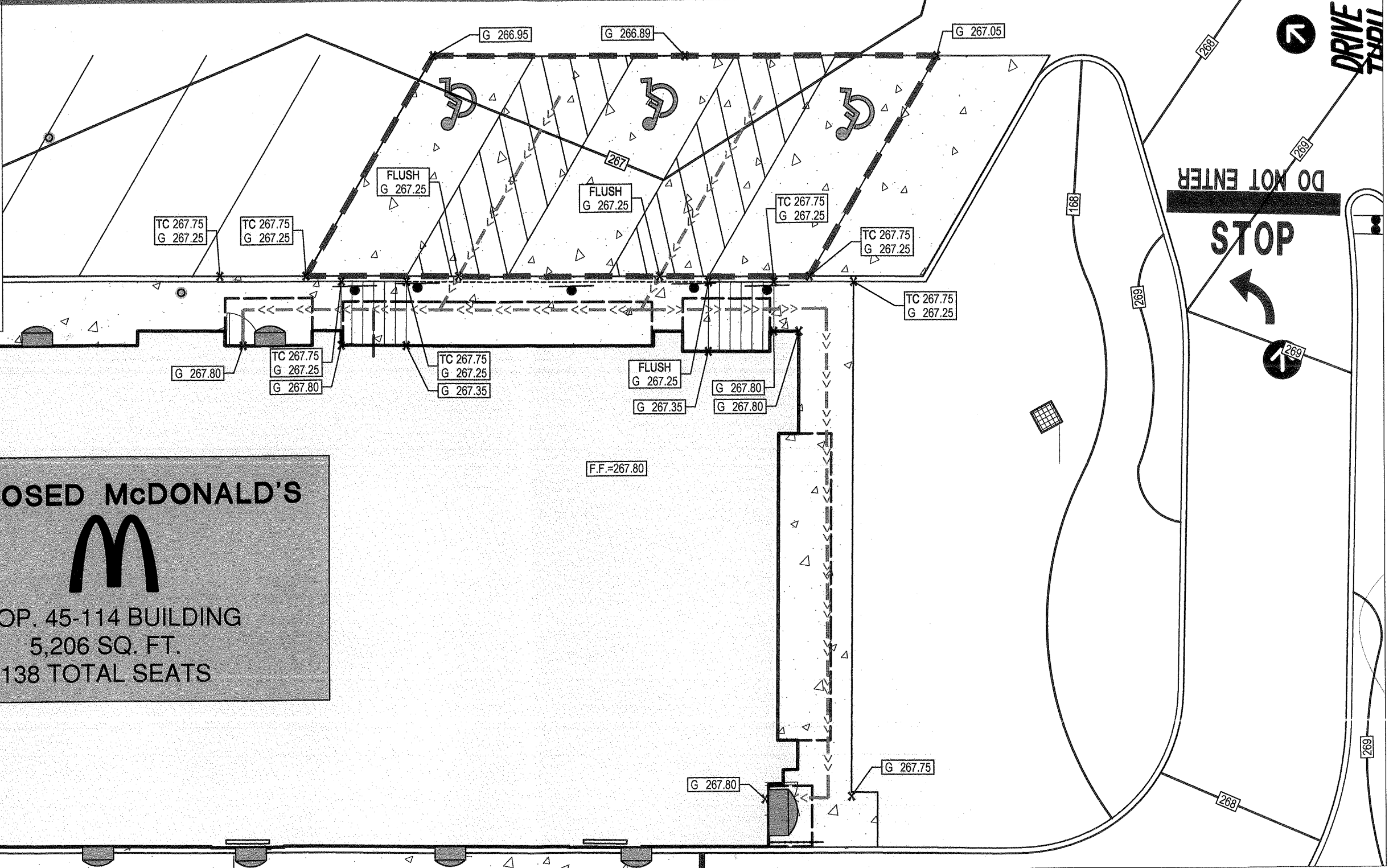
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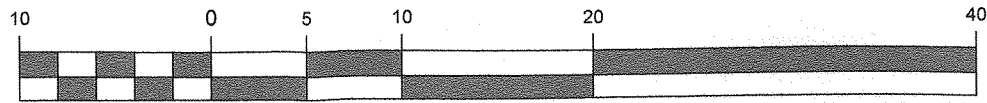


PROPOSED McDONALD'S

PROP. 45-114 BUILDING
5,206 SQ. FT.
138 TOTAL SEATS



ADA ENLARGEMENT

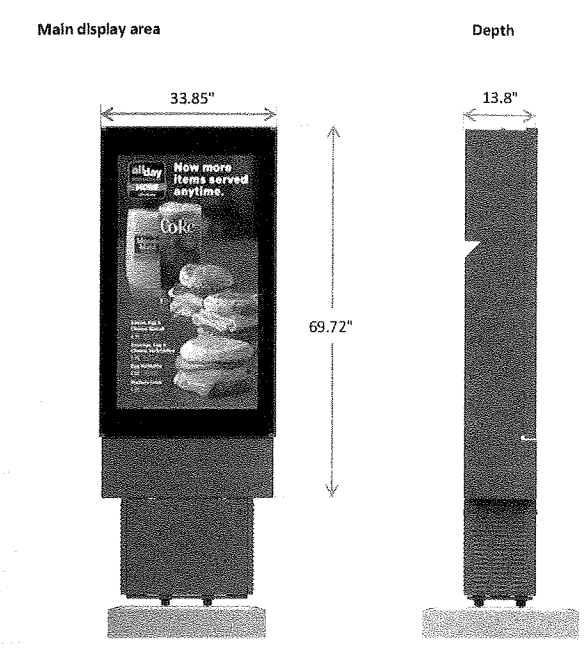
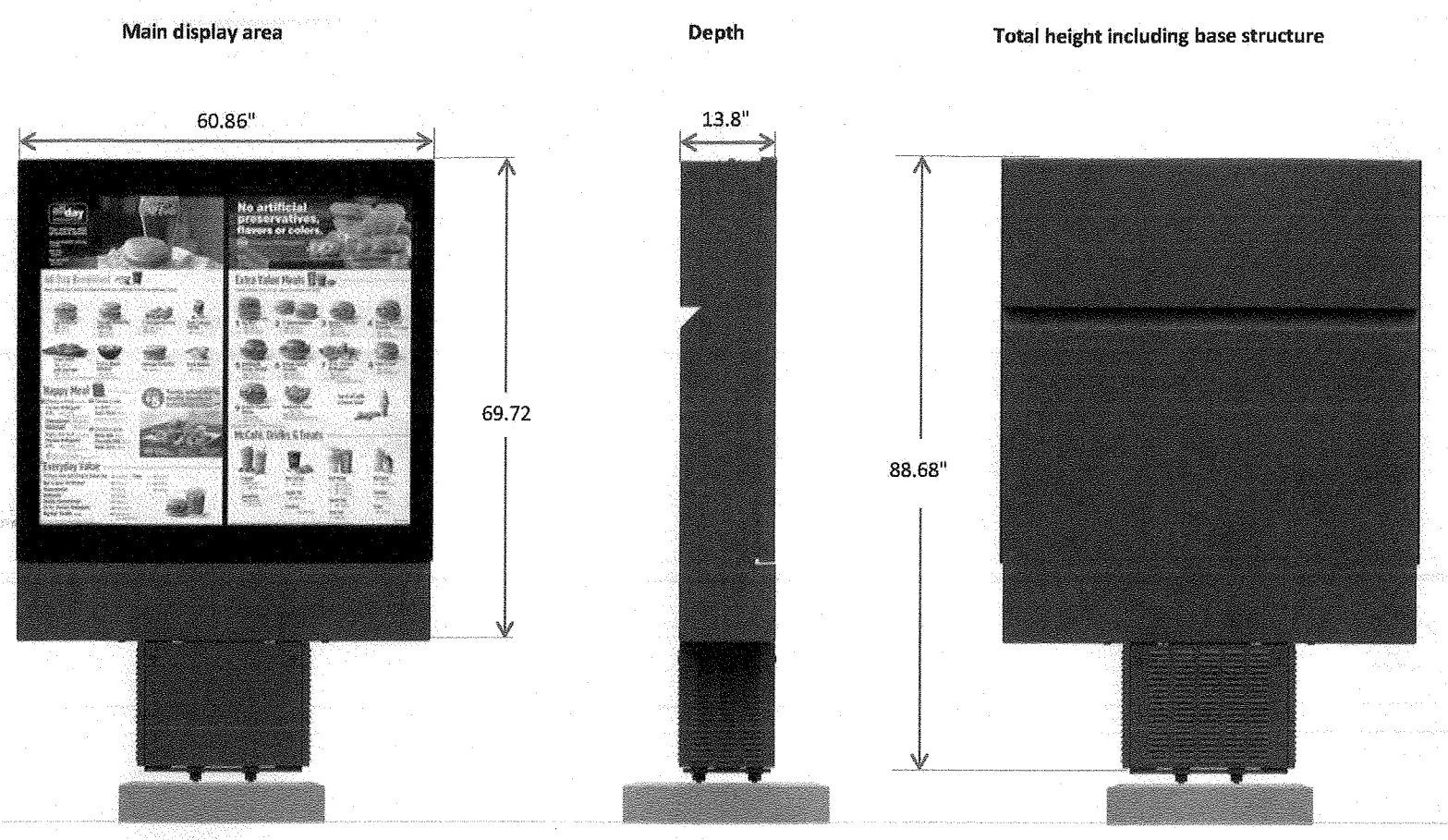
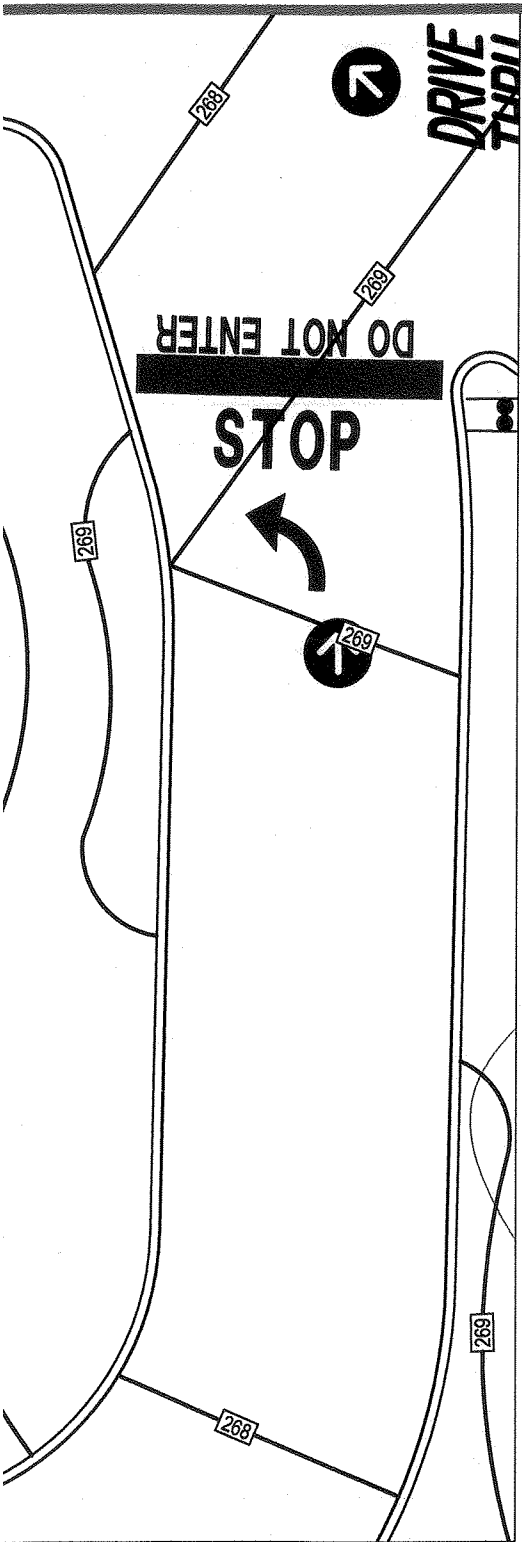
SCALE : 1" = 10'



GRAPHIC SCALE

1 INCH = 10 FEET

ADA PARKING ONLY SIGN W/ VAN ACCESSIBLE LEGEND	4" AZURE BLUE STRIPING (TYP.)	FLUSH CURB & CURB RAMP	AD PARKING SIGN
--	-------------------------------	------------------------	-----------------

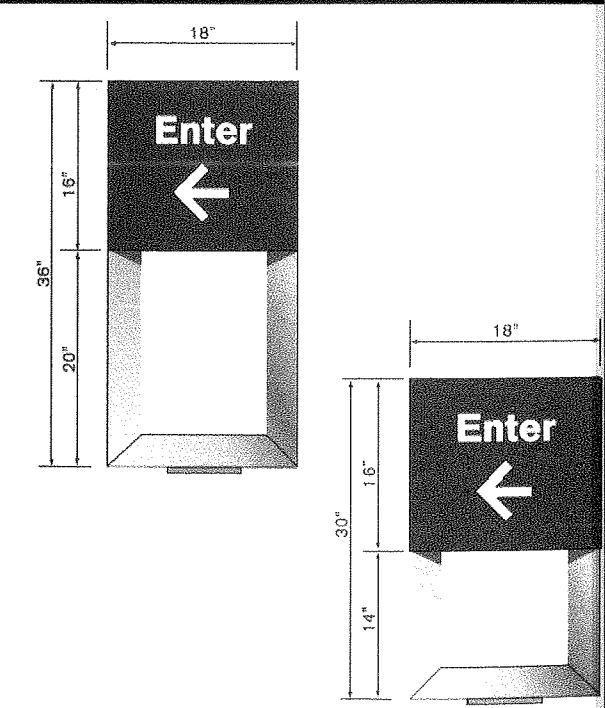
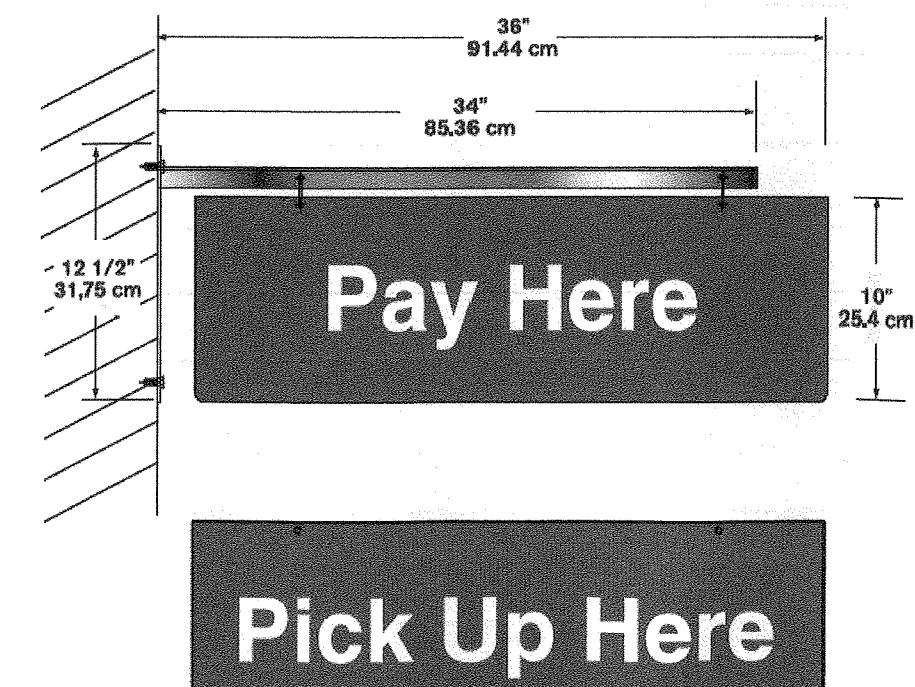
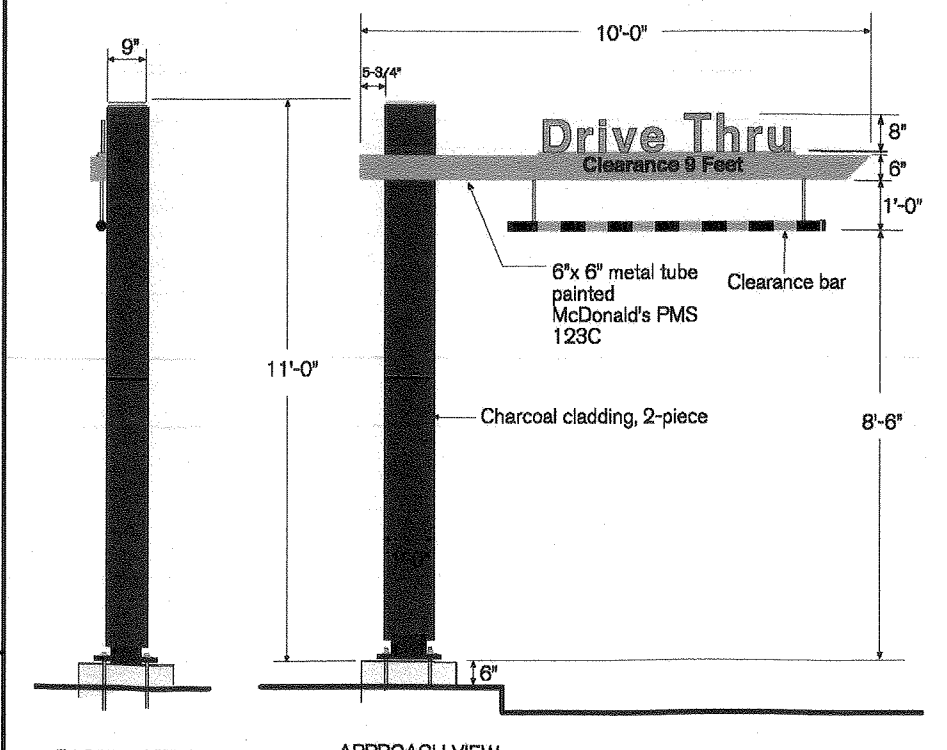


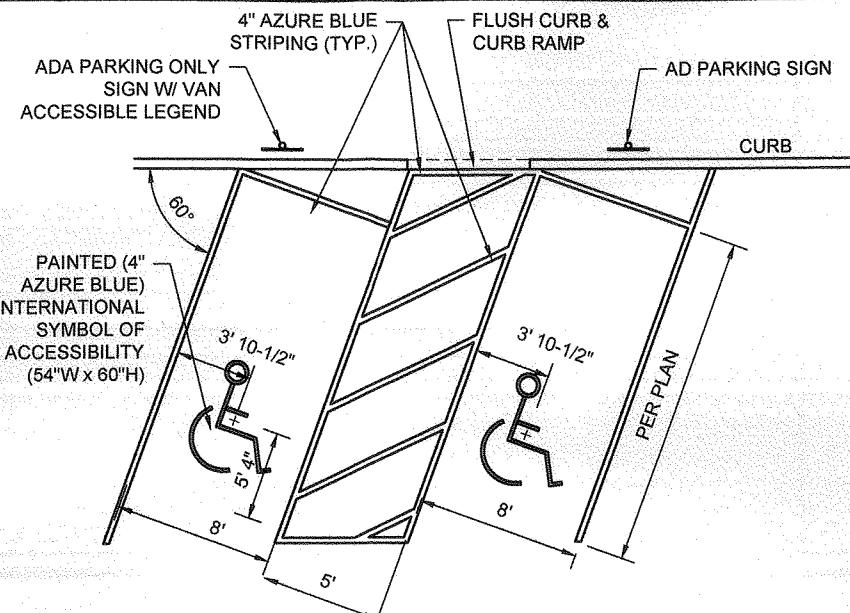
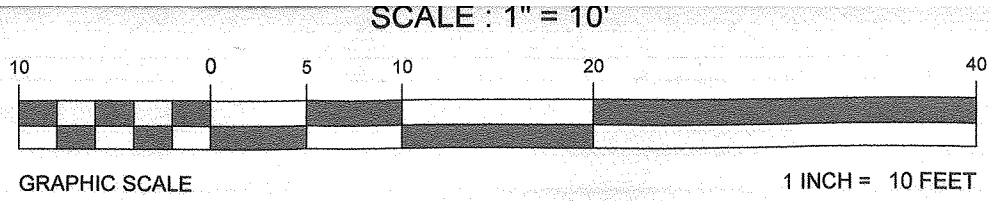
A

MENU BOARD SIGN
NOT TO SCALE (SOURCE McDONALD'S)

B

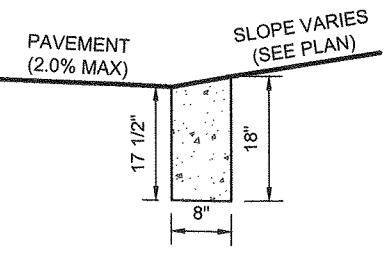
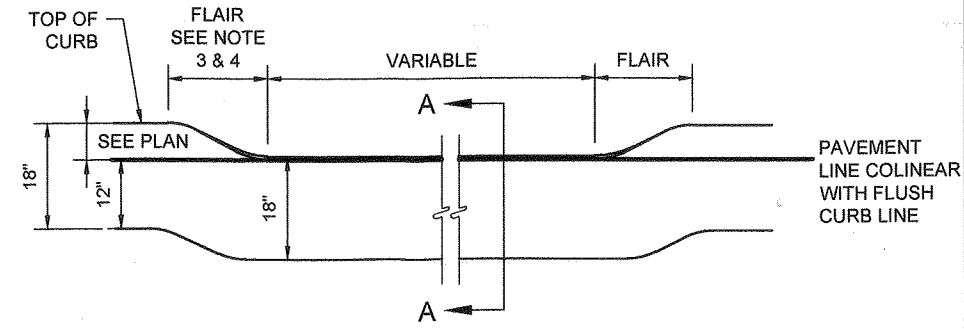
PRESALE MENU
NOT TO SCALE (SOURCE McDONALD'S)



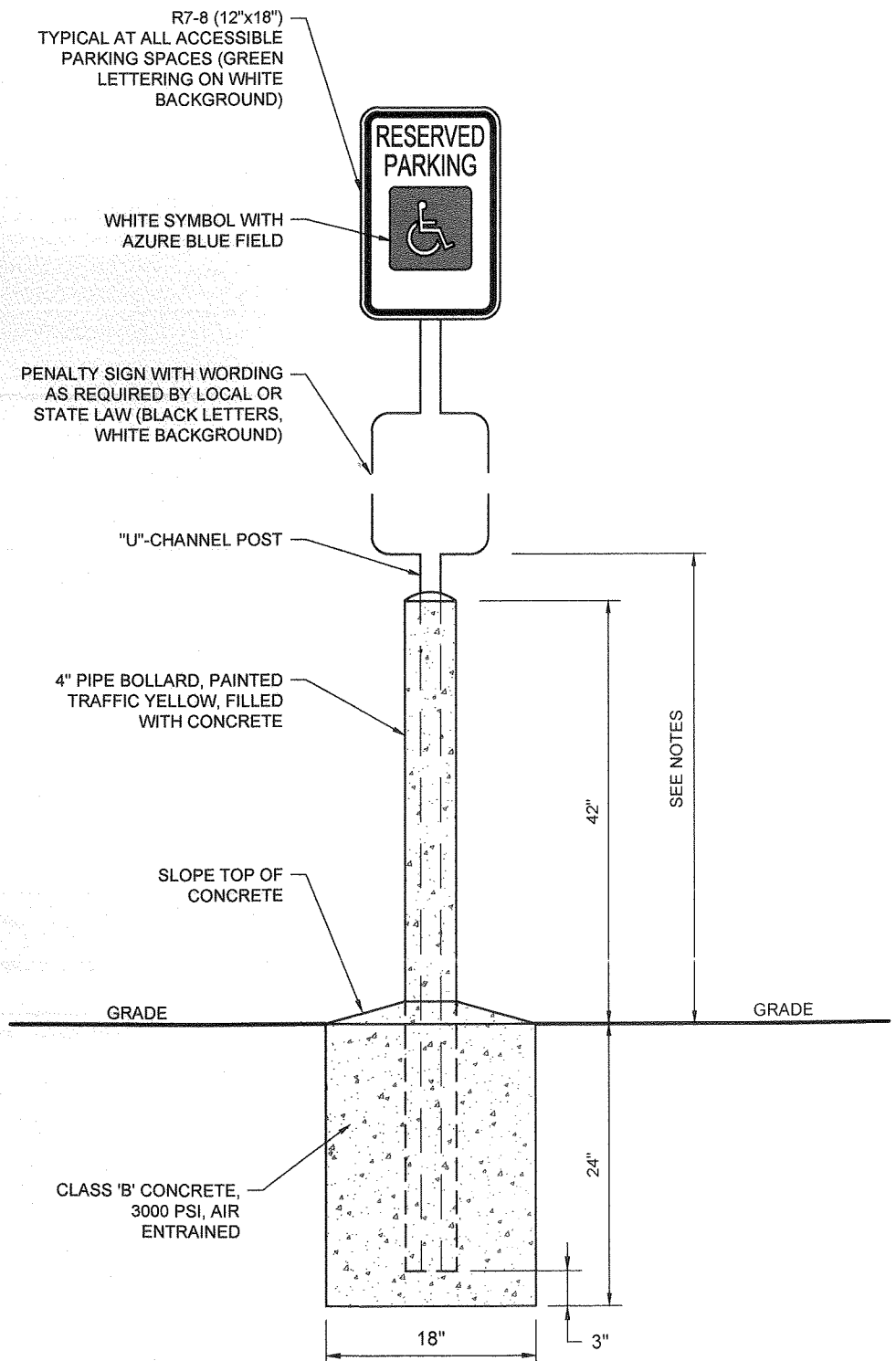


- NOTE:**
1. PARKING SPACES AND PARKING AISLE SLOPE MUST NOT EXCEED 1:48 (1/4" PER FOOT OR 2%) IN ANY DIRECTION.
 2. CURB RAMP SLOPE MUST NOT EXCEED 1:12 (8.3%) FOR A MAXIMUM OF 6 FEET.
 3. LANDINGS SHALL BE PROVIDED AT EACH END OF THE RAMP, MUST PROVIDE POSITIVE DRAINAGE, AND MUST NOT EXCEED 1:48 (1/4" PER FOOT OR 2%) CROSS SLOPE.
 4. A MINIMUM ACCESSIBLE ACCESS ROUTE WIDTH OF 36 INCHES MUST BE PROVIDED AND MAINTAINED. A WHEEL STOP CAN BE PROVIDED WHERE NECESSARY TO PREVENT VEHICLE OVERHANG FROM REDUCING WIDTH OF THE PATH OF TRAVEL TO LESS THAN 36".

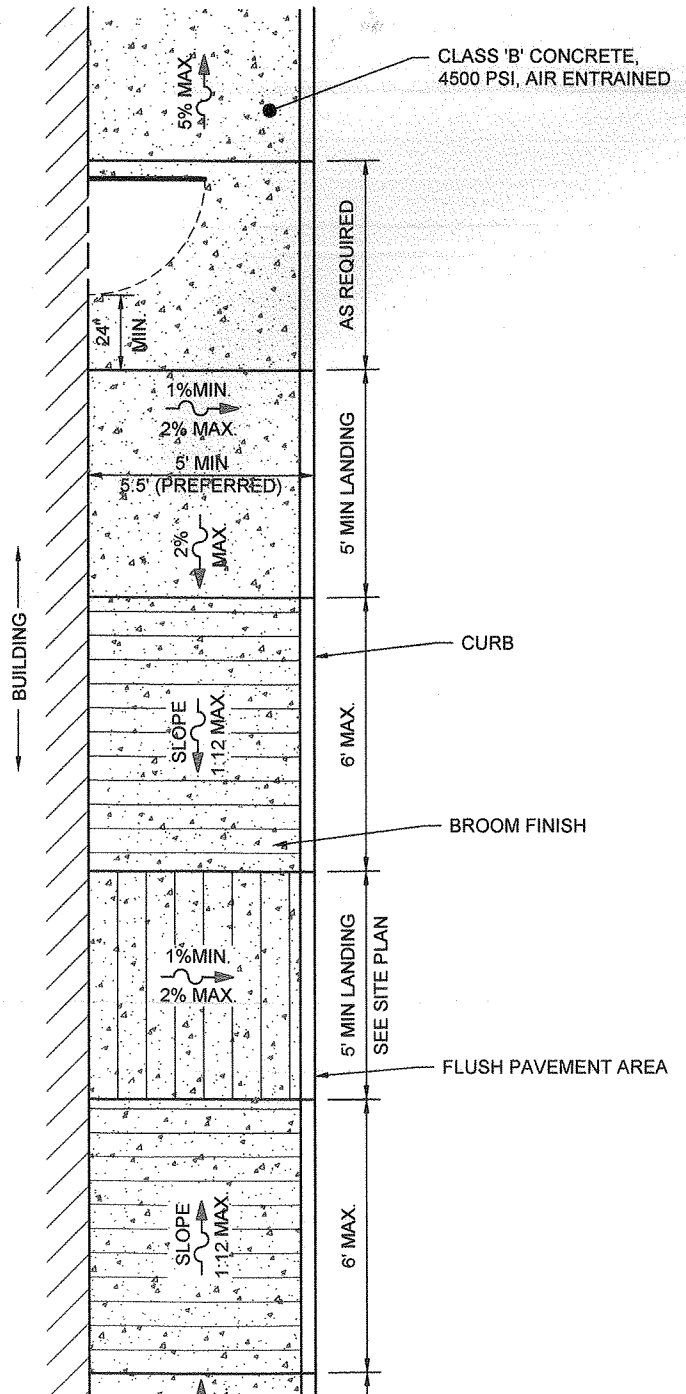
2 ANGLED ADA PARKING STALL MARKING DETAIL
NOT TO SCALE (S070301.01-05/2017)

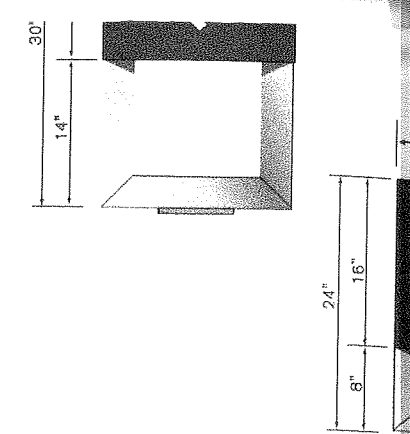
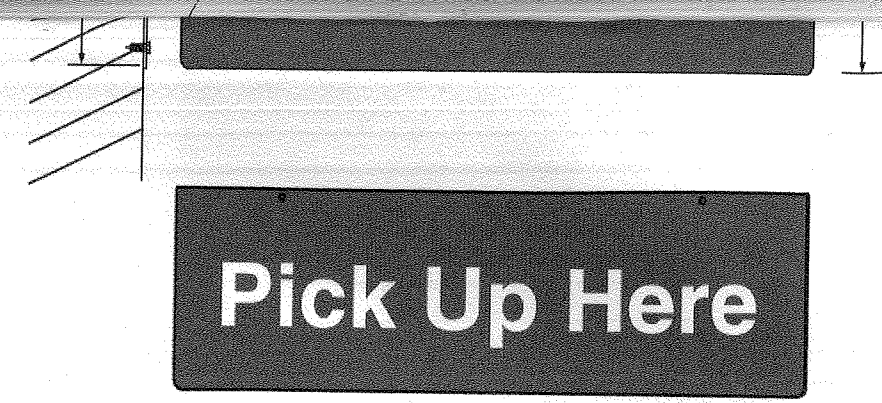
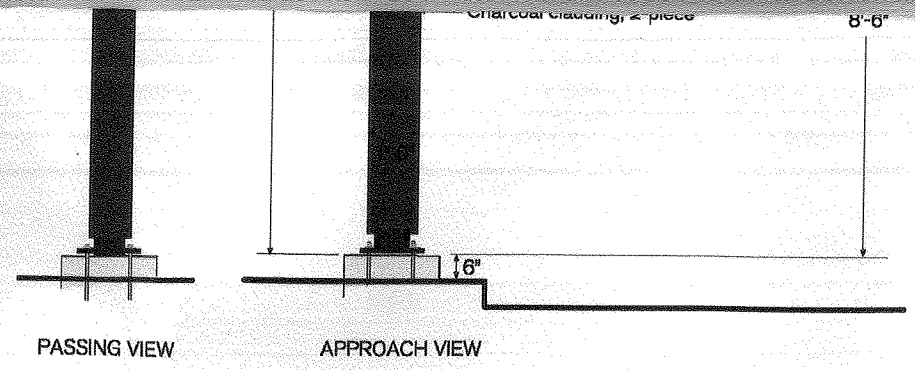


- NOTES:**
1. ALL CONSTRUCTION TO COMPLY WITH BARRIER FREE CODE REQUIREMENTS
 2. SEE STANDARD CURB DETAIL FOR OTHER REQUIREMENTS
 3. WHEN ADJACENT TO A WALKWAY, FLARED SIDE SHALL NOT BE SLOPED MORE THAN 1:12.
 4. WHEN NOT ADJACENT TO A HARD SURFACE WALKWAY, FLARE CAN BE REDUCED TO 18".

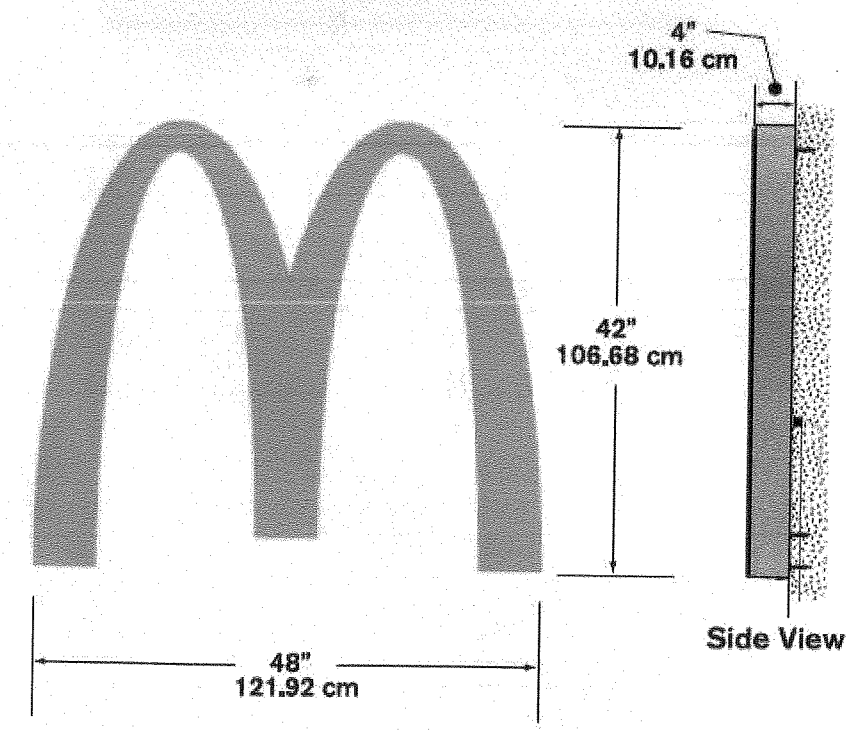


- NOTES:**
1. THE BOTTOM OF THE LOWEST SIGN SHALL BE MOUNTED APPROXIMATELY 60 INCHES ABOVE THE PARKING LOT OR SIDEWALK SURFACE WHEN SIGN IS PARALLEL TO SIDEWALK AND APPROXIMATELY 72 INCHES ABOVE THE PARKING LOT OR SIDEWALK WHEN THE SIGN IS PERPENDICULAR TO THE SIDEWALK
 2. THE PENALTY SIGN SHALL BE CENTERED AND MOUNTED AT THE HEAD OF EACH PARKING SPACE

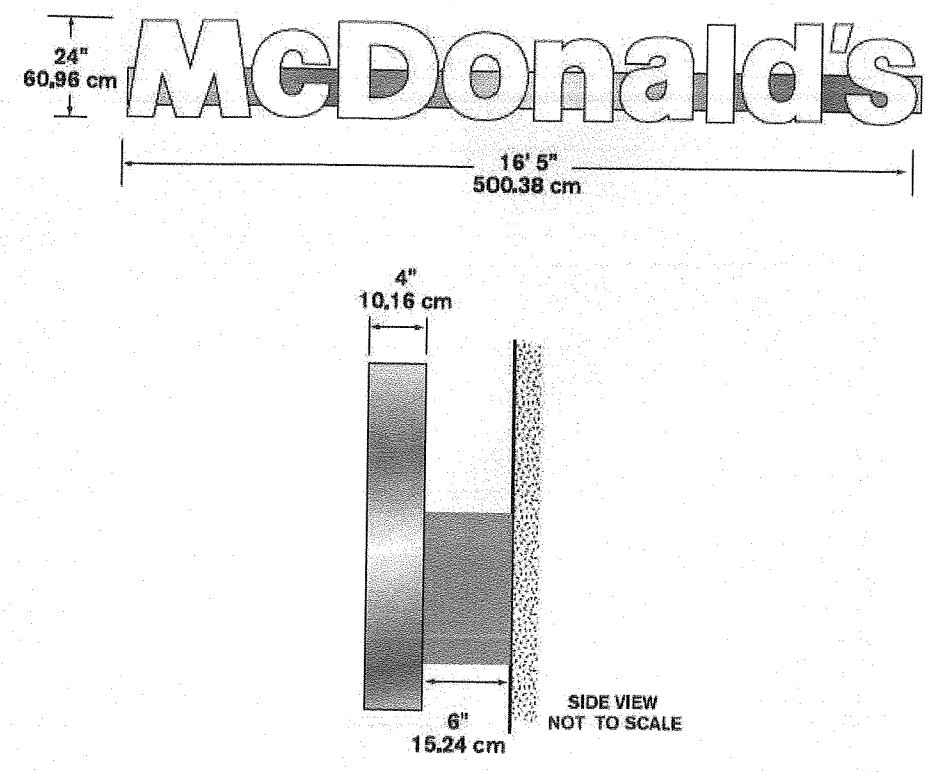




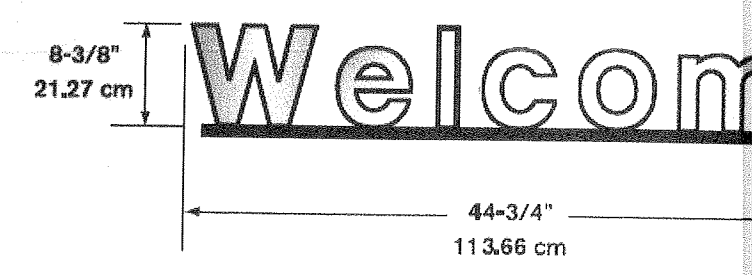
C GATEWAY SIGN (3.30 SF)
NOT TO SCALE (SOURCE: McDONALD'S)



D NEXT GEN WINDOW POSITION SIGNS
NOT TO SCALE (SOURCE: McDONALD'S)



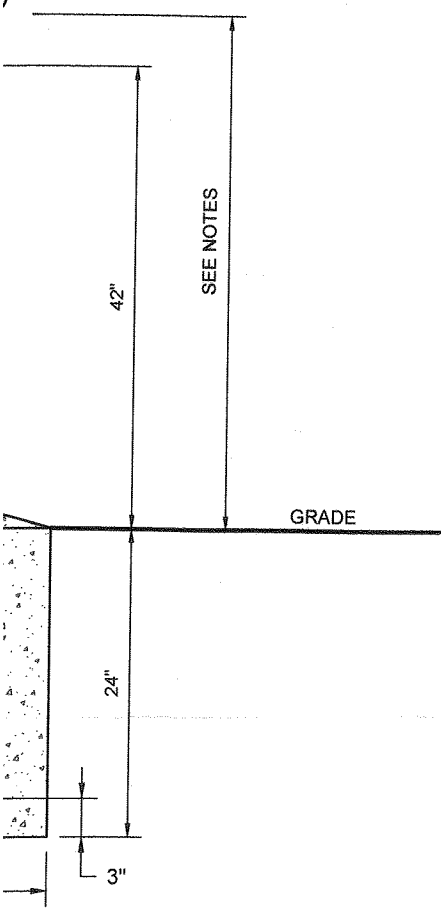
E NEXT GEN S2F DIRECTIONAL SIGN
NOT TO SCALE (SOURCE: McDONALD'S)



F ARCH 'M' LOGO SIGN
NOT TO SCALE (SOURCE: McDONALD'S)

G McDONALD'S FACIA SIGN
NOT TO SCALE (SOURCE: McDONALD'S)

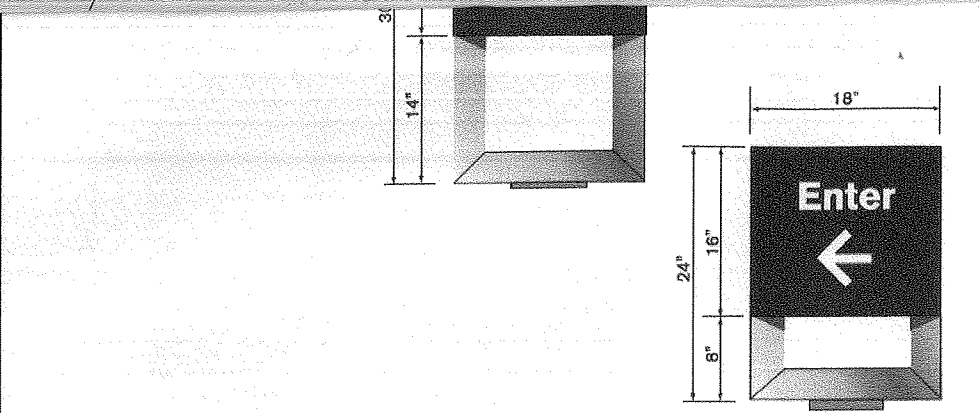
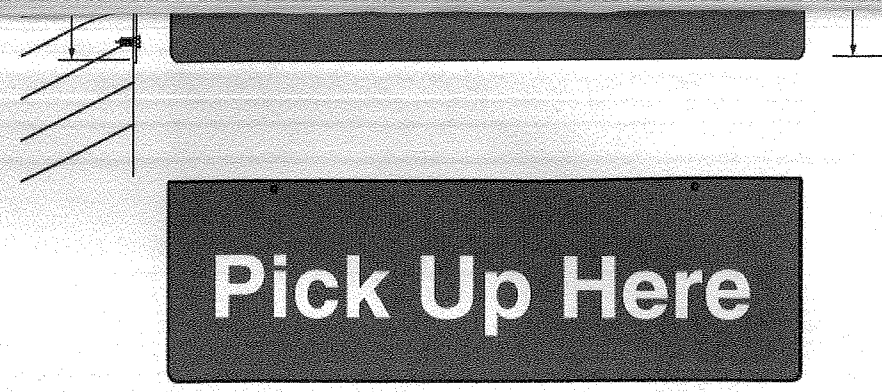
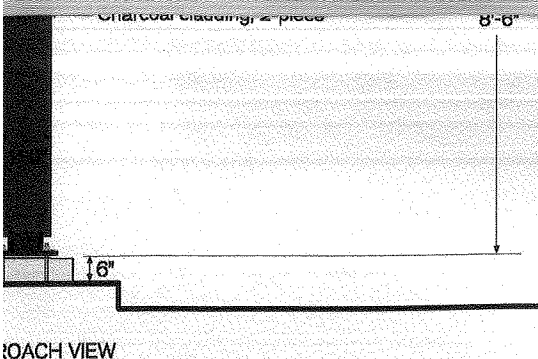
H NON-ILLUMINATED WELCOME SIGN
NOT TO SCALE (SOURCE: McDONALD'S)



SEE NOTES

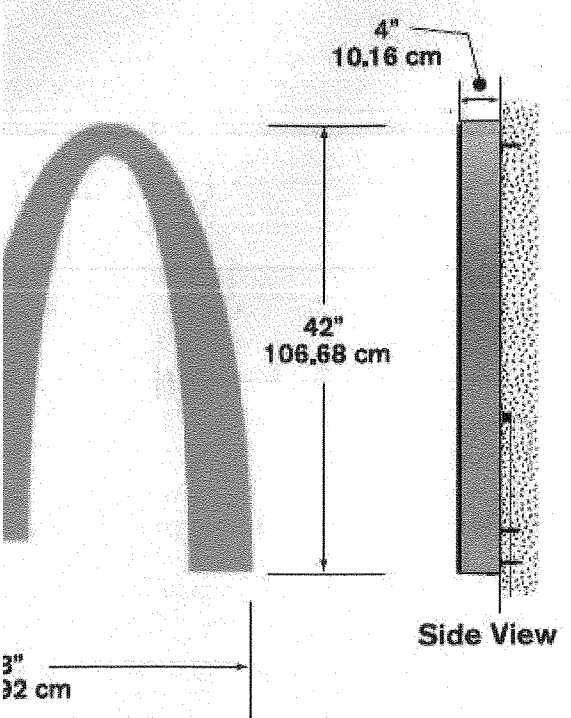
GRADE

BE
OVE
D
N
ND



ATEWAY SIGN (3.30 SF)

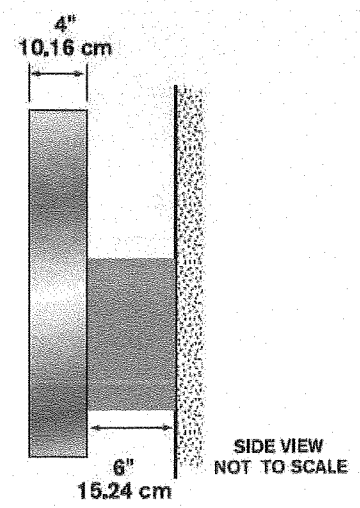
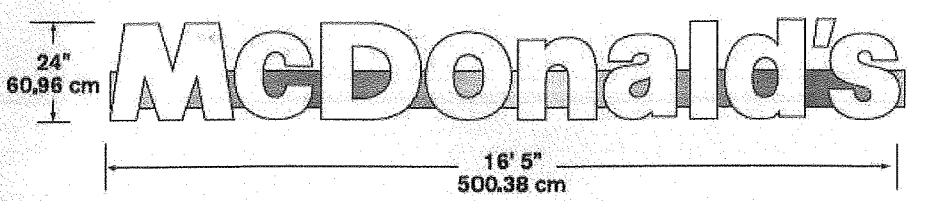
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D

NEXT GEN WINDOW POSITION SIGNS

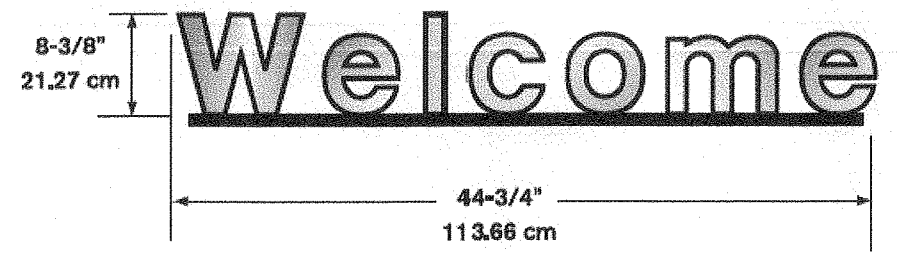
NOT TO SCALE (SOURCE: McDONALD'S)



E

NEXT GEN S2F DIRECTIONAL SIGNS

NOT TO SCALE (SOURCE: McDONALD'S)



ARCH 'M' LOGO SIGN

NOT TO SCALE (SOURCE: McDONALD'S)

G

McDONALD'S FACIA SIGN

NOT TO SCALE (SOURCE: McDONALD'S)

H

NON-ILLUMINATED WELCOME SIGN

NOT TO SCALE (SOURCE: McDONALD'S)

FINAL PLAN

P.M. _____

G.C. _____

O/O _____

McDonald's[®]

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PLAN APPROVALS

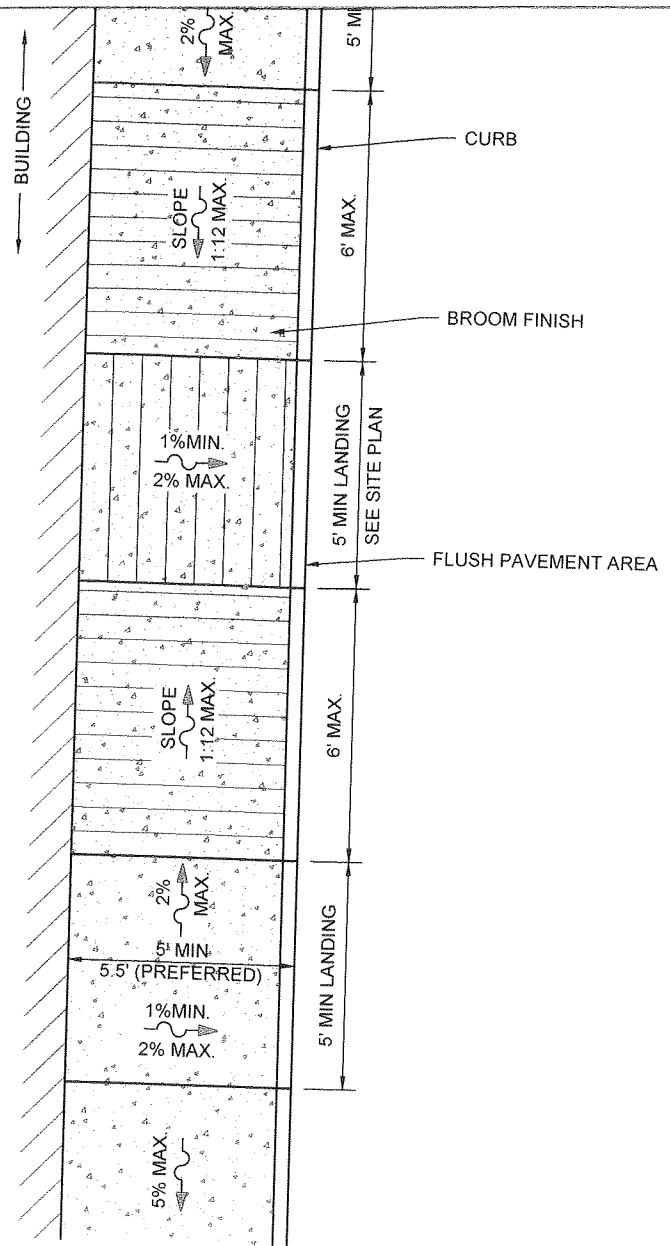
SIGNATURE (2 REQUIRED)	DATE

CO-SIGN SIGNATURES

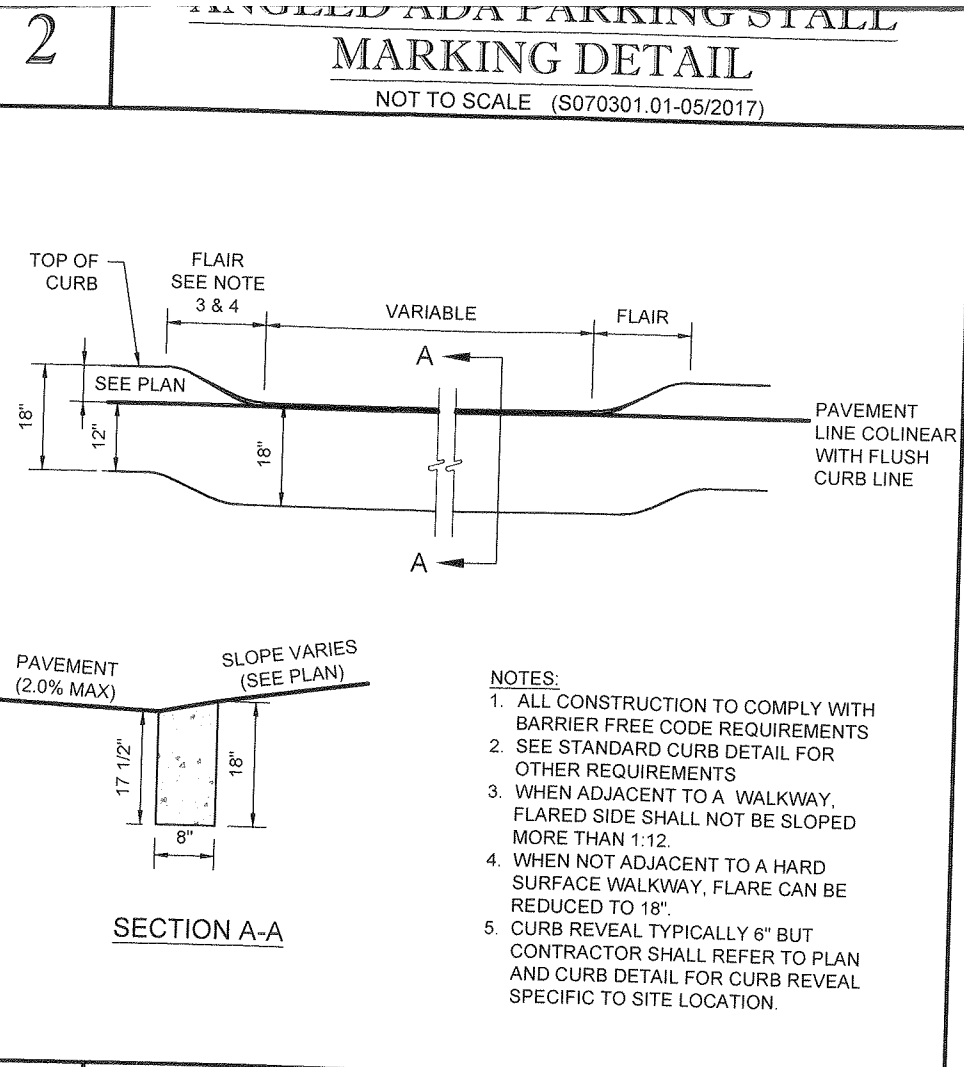
OFFICE: NEW YORK REGION

ADDRESS: 111 WOOD AVENUE SOUTH, SUITE 400
ISELIN, NJ 08830 (732) 623 - 8500

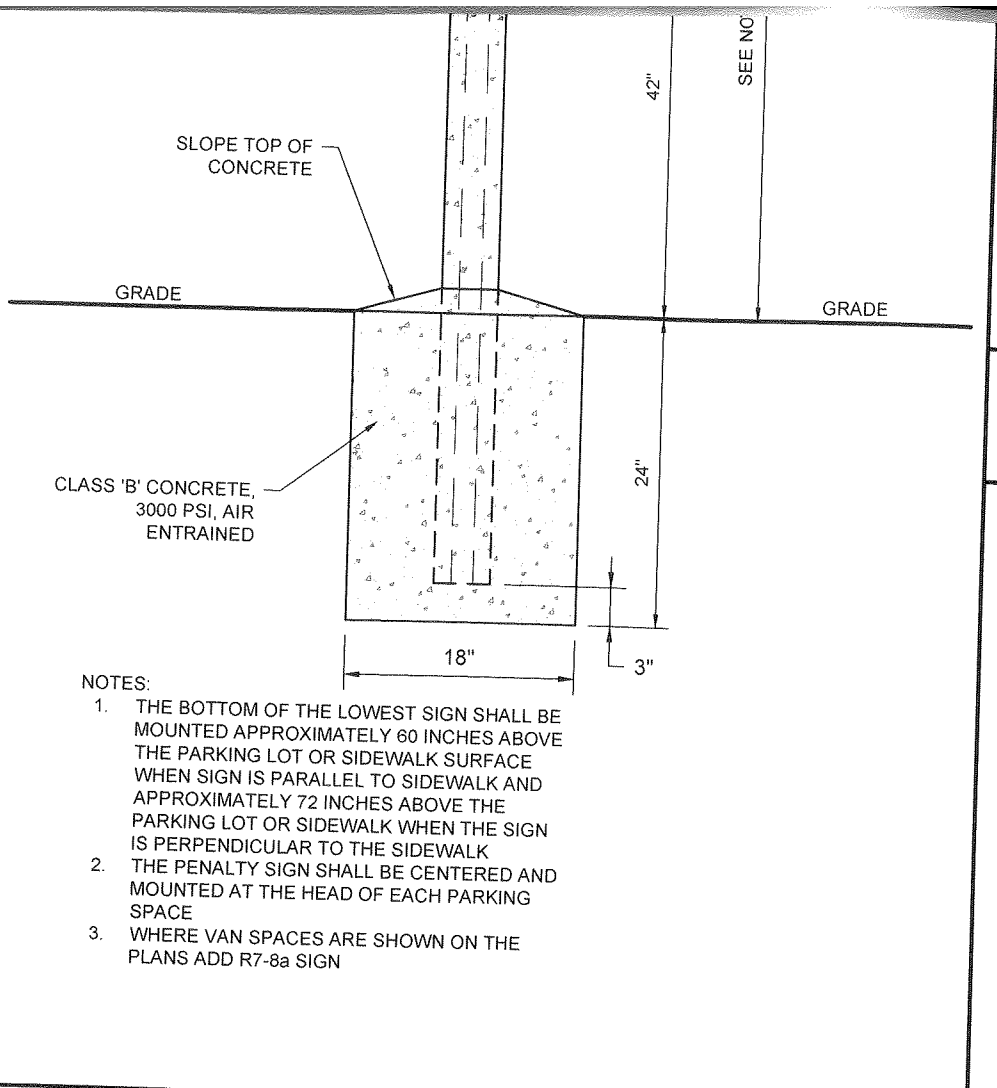
C:\2011\110599\DRAWINGS\CURRENT DRAWINGS\SITE PLAN PACKAGE\110599-SPP-0B-LAYOUT-C-12-ADA



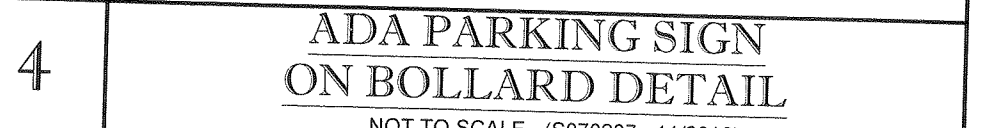
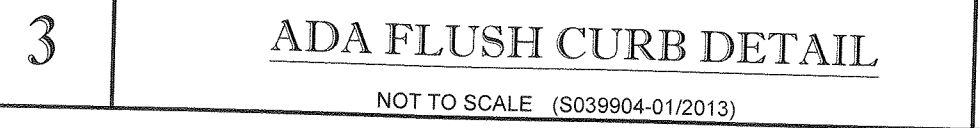
NOTE:
DOOR OPENING MINIMUM DIMENSIONS AND LANDINGS MAY OVERLAP IF APPROVED BY BUILDING CODE OFFICIAL, BUT LESS DESIRABLE.



- NOTES:**
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 2. SEE STANDARD CURB DETAIL FOR OTHER REQUIREMENTS
 3. WHEN ADJACENT TO A WALKWAY, FLARED SIDE SHALL NOT BE SLOPED MORE THAN 1:12.
 4. WHEN NOT ADJACENT TO A HARD SURFACE WALKWAY, FLARE CAN BE REDUCED TO 18".
 5. CURB REVEAL TYPICALLY 6" BUT CONTRACTOR SHALL REFER TO PLAN AND CURB DETAIL FOR CURB REVEAL SPECIFIC TO SITE LOCATION.



- NOTES:**
1. THE BOTTOM OF THE LOWEST SIGN SHALL BE MOUNTED APPROXIMATELY 60 INCHES ABOVE THE PARKING LOT OR SIDEWALK SURFACE WHEN SIGN IS PARALLEL TO SIDEWALK AND APPROXIMATELY 72 INCHES ABOVE THE PARKING LOT OR SIDEWALK WHEN THE SIGN IS PERPENDICULAR TO THE SIDEWALK
 2. THE PENALTY SIGN SHALL BE CENTERED AND MOUNTED AT THE HEAD OF EACH PARKING SPACE
 3. WHERE VAN SPACES ARE SHOWN ON THE PLANS ADD R7-8a SIGN

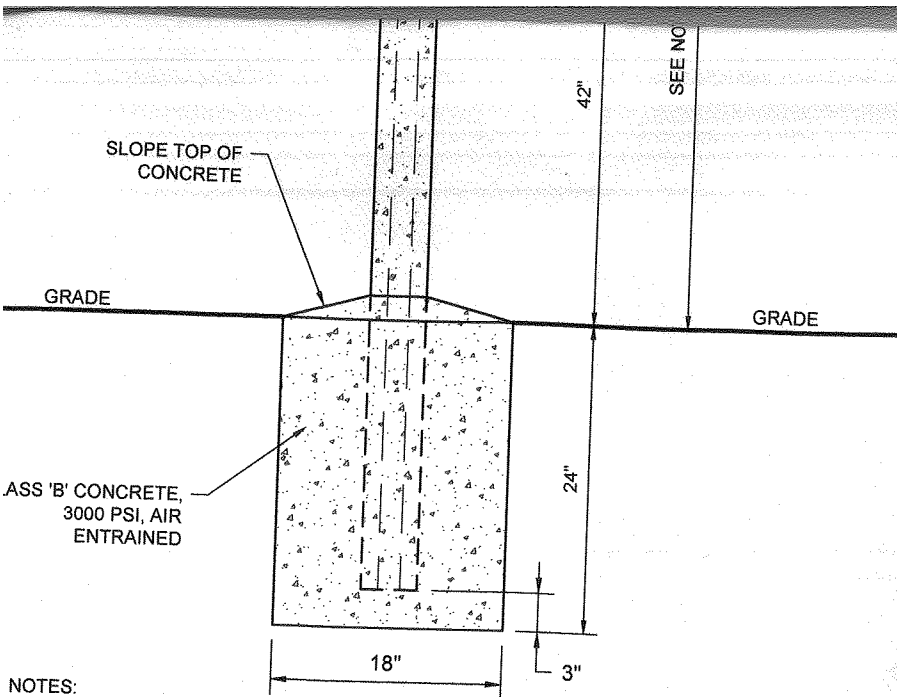


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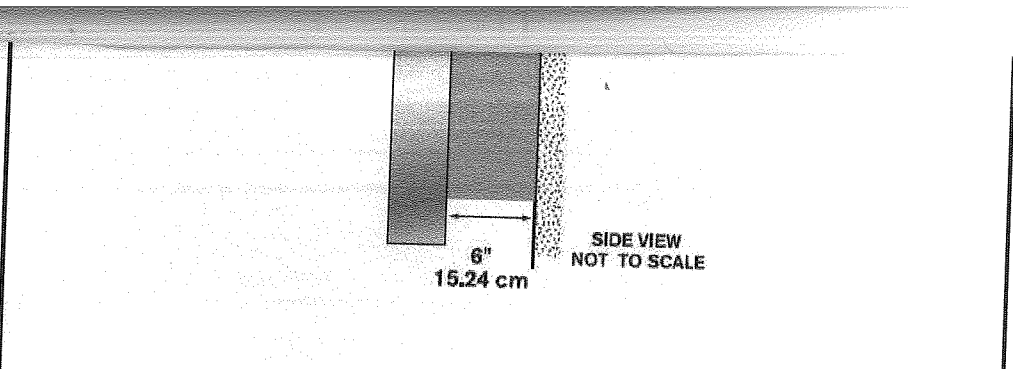
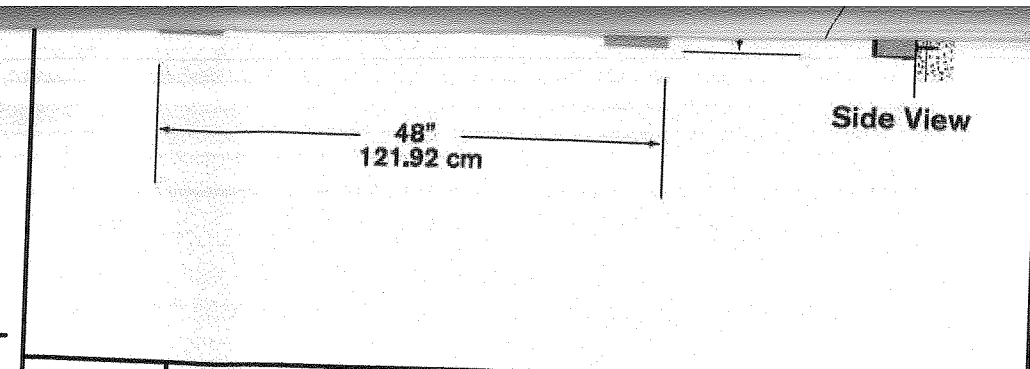
THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTH OF UTILITIES, PIPING, DRYWELLS, ETC. PRIOR TO THE START OF WORK. THE CONTRACTOR MUST CONTACT THE APPROPRIATE LOCAL "ONE CALL" SYSTEM TO ENSURE THAT ALL UTILITIES ARE PROPERLY AND COMPLETELY MARKED OUT IN THE FIELD PRIOR TO ANY WORK AT THE SITE. ANY DISCREPANCIES BETWEEN THE FIELD LOCATIONS AND THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY. THE CONTRACTOR IS ADVISED THAT UTILITY INFORMATION SHOWN ON THE PLAN IS A COMPILATION OF FIELD LOCATIONS BY VARIOUS UTILITY AGENCIES AND RECORD DRAWINGS AND MUST BE CONFIRMED ACCORDINGLY. THE CONTRACTOR IS ALSO ADVISED THAT ALL SUCH FACILITIES DISTURBED DURING CONSTRUCTION MUST BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.

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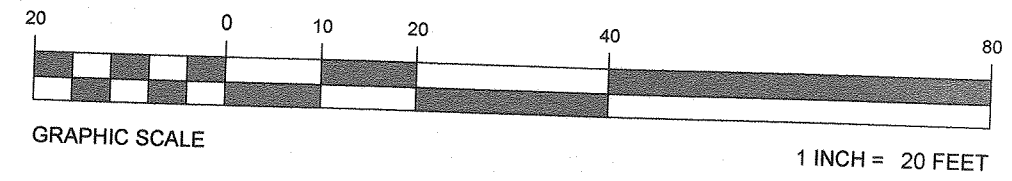


F **ARCH 'M' LOGO SIGN**
NOT TO SCALE (SOURCE: McDONALD'S)

G **McDONALD'S FACIA SIGN**
NOT TO SCALE (SOURCE: McDONALD'S)

H

ADA PARKING SIGN ON BOLLARD DETAIL
NOT TO SCALE (S070207 - 11/2016)



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35 TECHNOLOGY DRIVE
WARREN, NJ 07059
Phone: (908) 668-8300
Fax: (908) 754-4401
www.BohlerEngineering.com

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CORPORATE OFFICE:
♦ WARREN, NJ

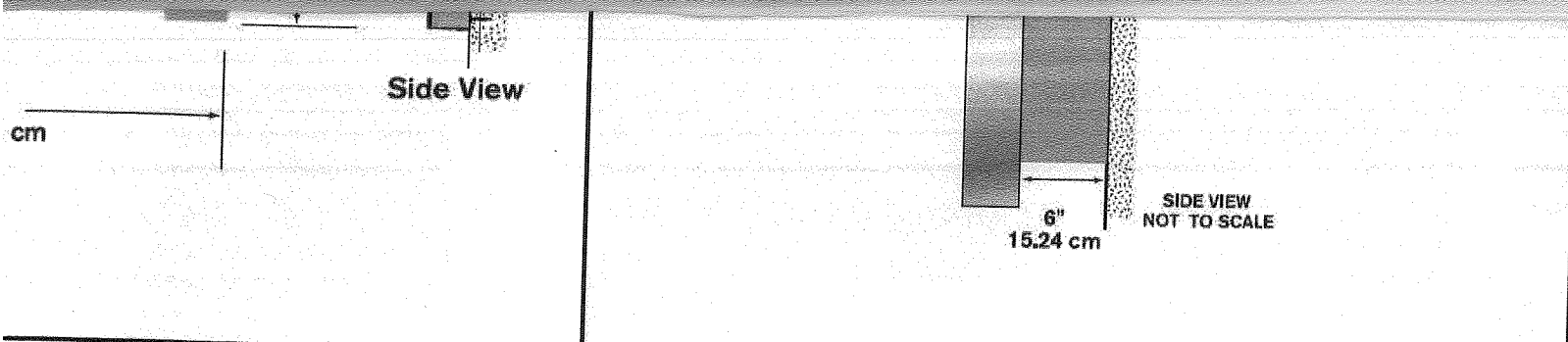
- ♦ SOUTHBOROUGH, MA
- ♦ BOWIE, MD
- ♦ TOWSON, MD
- ♦ ALBANY, NY
- ♦ HAUPPAUGE, NY
- ♦ CENTER VALLEY, PA
- ♦ CHALFONT, PA
- ♦ PHILADELPHIA, PA
- ♦ CHARLOTTE, NC
- ♦ STERLING, VA
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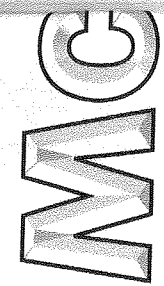
PRO
SEC
6
TOWN
REGIONAL D
031-01
CAD FILE: J110599-SPP-0B



CH 'M' LOGO SIGN
NOT TO SCALE (SOURCE: McDONALD'S)

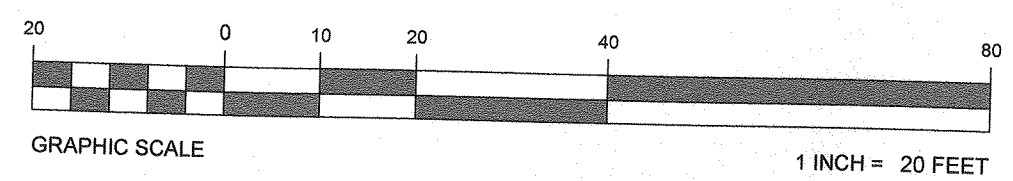
G **McDONALD'S FACIA SIGN**
NOT TO SCALE (SOURCE: McDONALD'S)

H **NON-ILLUMINATED WELCOME SIGN**
NOT TO SCALE (SOURCE: McDONALD'S)



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OFFICE ADDRESS



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PROPOSED SITE IMPROVEMENTS	
SECTION 76, BLOCK 1, LOT 1.1	
STREET ADDRESS 65 NORTH PLANK ROAD	
CITY TOWN OF NEWBURGH	STATE NY
COUNTY ORANGE	
REGIONAL DWG. NO 031-0145	PLAN DESCRIPTION ADA PLAN & DETAILS
CAD FILE: J110599-SPP-0B	

PLAN APPROVALS	SIGNATURE (2 REQUIRED)	DATE	CO-SIGN SIGNATURES		
	REGIONAL MGR.	CONST. MGR.		OPERATIONS DEPT.	REAL ESTATE DEPT.
STATUS		DATE	BY		
PRELIMINARY		08/15/17	KF		
PLAN CHECKED		08/15/17	LM		
AS-BUILT		N/A	N/A		
C-12					
OF 15					

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OFFICE: SOUTHBOROUGH, MA; BOWIE, MD; TOWSON, MD; ALBANY, NY; HAUPPAUGE, NY; CENTER VALLEY, PA; CHALFONT, PA; PHILADELPHIA, PA; CHARLOTTE, NC; STERLING, VA; WARRENTON, VA; FORT LAUDERDALE, FL; TAMPA, FL

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